



**AGENDA FOR THE REGULAR MEETING OF COUNCIL,
HELD ELECTRONICALLY FROM CITY HALL,
141 WEST 14TH STREET, NORTH VANCOUVER, BC, ON
MONDAY, DECEMBER 7, 2020 AT 5:30 PM**

“Live” Broadcast via City Website www.cnv.org/LiveStreaming
Complete Agenda Package available at www.cnv.org/CouncilMeetings

CALL TO ORDER

APPROVAL OF AGENDA

1. Regular Council Meeting Agenda, December 7, 2020

ADOPTION OF MINUTES

2. Regular Council Meeting Minutes, November 23, 2020

PUBLIC INPUT PERIOD

CONSENT AGENDA

Items *3, *4 and *5 are listed in the Consent Agenda and may be considered separately or in one motion.

BYLAWS – ADOPTION

- *3. “Street and Traffic Bylaw, 1991, No. 6234, Amendment Bylaw, 2020, No. 8791”
(E-Bike Share Service Permit)
- *4. “Bylaw Notice Enforcement Bylaw, 2018, No. 8675, Amendment Bylaw, 2020, No. 8792” (E-Bike Share Permit Enforcement)
- *5. “Zoning Bylaw, 1995, No. 6700, Amendment Bylaw, 2020, No. 8700”
(Bryan Lockhart / Black Kettle Brewing Inc., 720 Copping Street, CD-728)

PUBLIC HEARING – 115 East 1st Street

BYLAW – THIRD READING

6. “Zoning Bylaw, 1995, No. 6700, Amendment Bylaw, 2020, No. 8808”
(Eggs Cana / Oana Nicoara, 115 East 1st Street, CD-731)

PRESENTATION

2021 Utility Rate Setting – Director, Finance

REPORT

7. 2021 Utility Rates

BYLAWS – FIRST, SECOND AND THIRD READINGS

8. “Water Utility Bylaw, 1994, No. 6417, Amendment Bylaw, 2020, No. 8811”
9. “Sewerage and Drainage Utility Bylaw, 1995, No. 6746, Amendment Bylaw, 2020, No. 8812”
10. “Solid Waste Management Service Bylaw, 1997, No. 6920, Amendment Bylaw, 2020, No. 8813”

REPORTS

11. Lonsdale Energy Corp. Director Nomination
12. Metro Vancouver Regional Industrial Lands Strategy
13. Parkland Acquisition – Portion of 250 East 15th Street – Funding Appropriation

BYLAW – FIRST, SECOND AND THIRD READINGS

14. “Development Cost Charge (Parks) Reserve Fund Bylaw, 2020, No. 8809”
(Purchase of Land for Park Purposes at 16th Street and St. Andrews Avenue)

REPORTS

15. 2020 Children and Youth Initiatives Fund – Special Project Grant Applications
16. Spirit Trail Mosquito Creek Marina – Funding Reallocation
17. Mickey McDougall Feasibility Study – Extension of Funding Request

COVID-19 UPDATE

COUNCIL INQUIRIES / REPORTS

NEW ITEMS OF BUSINESS

NOTICES OF MOTION

RECESS TO CLOSED SESSION

THAT Council recess to the Committee of the Whole (Closed Session) pursuant to the *Community Charter*, Sections 90(1)(a) [personal information] and 90(1)(c) [employee relations].

REPORT OF THE COMMITTEE OF THE WHOLE (CLOSED SESSION)

ADJOURN

CALL TO ORDER

APPROVAL OF AGENDA

1. Regular Council Meeting Agenda, December 7, 2020

ADOPTION OF MINUTES

2. Regular Council Meeting Minutes, November 23, 2020

PUBLIC INPUT PERIOD

The Public Input Period is addressed in sections 12.20 to 12.28 of “Council Procedure Bylaw, 2015, No. 8500.”

The time allotted for each speaker addressing Council during the Public Input Period is 2 minutes, with the number of speakers set at 5 persons. Speakers’ comments will be audio recorded, as well as live-streamed on the City’s website, and will form part of the public record.

As City Hall remains closed to the public, the Regular Council Meetings will be held electronically via “WebEx”. To speak during the Public Input Period of a Regular Council Meeting, pre-registration is required by completing an online form at cnv.org/PublicInputPeriod. Persons can also pre-register by phoning 604-990-4230 and providing contact information. **All pre-registration must be submitted no later than 12:00 noon on the day of the meeting.**

Once you have pre-registered, you will receive login/call-in instructions via email/phone.

You will be required to login or phone into the Council meeting between 5:00 and 5:15 pm on the day of the meeting. At the meeting, speakers will be asked to state their name and address for the record. If speakers have written materials to accompany their presentation, these materials must be emailed to the Corporate Officer at clerks@cnv.org no later than 12:00 noon on the day of the meeting.

The Public Input Period provides an opportunity for comment only and places the speaker’s concern on record, without the expectation of a response from Council.

Speakers must comply with the General Rules of Conduct set out in section 5.1 of “Council Procedure Bylaw, 2015, No. 8500” and may not speak with respect to items as listed in section 12.25(2).

Speakers are requested not to address matters that refer to items from a concluded Public Hearing/Public Meeting or to Public Hearings, Public Meetings and Committee meetings when those matters are scheduled on the same evening’s agenda, as an opportunity for public input is provided when the particular item comes forward for discussion.

Please address the Mayor as “Your Worship” or “Mayor, followed by his/her surname”. Councillors should be addressed as “Councillor, followed by their surname”.

CONSENT AGENDA

Items *3, *4 and *5 are listed in the Consent Agenda and may be considered separately or in one motion.

RECOMMENDATION:

THAT the recommendations listed within the “Consent Agenda” be approved.

START OF CONSENT AGENDA

BYLAWS – ADOPTION

- *3. “Street and Traffic Bylaw, 1991, No. 6234, Amendment Bylaw, 2020, No. 8791” (E-Bike Share Service Permit)

RECOMMENDATION:

THAT “Street and Traffic Bylaw, 1991, No. 6234, Amendment Bylaw, 2020, No. 8791” (E-Bike Share Service Permit) be adopted, signed by the Mayor and Corporate Officer and affixed with the corporate seal.

- *4. “Bylaw Notice Enforcement Bylaw, 2018, No. 8675, Amendment Bylaw, 2020, No. 8792” (E-Bike Share Permit Enforcement)

RECOMMENDATION:

THAT “Bylaw Notice Enforcement Bylaw, 2018, No. 8675, Amendment Bylaw, 2020, No. 8792” (E-Bike Share Permit Enforcement) be adopted, signed by the Mayor and Corporate Officer and affixed with the corporate seal.

- *5. “Zoning Bylaw, 1995, No. 6700, Amendment Bylaw, 2020, No. 8700” (Bryan Lockhart / Black Kettle Brewing Inc., 720 Copping Street, CD-728)

RECOMMENDATION:

THAT “Zoning Bylaw, 1995, No. 6700, Amendment Bylaw, 2020, No. 8700” (Bryan Lockhart / Black Kettle Brewing Inc., 720 Copping Street, CD-728) be adopted, signed by the Mayor and Corporate Officer and affixed with the corporate seal.

END OF CONSENT AGENDA

PUBLIC HEARING – 115 East 1st Street – 5:30 PM

“Zoning Bylaw, 1995, No. 6700, Amendment Bylaw, 2020, No. 8808” (Eggs Cana / Oana Nicoara, 115 East 1st Street, CD-731) would rezone the subject property from the Lower Lonsdale 2 (LL-2) Zone to a Comprehensive Development 730 (CD-730) Zone to allow a Cannabis Retail Store on the property.

Bylaw No. 8808 to be considered under Item 6.

AGENDA

Staff presentation

Applicant presentation

Representations from the public

Questions from Council

Motion to conclude the Public Hearing

BYLAW – THIRD READING

6. “Zoning Bylaw, 1995, No. 6700, Amendment Bylaw, 2020, No. 8808”
(Eggs Cana / Oana Nicoara, 115 East 1st Street, CD-731)

RECOMMENDATION:

THAT “Zoning Bylaw, 1995, No. 6700, Amendment Bylaw, 2020, No. 8808”
(Eggs Cana / Oana Nicoara, 115 East 1st Street, CD-731) be given third reading.

PRESENTATION

2021 Utility Rate Setting – Director, Finance

Item 7 refers.

REPORT

7. 2021 Utility Rates

Report: Director, Finance, November 25, 2020

RECOMMENDATION:

PURSUANT to the report of the Director, Finance, dated November 25, 2020,
entitled “2021 Utility Rates”:

Continued...

REPORT – Continued

7. 2021 Utility Rates – Continued

THAT the following bylaws be considered:

- “Water Utility Bylaw, 1994, No. 6417, Amendment Bylaw, 2020, No. 8811”;
- “Sewerage and Drainage Utility Bylaw, 1995, No. 6746, Amendment Bylaw, 2020, No. 8812”; and
- “Solid Waste Management Service Bylaw, 1997, No. 6920, Amendment Bylaw, 2020, No. 8813”.

Items 8, 9 and 10 refer.

Information Report, November 25, 2020 – “2021 Water Utility Update”

Information Report, November 25, 2020 – “2021 Sewerage and Drainage Utility Update”

Information Report, November 25, 2020 – “2021 Solid Waste Utility Update”

BYLAWS – FIRST, SECOND AND THIRD READINGS

8. “Water Utility Bylaw, 1994, No. 6417, Amendment Bylaw, 2020, No. 8811”

RECOMMENDATION:

THAT “Water Utility Bylaw, 1994, No. 6417, Amendment Bylaw, 2020, No. 8811” be given first, second and third readings.

9. “Sewerage and Drainage Utility Bylaw, 1995, No. 6746, Amendment Bylaw, 2020, No. 8812”

RECOMMENDATION:

THAT “Sewerage and Drainage Utility Bylaw, 1995, No. 6746, Amendment Bylaw, 2020, No. 8812” be given first, second and third readings.

10. “Solid Waste Management Service Bylaw, 1997, No. 6920, Amendment Bylaw, 2020, No. 8813”

RECOMMENDATION:

THAT “Solid Waste Management Service Bylaw, 1997, No. 6920, Amendment Bylaw, 2020, No. 8813” be given first, second and third readings.

REPORTS

11. Lonsdale Energy Corp. Director Nomination – File: 11-5500-06-0001/1

Report: Secretary-Treasurer, Lonsdale Energy Corp., November 24, 2020

RECOMMENDATION:

PURSUANT to the report of the Secretary-Treasurer, Lonsdale Energy Corp., dated November 24, 2020, entitled “Lonsdale Energy Corp. Director Nomination”:

THAT the Unanimous Consent Resolutions of the Shareholder of Lonsdale Energy Corp. be endorsed;

AND THAT the Mayor and Corporate Officer be authorized to sign and seal the resolution.

12. Metro Vancouver Regional Industrial Lands Strategy
– File: 13-6440-01-0001/2020

Report: Manager, Long Range and Community Planning, November 25, 2020

RECOMMENDATION:

PURSUANT to the report of the Manager, Long Range and Community Planning, dated November 25, 2020, entitled “Metro Vancouver Regional Industrial Lands Strategy”:

THAT the Metro Vancouver Regional Industrial Lands Strategy be endorsed;

AND THAT staff be directed to consider and recommend opportunities to implement the Strategy's actions, as appropriate.

13. Parkland Acquisition – Portion of 250 East 15th Street – Funding Appropriation
– File: 02-2260-01-0001/2020

Report: Property Services Coordinator and Manager, Parks and Environment, November 25, 2020

RECOMMENDATION:

PURSUANT to the report of the Property Services Coordinator and the Manager, Parks and Environment, dated November 25, 2020, entitled “Parkland Acquisition – Portion of 250 East 15th Street – Funding Appropriation”:

Continued...

REPORTS – Continued

13. Parkland Acquisition – Portion of 250 East 15th Street – Funding Appropriation
– File: 02-2260-01-0001/2020 – Continued

THAT “Development Cost Charge (Parks) Reserve Fund Bylaw, 2020, No. 8809” (Purchase of Land for Park Purposes at 16th Street and St. Andrews Avenue), a Bylaw to appropriate an amount of \$1,386,000 from the Development Cost Charge (Parks) Reserve Fund, be considered;

THAT (Funding Appropriation #2076) an amount of \$14,000 be appropriated from the General Reserve Fund for the purpose of funding the Project;

THAT should any of the amount remain unexpended as at December 31, 2023, the unexpended balance shall be returned to the credit of the General Reserve Fund;

THAT staff be authorized to purchase a portion of 250 East 15th Street for park purposes, in an amount not to exceed \$1,400,000, including applicable taxes, expenses and fees;

THAT the Mayor and Corporate Officer be authorized to sign and seal any such documents as may be required in order to give effect to this motion;

AND THAT the Mayor and Corporate Officer be authorized to sign a Licence Agreement allowing the owners of 250 East 15th Street to use the City’s newly purchased property as a staging area during the construction of the proposed adjacent development.

Item 14 refers.

BYLAW – FIRST, SECOND AND THIRD READINGS

14. “Development Cost Charge (Parks) Reserve Fund Bylaw, 2020, No. 8809” (Purchase of Land for Park Purposes at 16th Street and St. Andrews Avenue)

RECOMMENDATION:

THAT “Development Cost Charge (Parks) Reserve Fund Bylaw, 2020, No. 8809” (Purchase of Land for Park Purposes at 16th Street and St. Andrews Avenue) be given first, second and third readings.

REPORTS

15. 2020 Children and Youth Initiatives Fund – Special Project Grant Applications
– File: 05-1850-20-0003/2020

Report: Community Development Coordinator, November 24, 2020

RECOMMENDATION:

PURSUANT to the report of the Community Development Coordinator, dated November 24, 2020, entitled “2020 Children and Youth Initiatives Fund – Special Project Grant Applications”:

THAT the following Children and Youth Initiative Fund grant allocations totaling \$20,500 for 2020 be approved:

Girl Guides of Canada	
Cedar Shadows on the Move.....	\$1,000
Mountainside Secondary School	
Bicycle Mechanics.....	\$3,000
North Shore Crisis Services Society	
PEACE Camp.....	\$1,500
North Shore Neighbourhood House / Edible Garden Project	
Garden-Based Learning at Loutet Farm.....	\$3,000
North Shore Restorative Justice Society	
Youth Justice Lab Series.....	\$2,000
North Shore Women’s Centre	
Fearless: Girls’ Empowerment Camp.....	\$2,000
RC Life Skills Society	
Life Skills Boxing Program via Griffins Boxing & Fitness.....	\$1,500
St. Thomas Aquinas Regional High School	
BariSTA Café.....	\$2,000
Sutherland Secondary School	
Girls Group Glam (Bread and Roses).....	\$2,000
Washington Kids Foundation	
WKF Her Time.....	\$1,000
Jack.org	
Talk at the Top Jack Summit.....	\$1,500
TOTAL.....	\$20,500

THAT the following organizations be notified that their application for a 2020 Children and Youth Initiatives grant will not be funded:

Queen Mary Community Elementary
Developing Self-Advocacy Skills and Awareness for Safety
Sutherland Secondary School
Sutherland’s Got Talent

Continued...

REPORTS – Continued

15. 2020 Children and Youth Initiatives Fund – Special Project Grant Applications
– File: 05-1850-20-0003/2020 – Continued

THAT a copy of Council's resolution be sent to the District of North Vancouver and the North Vancouver School Board (District #44) for information;

THAT the Children and Youth Initiatives Grant Committee be thanked for their time and participation;

AND THAT staff be directed to review the Children and Youth Initiatives Fund and report back on opportunities to improve the efficiency and effectiveness of the granting process and adequacy of funding levels.

16. Spirit Trail Mosquito Creek Marina – Funding Reallocation
– File: 12-6130-20-0056/1

Report: Deputy Director, Engineering, Parks and Environment,
November 25, 2020

RECOMMENDATION:

PURSUANT to the report of the Deputy Director, Engineering, Parks and Environment, dated November 25, 2020, entitled "Spirit Trail Mosquito Creek Marina – Funding Reallocation":

THAT (Funding Appropriation #50154) an amount of \$67,371.93 be reallocated from the Green Necklace: West 21st Street-Jones project to the Mosquito Creek Marina Spirit Trail project;

AND THAT (Funding Appropriation #50155) an amount of \$51,531.30 be reallocated from the Green Necklace: Lonsdale to Grand Blvd/East 19th project to the Mosquito Creek Marina Spirit Trail project.

17. Mickey McDougall Feasibility Study – Extension of Funding Request
– File: 02-0800-30-0013/1

Report: Deputy Director, Strategic and Corporate Services, November 16, 2020

RECOMMENDATION:

PURSUANT to the report of the Deputy Director, Strategic and Corporate Services, dated November 16, 2020, entitled "Mickey McDougall Feasibility Study – Extension of Funding Request":

Continued...

REPORTS – Continued

17. Mickey McDougall Feasibility Study – Extension of Funding Request
– File: 02-0800-30-0013/1 – Continued

THAT (Funding Appropriation #1738) an amount of \$150,000, appropriated for Mickey McDougall Feasibility Study with Flicka Gymnastics, be extended by one year;

AND THAT should any of the amount remain unexpended as at December 31, 2021, the unexpended balance shall be returned to the credit of the Civic Amenity Reserve Fund.

COVID-19 UPDATE

COUNCIL INQUIRIES / REPORTS

NEW ITEMS OF BUSINESS

NOTICES OF MOTION

RECESS TO CLOSED SESSION

THAT Council recess to the Committee of the Whole (Closed Session) pursuant to the Community Charter, Sections 90(1)(a) [personal information] and 90(1)(c) [employee relations].

REPORT OF THE COMMITTEE OF THE WHOLE (CLOSED SESSION)

ADJOURN



THE CORPORATION OF THE CITY OF NORTH VANCOUVER

PUBLIC HEARING GUIDELINES FOR ELECTRONIC MEETINGS

Public Hearings are included as part of a Regular Council agenda and governed by the provisions of the *Local Government Act*.

A Public Hearing is held to allow the public an opportunity to make representations to Council – in person at the Public Hearing or by written submission – on a proposed amendment to the City's Official Community Plan and/or Zoning Bylaw. All persons who believe their interest in property is affected by a proposed bylaw(s) are afforded a reasonable opportunity to be heard, voice concerns or present written submissions regarding matters contained within the bylaw(s).

All written submissions and representations made at the Public Hearing form part of the official public record. Minutes of the Public Hearing and a video recording of the proceedings will be posted on the City's website at cnv.org.

All written submissions must include the person's name and address. If this information is not provided, it cannot be included as part of the public record. Electronic submissions are preferred, and hand-delivered or mailed submissions will also be accepted. The deadline to submit email submissions is 12:00 noon on the day of the Public Hearing. Due to COVID-19, safety quarantine restrictions have been put in place and the deadline for submissions by mail or delivery to City Hall is 4:00 pm on the Friday prior to the Monday Public Hearing (a minimum of one clear day prior to the Public Hearing).

If persons wish to speak at the Public Hearing, we ask that everyone pre-register to be placed on the speaker's list. The pre-registration form is available at cnv.org/PublicHearings, or speakers can pre-register by contacting the Corporate Officer's office. All pre-registrations must be submitted no later than 12:00 noon on the day of the Public Hearing, to allow City staff time to contact all participants and provide them with call-in/online access instructions.

Comments from the public must specifically relate to the proposed bylaw(s). Speakers are asked to avoid repetitive comments and not to divert to other matters.

Speakers will be asked to confirm their name and address for the record and will be provided one, 5-minute opportunity to present their comments. There will be no opportunity to speak a second time. After all persons who have pre-registered have spoken, the Mayor (Chair) will ask if anyone else from the public has new information to provide. Speakers who have not pre-registered will also have an opportunity to provide input at cnv.org/PublicHearings. Call-in details will be displayed on-screen at the Public Hearing (watch web livestream). Once all registered speakers have provided input, the Mayor will call for a recess to allow additional speakers time to phone in.

Continued...



THE CORPORATION OF THE CITY OF NORTH VANCOUVER

PUBLIC HEARING GUIDELINES FOR ELECTRONIC MEETINGS (continued)

Everyone will be given a reasonable opportunity to be heard and no one should feel discouraged or prevented from making their views known. The City asks for everyone's patience during the electronic Public Hearing.

Procedural rules for the conduct of the Public Hearing are set at the call of the Chair and Council's main function is to listen to the views of the public regarding the change of land use in the proposed bylaw(s). It is not the function of Council to debate the merits of an application with speakers. Questions from members of the public and Council must be addressed through the Chair.

Once the Public Hearing concludes, no further information or submissions can be considered by Council.

Following adjournment of the Public Hearing, the Regular meeting reconvenes and the Zoning and/or Official Community Plan bylaw amendment(s) are discussed and debated by members of Council, followed by consideration of third reading of the bylaw(s).

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**MINUTES OF THE REGULAR MEETING OF COUNCIL, HELD
ELECTRONICALLY FROM CITY HALL, 141 WEST 14TH STREET, NORTH
VANCOUVER, BC, ON **MONDAY, NOVEMBER 23, 2020****

PRESENT

COUNCIL MEMBERS

Mayor L. Buchanan
Councillor H. Back
Councillor D. Bell
Councillor A. Girard
Councillor T. Hu
Councillor J. McIlroy
Councillor T. Valente

STAFF MEMBERS

L. McCarthy, CAO
K. Graham, Corporate Officer
C. Baird, Deputy Corporate Officer
J. Peters, Assistant City Clerk
H. Granger, City Solicitor
B. Pearce, Director, Strategic and Corporate Services
B. Themens, Director, Finance
M. Epp, Director, Planning and Development
A. Devlin, Manager, Transportation Planning
Y. Zeng, Manager, Development Planning
R. de St. Croix, Manager, Long Range and Community Planning
E. Macdonald, Planner
H. Evans, Community Planner
K. Magnusson, Deputy Director, Engineering, Parks and Environment
R. Skene, Director, Community and Partner Engagement
K. Veng, CEO, Lonsdale Energy Corp.

Mayor Buchanan introduced Karsten Veng, the new Chief Executive Officer of Lonsdale Energy Corp.

The meeting was called to order at 5:30 pm.

APPROVAL OF AGENDA

Moved by Councillor Girard, seconded by Councillor Valente

1. Regular Council Meeting Agenda, November 23, 2020

CARRIED UNANIMOUSLY

ADOPTION OF MINUTES

Moved by Councillor Bell, seconded by Councillor McIlroy

2. Regular Council Meeting Minutes, November 16, 2020

CARRIED UNANIMOUSLY

PROCLAMATIONS

Mayor Buchanan declared the following proclamations:

International Day of Persons with Disabilities – December 3, 2020
– read by Councillor Bell

National Day of Remembrance and Action on Violence Against Women – December 6, 2020
– read by Councillor Girard

PUBLIC INPUT PERIOD

Nil.

CONSENT AGENDA

Moved by Councillor Bell, seconded by Councillor Girard

THAT the recommendations listed within the “Consent Agenda” be approved.

CARRIED UNANIMOUSLY

START OF CONSENT AGENDA

BYLAWS – ADOPTION

3. “Financial Plan for the Years 2020 to 2029 Bylaw, 2020, No. 8771, Amendment Bylaw, 2020, No. 8797” (Revised Financial Plan)

Moved by Councillor Bell, seconded by Councillor Girard

THAT “Financial Plan for the Years 2020 to 2029 Bylaw, 2020, No. 8771, Amendment Bylaw, 2020, No. 8797” (Revised Financial Plan) be adopted, signed by the Mayor and Corporate Officer and affixed with the corporate seal.

(CARRIED UNANIMOUSLY)

4. “Construction Regulation Bylaw, 2003, No. 7390, Amendment Bylaw, 2020, No. 8810” (Low Carbon Pathway Amendments Under the BC Energy Step Code)

Moved by Councillor Bell, seconded by Councillor Girard

THAT “Construction Regulation Bylaw, 2003, No. 7390, Amendment Bylaw, 2020, No. 8810” (Low Carbon Pathway Amendments Under the BC Energy Step Code) be adopted, signed by the Mayor and Corporate Officer and affixed with the corporate seal.

(CARRIED UNANIMOUSLY)

CONSENT AGENDA – Continued

REPORT

5. 2021 Schedule of Council Meetings – File: 01-0550-01-0001/2020

Report: Corporate Officer, November 12, 2020

Moved by Councillor Bell, seconded by Councillor Girard

PURSUANT to the report of the Corporate Officer, dated November 12, 2020, entitled “2021 Schedule of Council Meetings”:

THAT Council endorse the 2021 Schedule of Council Meetings.

(CARRIED UNANIMOUSLY)

END OF CONSENT AGENDA

PUBLIC HEARING – 200 West Esplanade

Moved by Councillor Valente, seconded by Councillor McIlroy

THAT the meeting recess to the Public Hearing regarding “Zoning Bylaw, 1995, No. 6700, Amendment Bylaw, 2020, No. 8798” (First Capital (200 West Esplanade) Corporation / Rafii Architects Inc., 200 West Esplanade, CD-729) and “Housing Agreement Bylaw, 2020, No. 8799” (First Capital (200 West Esplanade) Corporation, 200 West Esplanade, CD-729, Rental Housing Commitments).

CARRIED UNANIMOUSLY

The meeting recessed to the Public Hearing at 5:37 pm and reconvened at 7:04 pm.

BYLAWS – THIRD READING

6. “Zoning Bylaw, 1995, No. 6700, Amendment Bylaw, 2020, No. 8798” (First Capital (200 West Esplanade) Corporation / Rafii Architects Inc., 200 West Esplanade, CD-729)

Moved by Councillor Back, seconded by Councillor Girard

THAT “Zoning Bylaw, 1995, No. 6700, Amendment Bylaw, 2020, No. 8798” (First Capital (200 West Esplanade) Corporation / Rafii Architects Inc., 200 West Esplanade, CD-729) be given third reading.

CARRIED

Councillor Bell is recorded as voting contrary to the motion.

BYLAWS – THIRD READING – Continued

7. “Housing Agreement Bylaw, 2020, No. 8799” (First Capital (200 West Esplanade) Corporation, 200 West Esplanade, CD-729, Rental Housing Commitments)

Moved by Councillor Back, seconded by Councillor Girard

THAT “Housing Agreement Bylaw, 2020, No. 8799” (First Capital (200 West Esplanade) Corporation, 200 West Esplanade, CD-729, Rental Housing Commitments) be given third reading.

CARRIED

Councillor Bell is recorded as voting contrary to the motion.

8. “Zoning Bylaw, 1995, No. 6700, Amendment Bylaw, 2020, No. 8805” (518166 British Columbia Ltd., 801-925 Harbourside Drive and 18 Fell Avenue, CD-646 Text Amendment)

Moved by Councillor Bell, seconded by Councillor Girard

THAT “Zoning Bylaw, 1995, No. 6700, Amendment Bylaw, 2020, No. 8805” (518166 British Columbia Ltd., 801-925 Harbourside Drive and 18 Fell Avenue, CD-646 Text Amendment) be given third reading.

CARRIED UNANIMOUSLY

REPORTS

9. 2020 Round Two Community Grant Recommendations – File: 05-1850-20-0005/2020

Report: Chair, Social Planning Advisory Committee, and Community Planner,
November 10, 2020

Moved by Councillor Bell, seconded by Councillor Valente

PURSUANT to the report of the Chair, Social Planning Advisory Committee, and the Community Planner, dated November 10, 2020, entitled “2020 Round Two Community Grant Recommendations”:

THAT grants be allocated to the following organizations from the 2020 Community Grants budget:

Autism BC – Program – North Shore Information & Resources and Parent Community Group	\$5,000
Big Sisters of BC Lower Mainland – Operating	\$2,000
Greater Vancouver Youth Unlimited North Shore – Program – Mobile Youth Drop-In	\$6,000
Highlands United Church – Program – Shelter to Home	\$5,671

Continued...

REPORTS – Continued

9. 2020 Round Two Community Grant Recommendations – File: 05-1850-20-0005/2020
– Continued

Hollyburn Family Services – Program – Life Success	\$1,000
Hollyburn Family Services – Program – North Shore Youth Safe House	\$2,000
Hollyburn Family Services – Program – Seniors Safe House	\$4,000
Hollyburn Family Services – Program – Wired 4 Success	\$500
Hollyburn Family Services – Program – Youth Education Navigator	\$500
Living Systems Family Systems Counselling & Education Society – Program – Access Counselling	\$5,000
Muscular Dystrophy Canada – Program – North Vancouver Equipment Program	\$6,000
North Shore Meals on Wheels Society – Program – New Food Packaging Procedures Due to COVID-19	\$6,000
North Shore Stroke Recovery Centre – Program – Young Survivor of Stroke	\$2,500
North Shore Table Tennis Society – Operating	\$7,000
Lynn Valley Services Society – Program – Better Balance	\$500
TOTAL	\$53,671

AND THAT the following organizations be notified that their application for a 2020 Community Grant will not be funded:

North Shore Table Tennis Society – Program – Adapted Table Tennis Program for Persons with Disabilities
Alliance of British Columbia Students Society – Operating
The Harlequin Theatre Society – Operating
Keats Camps – Operating
Lynn Valley Services Society – Program – Power for Life

CARRIED UNANIMOUSLY

10. Water Utility Fund Transfer for Watermain Replacement Project at Gladstone Avenue and Cotton Road – File: 11-5430-01-0001/2020

Report: Section Manager, Engineering Design, November 10, 2020

Moved by Councillor Valente, seconded by Councillor Bell

PURSUANT to the report of the Section Manager, Engineering Design, dated November 10, 2020, entitled “Water Utility Fund Transfer for Watermain Replacement Project at Gladstone Avenue and Cotton Road”:

Continued...

REPORTS – Continued

10. Water Utility Fund Transfer for Watermain Replacement Project at Gladstone Avenue and Cotton Road – File: 11-5430-01-0001/2020 – Continued

THAT \$51,749.27 of capital funds be transferred from Project 56075 to Project 56086 to fund the replacement of the watermain in the 300 Block of Gladstone Avenue, from Cotton Road to 4th Street;

AND THAT \$1,632.10 be transferred from Project 56100 to Project 56086 to fund the replacement of the watermain in the 300 Block of Gladstone Avenue, from Cotton Road to 4th Street.

CARRIED UNANIMOUSLY

11. North Shore Homelessness Action Project: Terms of Reference
– File: 10-5040-04-0001/2020

Report: Manager, Long Range and Community Planning, November 10, 2020

Moved by Councillor Girard, seconded by Councillor Hu

PURSUANT to the report of the Manager, Long Range and Community Planning, dated November 10, 2020, entitled “North Shore Homelessness Action Project: Terms of Reference”:

THAT the North Shore Homelessness Action Project Terms of Reference be endorsed;

AND THAT a formal invitation be extended to the Steering Committee and Staff Working Group of the Squamish and Tsleil-Waututh Nations.

CARRIED UNANIMOUSLY

12. Proposed Bylaw Amendments to Enable E-Bike Share Permit
– File: 16-8330-11-0002/1

Report: Sustainable Transportation Coordinator and North Shore Mobility Options Coordinator, November 10, 2020

Moved by Councillor Girard, seconded by Councillor Valente

PURSUANT to the report of the Sustainable Transportation Coordinator and the North Shore Mobility Options Coordinator, dated November 10, 2020, entitled “Proposed Bylaw Amendments to Enable E-Bike Share Permit”:

THAT the following bylaws be considered:

- “Street and Traffic Bylaw, 1991, No. 6234, Amendment Bylaw, 2020, No. 8791” (E-Bike Share Service Permit)

Continued...

REPORTS – Continued

12. Proposed Bylaw Amendments to Enable E-Bike Share Permit
– File: 16-8330-11-0002/1 – Continued

with the following amendments:

- Under Item 2C.4, in the section heading, replace the words “Shared Mobility Zone” with “E-Bike Share Zone”;
- Under Item 2D, in the last paragraph, replace the words “Shared Mobility Zone” with “E-Bike Share Zone”; and
- Under Item 2G.5, replace the words “Shared Micromobility Permit” with “E-Bike Share Permit”;

and

- “Bylaw Notice Enforcement Bylaw, 2018, No. 8675, Amendment Bylaw, 2020, No. 8792” (E-Bike Share Permit Enforcement).

CARRIED UNANIMOUSLY

BYLAWS – FIRST, SECOND AND THIRD READINGS

13. “Street and Traffic Bylaw, 1991, No. 6234, Amendment Bylaw, 2020, No. 8791”
(E-Bike Share Service Permit)

Moved by Councillor Girard, seconded by Councillor Valente

THAT “Street and Traffic Bylaw, 1991, No. 6234, Amendment Bylaw, 2020, No. 8791” (E-Bike Share Service Permit) be given first and second readings.

CARRIED UNANIMOUSLY

Moved by Councillor Girard, seconded by Councillor Valente

THAT “Street and Traffic Bylaw, 1991, No. 6234, Amendment Bylaw, 2020, No. 8791” (E-Bike Share Service Permit) be given third reading.

CARRIED UNANIMOUSLY

14. “Bylaw Notice Enforcement Bylaw, 2018, No. 8675, Amendment Bylaw, 2020, No. 8792” (E-Bike Share Permit Enforcement)

Moved by Councillor Girard, seconded by Councillor Valente

THAT “Bylaw Notice Enforcement Bylaw, 2018, No. 8675, Amendment Bylaw, 2020, No. 8792” (E-Bike Share Permit Enforcement) be given first and second readings.

CARRIED UNANIMOUSLY

BYLAWS – FIRST, SECOND AND THIRD READINGS – Continued

14. “Bylaw Notice Enforcement Bylaw, 2018, No. 8675, Amendment Bylaw, 2020, No. 8792” (E-Bike Share Permit Enforcement) – Continued

Moved by Councillor Girard, seconded by Councillor Valente

THAT “Bylaw Notice Enforcement Bylaw, 2018, No. 8675, Amendment Bylaw, 2020, No. 8792” (E-Bike Share Permit Enforcement) be given third reading.

CARRIED UNANIMOUSLY

REPORT

15. Preliminary Report: OCP Amendment and Zoning Bylaw Text Amendment for 123-145 East 13th Street (Millennium Northmount) – File: 08-3360-20-0405/1

Report: Planner 1, November 10, 2020

Moved by Councillor McIlroy, seconded by Councillor Valente

PURSUANT to the report of the Planner 1, dated November 10, 2020, entitled “Preliminary Report: OCP Amendment and Zoning Bylaw Text Amendment for 123-145 East 13th Street (Millennium Northmount)”:

THAT the application for Official Community Plan and Zoning Bylaw amendments for a 7-storey height increase and additional 1.4 FSR be rejected;

THAT staff receive an application for a 1.7 metre increase in height (Option B - minor increase in height for improved commercial levels) and other variances (“Additional Amendments”);

AND THAT staff undertake a streamlined Official Community Plan and Zoning Bylaw amendment process.

CARRIED UNANIMOUSLY

COVID-19 UPDATE

Nil.

COUNCIL INQUIRIES / REPORTS

Nil.

NEW ITEMS OF BUSINESS

Nil.

NOTICES OF MOTION

Nil.

RECESS TO CLOSED SESSION

Moved by Councillor Girard, seconded by Councillor Valente

THAT Council recess to the Committee of the Whole, Closed session, pursuant to the *Community Charter*, Sections 90(1)(a) [personal information] and 90(1)(e) [land matter].

The meeting recessed at 8:25 pm and reconvened at 9:32 pm.

REPORT OF THE COMMITTEE OF THE WHOLE (CLOSED SESSION)

16. Appointments to the North Vancouver City Library Board
– File: 01-3060-20-0007/2020

Report: Corporate Officer, November 10, 2020

Moved by Councillor Girard, seconded by Councillor Back

PURSUANT to the report of the Corporate Officer, dated November 10, 2020, entitled “Appointments to the North Vancouver City Library Board”:

THAT Leane Neufeld and Gillian Quigley be reappointed to the North Vancouver City Library Board for a 2-year term concluding December 31, 2022;

THAT Brendan Harvey be appointed to the North Vancouver City Library Board for a 2-year term concluding December 31, 2022;

AND THAT the report of the Corporate Officer, dated November 10, 2020, entitled “Appointments to the North Vancouver City Library Board”, remain in the Closed session.

CARRIED UNANIMOUSLY

17. Appointment to the North Vancouver Recreation and Culture Commission
– File: 01-0360-20-0074/2020

Report: Corporate Officer, November 10, 2020

Moved by Councillor Girard, seconded by Councillor Back

PURSUANT to the report of the Corporate Officer, dated November 10, 2020, entitled “Appointment to the North Vancouver Recreation and Culture Commission”:

THAT Kevin Han be appointed to the North Vancouver Recreation and Culture Commission for a 3-year term concluding November 30, 2023;

AND THAT the report of the Corporate Officer, dated November 10, 2020, entitled “Appointment to the North Vancouver Recreation and Culture Commission”, remain in the Closed session.

CARRIED UNANIMOUSLY

REPORT OF THE COMMITTEE OF THE WHOLE (CLOSED SESSION) – Continued

18. Land Matter – File: 02-0890-20-0071/1

Report: Director, Planning and Development, and Director, Strategic and Corporate Services, November 4, 2020

Moved by Councillor Back, seconded by Councillor Hu

PURSUANT to the report of the Director, Planning and Development, and the Director, Strategic and Corporate Services, dated November 4, 2020, regarding a land matter:

THAT the action taken by the Committee of the Whole, Closed Session, be ratified;

AND THAT the wording of the recommendation and the report of the Director, Planning and Development, and the Director, Strategic and Corporate Services, dated November 4, 2020, remain in the Closed session.

CARRIED UNANIMOUSLY

19. Land Matter – File: 10-5040-03-0001/2020

Report: Manager, Long Range and Community Planning, November 20, 2020

Moved by Councillor McIlroy, seconded by Councillor Bell

PURSUANT to the report of the Manager, Long Range and Community Planning, dated November 20, 2020, regarding a land matter:

THAT the action taken by the Committee of the Whole, Closed Session, be ratified;

AND THAT the wording of the recommendation and the report of the Manager, Long Range and Community Planning, dated November 20, 2020, remain in the Closed session.

CARRIED UNANIMOUSLY

ADJOURN

Moved by Councillor Back seconded by Councillor Girard

THAT the meeting adjourn.

CARRIED UNANIMOUSLY

The meeting adjourned at 9:33 pm.

“Certified Correct by the Corporate Officer”

CORPORATE OFFICER

THE CORPORATION OF THE CITY OF NORTH VANCOUVER

BYLAW NO. 8791

A Bylaw to amend “Street and Traffic Bylaw, 1991, No. 6234”

The Council of The Corporation of the City of North Vancouver, in open meeting assembled, enacts as follows:

1. This Bylaw shall be known and cited for all purposes as “**Street and Traffic Bylaw, 1991, No. 6234, Amendment Bylaw, 2020, No. 8791**” (E-Bike Share Service Permit).

2. “Street and Traffic Bylaw, 1991, No. 6234” is amended as follows:

- A. In Part 3 – Definitions, by adding the following definitions to section 302:

“E-Bike Share Permit” means a permit issued by the City Engineer under section 520 of this Bylaw.

“E-Bike Share Service” means a legal entity whose business is to provide access to a fleet of Motor Assisted Cycles for a fee.

“E-Bike Share Zone” means the area or space on a roadway designated by a Traffic Control Device and established for the exclusive use of a specified E-Bike Share Service.

“Motor Assisted Cycle” or “E-Bike” means a type of Cycle:

- (a) with two or three wheels to which pedals or hand cranks are attached that will allow for the Cycle to be propelled by human power;
- (b) on which a person may ride;
- (c) to which is attached an electric motor that has an output not exceeding 500 W; and,
- (d) that meets the other criteria prescribed under the *Motor Vehicle Act*, the Motor Vehicle Act Regulations, and the Motor Assisted Cycle Regulation.

“Motor Vehicle Act Regulations” means the *Motor Vehicle Act Regulations*, B.C. Reg. 26/58.

- B. In section 508 Power to Establish Restrictive Parking Zones, by deleting:

“The City Engineer is hereby authorized to place or erect or designate or cause to be placed or erected or designated a Traffic Control Device establishing:

- (a) a Loading Zone, Commercial Loading Zone, Passenger Zone, Bus Zone, Taxi Zone, Film Zone, Disability Zone, Building Zone, Fire Zone, Time- Limited Zone, and Shared Vehicle Zone as he deems desirable;”

and replacing with the following:

“The City Engineer is hereby authorized to place or erect or designate or cause to be placed or erected or designated a Traffic Control Device establishing:

- (a) a Loading Zone, Commercial Loading Zone, Passenger Zone, Bus Zone, Taxi Zone, Film Zone, Disability Zone, Building Zone, Fire Zone, Time- Limited Zone, Shared Vehicle Zone, and E-Bike Share Zone as the City Engineer deems desirable;”
- C. In section 508 Power to Establish Restrictive Parking Zones, by adding section 508.4 after 508.3 as follows:
 - “.4 Power to Charge E-Bike Share Services for the Establishment of an E-Bike Share Zone

If the City Engineer places or erects or causes to be placed or erected a Traffic Control Device establishing an E-Bike Share Zone, the City Engineer may charge the E-Bike Share Service for which the E-Bike Share Zone was established, a fee for the placement or erection of the Traffic Control Device. If the City Engineer places or erects or causes to be placed or erected a Traffic Control Device establishing an E-Bike Share Zone in a Metered Zone, the City Engineer may charge the E-Bike Share Service for which the zone-Bike Share Zone was established an annual fee equal to the meter revenue that would have been collected by the City had that area not been established as an E-Bike Share Zone.”
- D. In section 509 Parking Zone Restrictions, by deleting:

“No person shall stop or park a vehicle in any loading zone, commercial loading zone, passenger zone, bus zone, taxi zone, building zone, film zone, disabled zone, fire zone, shared vehicle zone, resident permit only zone and special parking permit zone except that a person may do so:”

and replacing with the following:

“No person shall stop or park a vehicle in any Loading zone, Commercial Loading Zone, Passenger Zone, Bus Zone, Taxi Zone, Building Zone, Film Zone, Disabled Zone, Fire Zone, Shared Vehicle Zone, E-Bike Share Zone, Resident Permit Only Zone, and Special Parking Permit Zone except that a person may do so.”
- E. In section 509 Parking Zone Restrictions, by adding section 509.14 after 509.13 as follows:
 - “.14 in an E-Bike Share Zone with an E-Bike belonging to an E-Bike Share Service assigned to that E-Bike Share Zone.”
- F. In Section 813 Obstructions – Removal, by deleting subsection 813.3 and replacing with the following:
 - “.3 The City Engineer and any person designated as a Bylaw Enforcement Officer pursuant to the “Bylaw Notice Enforcement Bylaw, 2005, No. 7675” may remove, detain or impound or cause to be removed, detained, or impounded such goods chattels, motor-vehicles, Cycles, and E-Bikes or other thing which unlawfully has been placed or maintained or permitted to remain or used in any way in contravention of this Bylaw upon any highway and the cost of removing, towing, impounding and storing the same shall be charged to the owner thereof or the

person placing, maintaining or permitting to remain or using in any way in contravention of this Bylaw the said goods, chattels, motor vehicles, Cycles, E-Bikes, or other things.”

- G. By adding section 822 E-Bike Share Services after section 821 Utility Access Agreement as follows:

“822 E-Bike Share Services

- .1 The City Engineer may establish and designate Geo-fenced Areas and regulate and control the operation of E-Bike Share Services within any Geo-fenced Area, including the speed of E-Bikes and the regulation or prohibition of parking of any E-Bikes within a Geo-fenced Area.
- .2 No person or legal entity may operate an E-Bike Share Service without a valid E-Bike Share Permit.
- .3 The holder of an E-Bike Share Permit may deploy a fleet of E-Bikes in any location where parking is permitted for the purpose of making E-Bikes available to reserve for use.
- .4 The City Engineer may issue an E-Bike Share Permit to an E-Bike Share Service for the fee set out in Schedule F to this Bylaw, as amended from time to time, which fee may vary based on the total number of E-Bikes deployed in a fleet by the E-Bike Share Service and which fee shall be payable prior to being issued a permit in a manner satisfactory to the City Engineer.
- .5 Successful applicants for the E-Bike Share Permit must pay a performance deposit set out in Schedule F to this Bylaw, as amended from time to time, prior to being issued a permit. The performance deposit fee is refundable, less any deductions.”

- H. In Schedule “D” – Table of Authority for the City Engineer and Their Duly Authorized Representatives, by adding a new line after “819 Tag Days” and inserting the following authority for “822 E-Bike Share Permit”:

Section No.	Section	City Engineer	Deputy City Engineer	Assistant City Engineer	Traffic Engineer	Superintendents	Engineering Staff	Corporate Officer	Enforcement Officer
822	E-Bike Share Permit	X	X				X		

- I. In Schedule “F” — Fees, by adding a new line after “Congestion and Curbside Management” and inserting the following fees for “E-Bike Share Permit—Permit for Shared E-Bikes” and “E-Bike Share Permit—Security for Performance”:

E-Bike Share Permit – Permit for Shared E-Bikes (Total fleet deployed)	s.822.3	\$250.00	\$40 per E-Bike deployed
E-Bike Share Permit – Security for Performance	s.822.4	N/A	\$10,000.00

READ a first time on the 23rd day of November, 2020.

READ a second time on the 23rd day of November, 2020.

READ a third time on the 23rd day of November, 2020.

ADOPTED on the <> day of <>, 2020.

MAYOR

CORPORATE OFFICER

THE CORPORATION OF THE CITY OF NORTH VANCOUVER

BYLAW NO. 8792

A Bylaw to amend “Bylaw Notice Enforcement Bylaw, 2018, No. 8675”

The Council of The Corporation of the City of North Vancouver, in open meeting assembled, enacts as follows:

1. This Bylaw shall be known and cited for all purposes as **“Bylaw Notice Enforcement Bylaw, 2018, No. 8675, Amendment Bylaw, 2020, No. 8792” (E-Bike Share Permit Enforcement)**.
2. “Bylaw Notice Enforcement Bylaw, 2018, No. 8675” is amended as follows:
 - A. Schedule “A” – Designated bylaw contraventions & penalties for bylaw contraventions is amended by adding the following new lines after “Street & Traffic No. 6234, Improper Use of a Special Parking Permit Zone, Contravene Street Use Permit Conditions, 1501” and inserting the following penalties relating to “E-Bike Share”:

Bylaw	Description	Section	A1 Compliance Agreement Available	A2 Penalty	A3 Early Payment Penalty	A4 Late Payment Penalty	A5 Compliance Agreement Discount
Street & Traffic No. 6234	Improper Use of an E-Bike Share Zone	509.14	No	\$50	\$40	\$80	N/A
Street & Traffic No. 6234	E-Bike Share Permit Infraction	822	No	\$50	\$40	\$80	N/A

READ a first time on the 23rd day of November, 2020.

READ a second time on the 23rd day of November, 2020.

READ a third time on the 23rd day of November, 2020.

ADOPTED on the <> day of <>, 2020.

MAYOR

CORPORATE OFFICER

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THE CORPORATION OF THE CITY OF NORTH VANCOUVER

BYLAW NO. 8700

A Bylaw to amend “Zoning Bylaw, 1995, No. 6700”

The Council of The Corporation of the City of North Vancouver, in open meeting assembled, enacts as follows:

1. This Bylaw shall be known and cited for all purposes as “**Zoning Bylaw, 1995, No. 6700, Amendment Bylaw, 2020, No. 8700**” (Bryan Lockhart / Black Kettle Brewing Inc., 720 Copping Street, CD-728).
2. Division VI: Zoning Map of Document “A” of “Zoning Bylaw, 1995, No. 6700” is hereby amended by reclassifying the following lots as henceforth being transferred, added to and forming part of CD-728 (Comprehensive Development 728 Zone):

Lots	D.L.	Plan	
A	265 AND 2654	16779	from W-1

3. Part 11 of Division V: Comprehensive Development Regulations of Document “A” of “Zoning Bylaw, 1995, No. 6700” is hereby amended by:

- A. Adding the following section to Section 1100, thereof, after the designation “CD-727 Comprehensive Development 727 Zone”:

“CD-728 Comprehensive Development 728 Zone”

- B. Adding the following to Section 1101, thereof, after the “CD-727 Comprehensive Development 727 Zone”:

“CD-728 Comprehensive Development 728 Zone”

In the CD-728 Zone, permitted Uses, regulations for permitted Uses, regulations for the size, shape and siting of Buildings and Structures and required Off-Street Parking shall be as in the W-1 Zone, except that:

- (1) Permitted Accessory Uses shall include, in addition to the uses permitted in the W-1 zone:

- (a) Accessory Lounge Use, subject to Section 702(7), except that:

- i. together with Accessory Retail Service Use and Accessory Manufacturing Retail Use, shall not occupy a combined area of more than 30% of the Gross Floor Area of the Principal Industrial Use up to a maximum of 157.6 square metres (1696.4 square feet).

(b) Accessory Manufacturing Retail Use, subject to 702(4), except that:

- i. together with Accessory Retail Service Use and Accessory Lounge Use, shall not occupy a combined area of more than 30% of the Gross Floor Area of the Principal Industrial Use.

READ a first time on the 19th day of October, 2020.

READ a second time on the 19th day of October, 2020.

READ a third time on the 16th day of November, 2020.

ADOPTED on the <> day of <>, 2020.

MAYOR

CORPORATE OFFICER



 Department Manager	 Director	 CAO
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The Corporation of **THE CITY OF NORTH VANCOUVER**
PLANNING & DEVELOPMENT DEPARTMENT

REPORT

To: Mayor Linda Buchanan and Members of Council

From: David Johnson, Development Planner

Subject: REZONING APPLICATION: 115 EAST 1ST STREET (EGGS CANA / OANA NICOARA)

Date: November 4, 2020 File No: 08-3400-20-0027/1

The following is a suggested recommendation only. Refer to Council Minutes for adopted resolution.

RECOMMENDATION

PURSUANT to the report of the Development Planner, dated November 4, 2020, entitled "Rezoning Application: 115 East 1st Street (Eggs Cana / Oana Nicoara)":

THAT "Zoning Bylaw, 1995, No. 6700, Amendment Bylaw, 2020, No. 8808" (Eggs Cana / Oana Nicoara, 115 East 1st Street, CD-731) be considered and referred to a Public Hearing;

AND THAT notification be circulated in accordance with the *Local Government Act*.

ATTACHMENTS

1. Context Map (CityDocs# [1952300](#))
2. Submitted Drawings (CityDocs# [1925038](#))
3. Business Plan and Community Impact Statement (CityDocs# [1925039](#))
4. Security Plan (CityDocs# [1925043](#))
5. Change to Provincial Cannabis Licensing Regulation (CityDocs# [1988760](#))
6. Recreational Cannabis Retail Policy (CityDocs# [1774193](#))
7. Public Consultation Summary (CityDocs# [1982452](#))
8. "Zoning Bylaw, 1995, No. 6700, Amendment Bylaw, 2020, No. 8808" (CityDocs# [1952293](#))

PURPOSE

The purpose of this report is to present, for Council consideration, a rezoning application for a recreational cannabis retail store at 115 East 1st Street and options for Council's consideration.

BACKGROUND

With the legalization of recreational cannabis on October 17, 2018, the federal government, under the *Cannabis Act*, provided the legal framework for the production, distribution, sale and possession of both medical and non-medical (recreational) cannabis. The Province of British Columbia created a legislative framework through the Cannabis Control and Licensing Act to supplement federal legislation and facilitate legal and controlled access to recreational cannabis in the province.

In coordination with both federal and provincial regulations, Council approved the Recreational Cannabis Retail Policy on September 17, 2018, which created the framework under which recreational cannabis retail businesses can be established in the City (Attachment #6). The policy considers the sale of non-medical grade cannabis and does not consider the production, distribution or sale of medicinal cannabis. Medicinal cannabis can be obtained through a federally licensed producer.

The policy focused on the location criteria for where businesses can be located in the City, as well as establishing an application procedure due to the anticipated high level of interest. The criteria included limiting Cannabis Retail Stores to properties with the following Official Community Plan designations:

- Mixed Use Level 2 (Medium Density);
- Mixed Use Level 3 (Medium Density);
- Mixed Use Level 4A (High Density);
- Mixed Use Level 4B (High Density)
- Harbourside Waterfront (Mixed Use); and
- Commercial.

The policy also limits locations that are within 100 metres of the following sensitive uses:

- Community Recreational Centres;
- North Shore Neighbourhood House;
- North Shore Shelter;
- North Vancouver school District Office; and
- Public elementary and secondary schools.

The policy includes a maximum of six (6) retail cannabis businesses to be considered, and prescribes a distribution throughout the City as a maximum of:

- Two businesses in the Central Lonsdale area;
- Two in the Lower Lonsdale area;
- One in the west part of the City; and
- One to the east part of the City.

To date, four of the six retail cannabis spots have been filled with the first two at 221 West 1st Street (Lower Lonsdale) and 333 Brooksbank Avenue (Park and Tilford – east part of the City) getting Council approval in July 2019, another two at 820 Marine Drive (west part of the City) and 1433 Lonsdale Avenue (Central Lonsdale) getting approval in February 2020. To date, only 221 West 1st Street and 820 Marine Drive locations have opened for business.

1433 Lonsdale Avenue is expected to open soon, and a Building permit application for the new building at 333 Brooksbank Avenue is expected shortly.

Those that were denied rezoning of the property to allow Cannabis Sales use include the following:

- 1717 Lonsdale Avenue (Central Lonsdale);
- 1200 Lonsdale Avenue (Central Lonsdale);
- 315 Lonsdale Avenue (Lower Lonsdale);
- 143 East 2nd Street (Lower Lonsdale); and
- 725 West 14th Street (West).

This report is for a potential store in the Lower Lonsdale area that was the next location on the list to make a rezoning application. Should Council decide to approve the application, the two Lower Lonsdale locations would be fulfilled, leaving only Central Lonsdale with one available business spot to consider a cannabis retail store. Under the Recreational Cannabis Retail Policy, the City is accepting rezoning applications for Central and Lower Lonsdale areas on a first-come-first-served basis, with each application to be evaluated through the rezoning process and presented for Council's consideration.

DISCUSSION

Project Description

The building on the subject site is a three-level mixed-use building that was built in 1911 and has two commercial units on the ground floor, which face East 1st Street, and 18 residential rental apartments above. On March 30, 2020, Council approved a development application for this site, granting the building protected heritage status as well as approving two additional commercial spaces in the basement of the building with access off of the rear lane. The application also granted variances to the existing building that would bring any non-conforming Zoning Bylaw items into conformance in exchange for the heritage designation. The ability for awnings to encroach onto City road allowances were also approved by Council as part of the heritage designation. The location for this application is the east unit on the ground floor fronting East 1st Street.

Pursuant to the application requirements of the City's Recreational Cannabis Retail Policy, the applicant has submitted a Community Impact Statement (Attachment #3), and their Security Plan (Attachment #4), which is intended to describe the positive economic, social and environmental impacts the business may have on the immediate neighbourhood and broader community. It also describes strategies for mitigating potential negative impacts, as well as outlining their security measures to meet Provincial regulations. The submitted Community Impact Statement is attached for Council's information, noting that the statement does not create an agreement between the City and the applicant that binds either party to carry out the proposed mitigation strategies. An agreement with the applicant will be made through a Community Good Neighbour Agreement at the time of the Business License application that would outline such mitigation measures to the immediate area.

Site Context and Surrounding Use

The subject site at 115 East 1st Street is along the south side of the street, between Lonsdale Avenue to the west and St. Georges Avenue to the east (Attachment #1). It is adjacent to other older one to three storey buildings, and across from the old post office building and a two storey building, as well as a parking lot and open space to the east. The immediate area is predominately older commercial and mixed-use buildings with newer mixed-use buildings to the south. The location is near the Lonsdale Avenue public transit corridor, and has several other commercial businesses within easy walking distance.

PLANNING ANALYSIS

Policy Context

2014 Official Community Plan

The subject site has a designated land use of Mixed-Use Level 4A which supports a mix of higher density multi-family and commercial uses in Lower Lonsdale. The proposed location is suited to this designation and no amendment to the OCP is required.

Since the last Zoning Bylaw amendment approvals for cannabis retail sales in February, the Province announced changes to the Cannabis Licencing Regulations to allow greater street front transparency by no longer requiring stores to screen the front of their stores from the street (Attachment #5). The new regulations will still not allow cannabis packaging and accessory products from being viewed from the outside, but viewing into the store from the outside will be possible as well as viewing onto the street from inside the store.

Zoning Bylaw 1995, No. 6700

The Zoning Bylaw currently does not allow Cannabis Sales as a permitted use anywhere in the City, but the Recreational Cannabis Retail Policy sets the framework where it can be permitted through the rezoning of the property. Cannabis Sales is defined in the Zoning Bylaw as follows:

means the retail or wholesale sale of Cannabis, and includes an operation which provides referrals and facilitates access to Cannabis not physically sold on the premises, but does not include sales by a British Columbia Registered Pharmacist In a British Columbia regulated Pharmacy.

Zoning Amendment Bylaw 8808 (Attachment #8) would allow Cannabis Sales in this location. The current Zone of Lower Lonsdale 2 (LL-2) does permit retail stores.

On March 30, 2020, Council adopted a Heritage Revitalization Agreement bylaw for the building that included specific variances to the existing building that otherwise did not conform to the LL-2 Zone, in exchange for Heritage Designation. One of these was to support zero off-street parking for the site as the size of the building has never provided enough space for any off-street parking. Council also permitted the signing of an Encroachment Agreement to allow limits of such things as awnings to encroach onto City road allowances. Should Council approve this application, a separate application for signage for the store is required and the review will take this agreement into account.

COMMUNITY CONSULTATION

A virtual Developer Information Session (DIS) was held on October 1, 2020 for members of the public to learn about and make comment on this application. Notifications for the DIS were conducted accordingly, and no one signed into the event. A summary of the consultation is provided in the applicant's report (Attachment #7).

As part of the applicant's Business Plan and Community Impact Statement (Attachment #3), the applicant outlines their social responsibilities to the local community. These include descriptions on how they will manage the surrounding area by enforcing non-smoking zones, litter pick-up, nuisance and noise mitigation, odour, and introducing community programs and open houses.

RECREATIONAL CANNABIS RETAIL POLICY

When Council approved the Recreational Cannabis Retail Policy on September 24, 2018, it stated that staff would monitor businesses and keep Council informed on the state of recreational cannabis retail in the City. The direction was that after a two-year period, staff would reassess the policy to determine whether any amendments would be warranted. Two options for proceeding are presented below:

- 1. Proceed with monitoring and policy review and report back to Council (Recommended)**

This option would allow staff to monitor and assess the approved cannabis stores as they begin operation and provide Council with a policy review in the first half of 2021. In the interim, staff would continue processing rezoning applications on a case by case basis. Should Council wish to pursue this option, no further resolution is required.

- 2. Direct staff to modify the Recreational Cannabis Retail policy per Council's direction**

This option would direct changes to the existing policy at Council's discretion. For example, Council may wish to limit the maximum number of cannabis retail stores further than currently suggested by the policy.

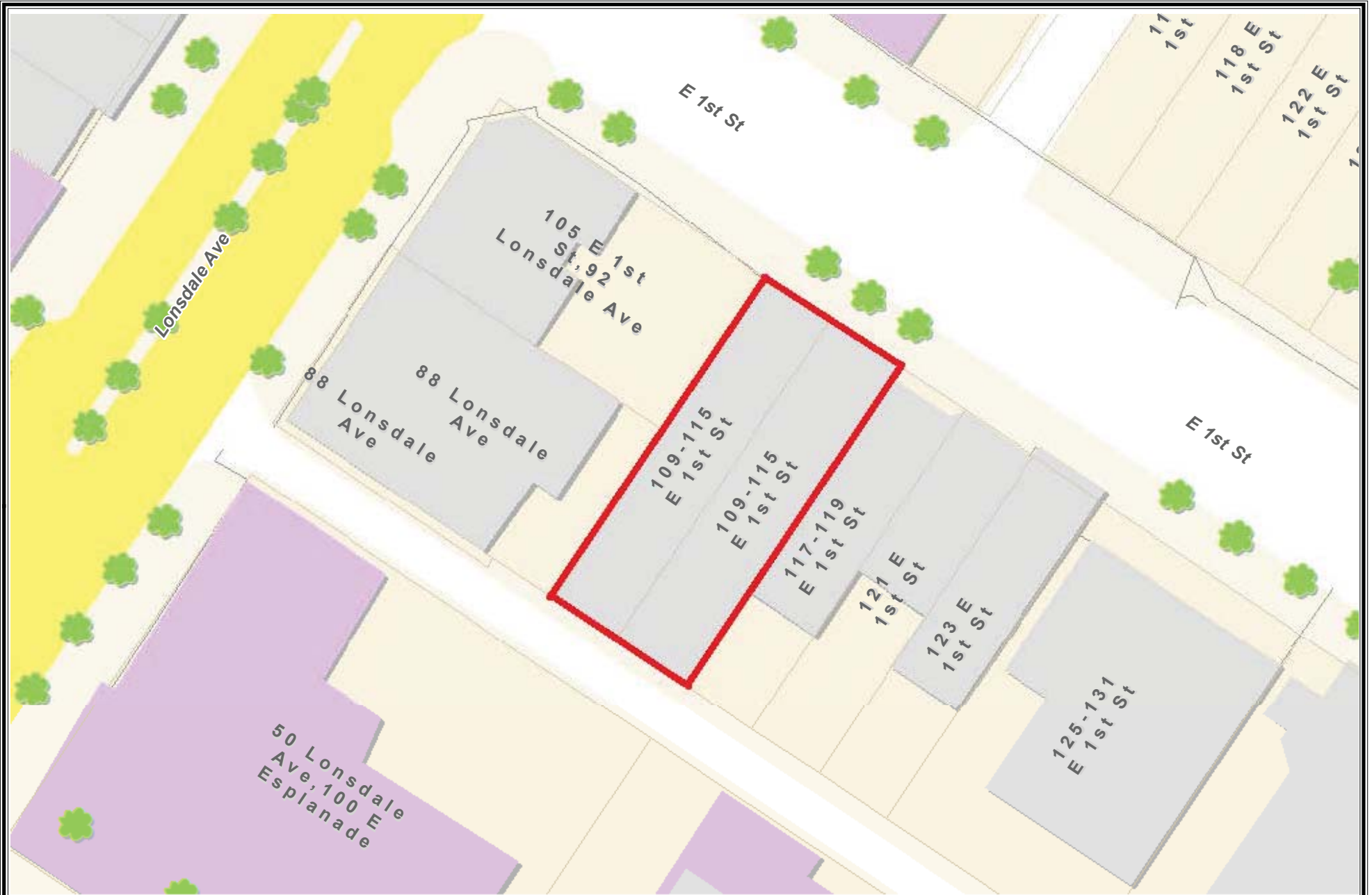
CONCLUSION

The proposal is consistent with the Official Community Plan and the criteria of the Recreational Cannabis Retail Policy. The proposed location of the store supports the commercial and retail orientation of the area.

RESPECTFULLY SUBMITTED:



David Johnson
Development Planner





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All dimensions shall be verified on site by contractor before proceeding with work.

Supersedes all prints bearing previous revision number.

EGGS CANNA

DEVELOPMENT PERMIT

115 East 1st Street
North Vancouver, BC
V7L 1B2

PROJECT DIRECTORY

OWNER
EGGS CANNA
2303 EAST HASTINGS STREET
VANCOUVER, BC CANADA, V6L 1V6
778.375-4344
ATTN: CANA NICORA

INTERIOR DESIGNER
SSDG INTERIORS INC.
606 GRANVILLE ST. SUITE 310
VANCOUVER, BC CANADA, V7Y 1G5
604.685.4845
ATTN: JASLEY KIM, LEANNA CHEUNG

SYMBOL LEGEND

ELEVATION REFERENCE		WALL TYPE	
ELEVATION REFERENCE		DOOR TYPE	
SECTION REFERENCE		DOOR NUMBER	
DETAIL NAME		NOTE NUMBER	
ELEVATION: SCALE 1/8"=1'-0"		MILLWORK NUMBER	
		FIXTURES	
		MISCELLANEOUS SPECIALTIES	
BREAK LINE		FINISH TAG	
BREAK ARROW		CEILING TAG	
CENTER LINE		FURNITURE NUMBER	
		REVISION NO.	
		CARD READER	
		SECURITY KEY PAD	

SHEET INDEX

ID0.1 COVER SHEET
ID1.0 FURNITURE PLAN
ID1.1 EXTERIOR STOREFRONT ELEVATION

04		
03		
02		
01	SUBJECT FOR DEVELOPMENT	20.06.10
NO.	REVISIONS	DATE

SSDG
INTERIORS INC.

SSDG Interiors Inc.
310 - 609 Granville Street Vancouver, BC Canada V7Y 1G5
O 604.685.4301 www.ssdg.com

Job Title:

EGGS CANNA
115 EAST 1ST ST
NORTH VANCOUVER, BC
V7L 1B2, CANADA

Drawing Title:

COVER SHEET

Project Date: 20.06.10
Drawn by: SM Checked by: RB
Project No: 2016 180 Scale: 1/8" = 1'-0" (24"X36")
Drawing No:

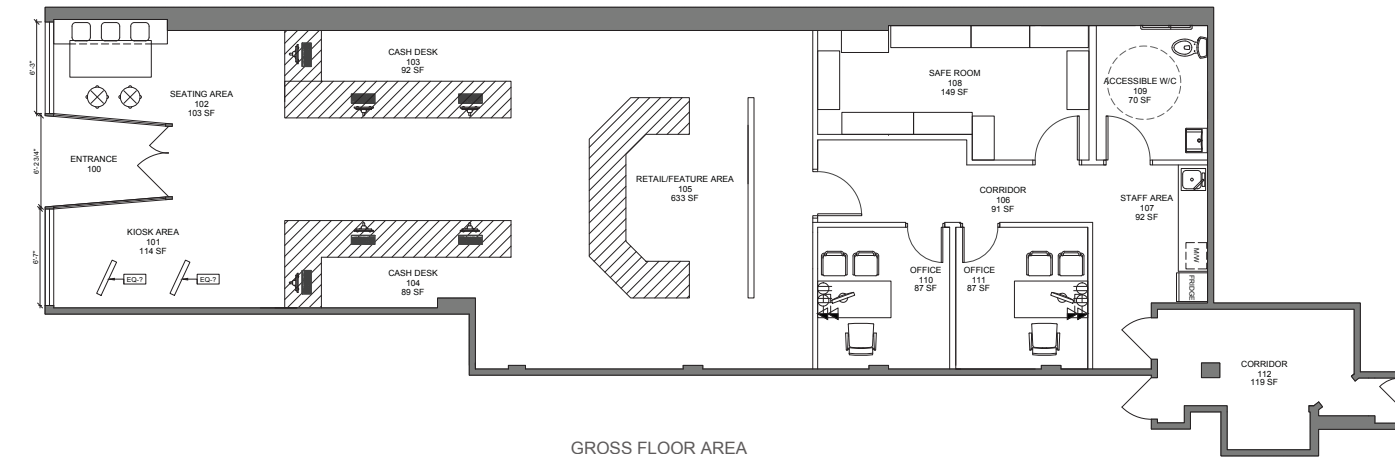
ID0.1



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All dimensions shall be verified on site by contractor before proceeding with work.

Supersedes all prints bearing previous revision number.



GROSS FLOOR AREA
1762 SF

01 FURNITURE PLAN
15.17 SCALE: 1/8" = 1'-0"

04		
03		
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01	SUBJECT FOR DEVELOPMENT	20.06.10
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Job Title:

EGGS CANNA
115 EAST 1ST ST
NORTH VANCOUVER, BC
V7L 1B2, CANADA

Drawing Title:

SPACE PLAN

Project Date: 20.06.10

Drawn by: SM Checked by: RB

Project No: 2016 180 Scale: 1/8" = 1'-0" (24"X36")

Drawing No:

ID5.1



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01 ELEVATION
SCALE 1/4\"/>

04		
03		
02		
01	ISSUED FOR DEVELOPMENT	20.06.10
NO.	REVISIONS	DATE

SSDG
INTERIORS INC.

SSDG Interiors Inc.
350 - 450 Granville Street, Vancouver, BC Canada V7Y 1G5
© 854 685 4301 www.ssdg.com

Job Title:
EGGS CANNA
115 EAST 1ST ST
NORTH VANCOUVER, BC
V7L 1B2, CANADA

Drawing Title:
EXTERIOR STOREFRONT
ELEVATION

Project Date: 20.06.10
Drawn by: SM Checked by: RB
Project No: 2010 150 Scale: 1/4\"/>

ID5.1



CITY OF NORTH VANCOUVER

MUNICIPAL CONSIDERATION & BUSINESS PLAN

- MAY 2020 -

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**This section is outlined in the complete security
guide provided as an addendum to this BP*

1 | ABOUT EGGS CANNA®

EC was founded in 2014; we opened our doors as part of the liberal movement in Vancouver driving the legalization of cannabis. EC was amongst the first retailers in Vancouver to obtain a Medical Marijuana Retail-Use License (MMRU) to operate a cannabis store. We have been offering evidence-based stakeholder input and guidance for many municipalities during local government discovery phase for drafting their cannabis bylaws. In 2015 we were the first retailer to negotiate benefit packages for employees in the cannabis sector—we believe in not only what we do but in the people, who have helped drive legalization.

EC has been very passionate and outspoken about the importance of community buy-in and support and as such, dedicates resources to hosting open houses and educational gatherings. Through these public forums and dialogue sessions we better understand the needs and concerns of the communities we operate in and establish ways to work together in ensuring that those who are legally allowed to purchase cannabis can do so in a safe and legal environment, while protecting and educating our youth.

With many years of hands-on cannabis retail experience, we are well positioned to bringing a controlled and safe distribution channel to our cities.

EC understands the complexity and social impact of legalization, and we are committed to continue being a trusted partner for our local government in conjunction with becoming a respected and trusted retailer for our neighbourhoods.

EC is proud to partner our industry expertise with local entrepreneurs in a partnership business model, keeping the responsibility and profits local thus ensuring we drive local economies and help better the communities we work in.

RESPONSIBLE REGULATED DISTRIBUTION

We are one of the few operators who have hands-on cannabis sales under a licensed Municipal framework; this has been instrumental in developing some of the most comprehensive SOP's we still use today, all of which are in-line with Provincially mandated standards. We will confidently say that by allowing us to operate in your municipality, we can ensure complete compliance in all regulatory processes.

As part of our operations we fostered long-lasting relationships with our communities, and local businesses. EC has been involved in, and worked with many local charities, local BIA's, community programs such as "car free days" and many other social gatherings. As regulated distributors we had the task of introducing cannabis to our communities maintaining the most refined, socially responsible and least negatively impactful approach.





Eggs Canna® understands that building strong community rapport, trust and buy-in is a crucial factor in a successful transition to cannabis retail. Canada, the Province of BC and the city of North Vancouver are making history in the legalization of cannabis retail; EC is a trusted partner for all levels of government to help facilitate a successful transition.

We understand the importance of shifting old stigmas, and complimenting the neighbourhoods we operate in. EC achieves this through a timeless exterior design; wainscoting and mullions give a warm Victorian je ne sais quoi--a timeless design that fits well in both old and new communities.

EC understands mandated window coverings are not an ideal feature; we have worked with our design team to offer a solution that is both desirable and beautiful. The covering will be a 3D image of the interior of the store. EC will seize the opportunity to highlight the interior elements that make our stores beautifully unique. Our storefronts feature antique brick walls, cast iron sconces and a preserved moss wall that brings much needed greenery and colour into the space. Our new black and white high-gloss cabinetry and Porcelona floor tiles will wow any passer-by while fitting perfectly in this heritage building.

KEY DESIGN ELELEMENTS





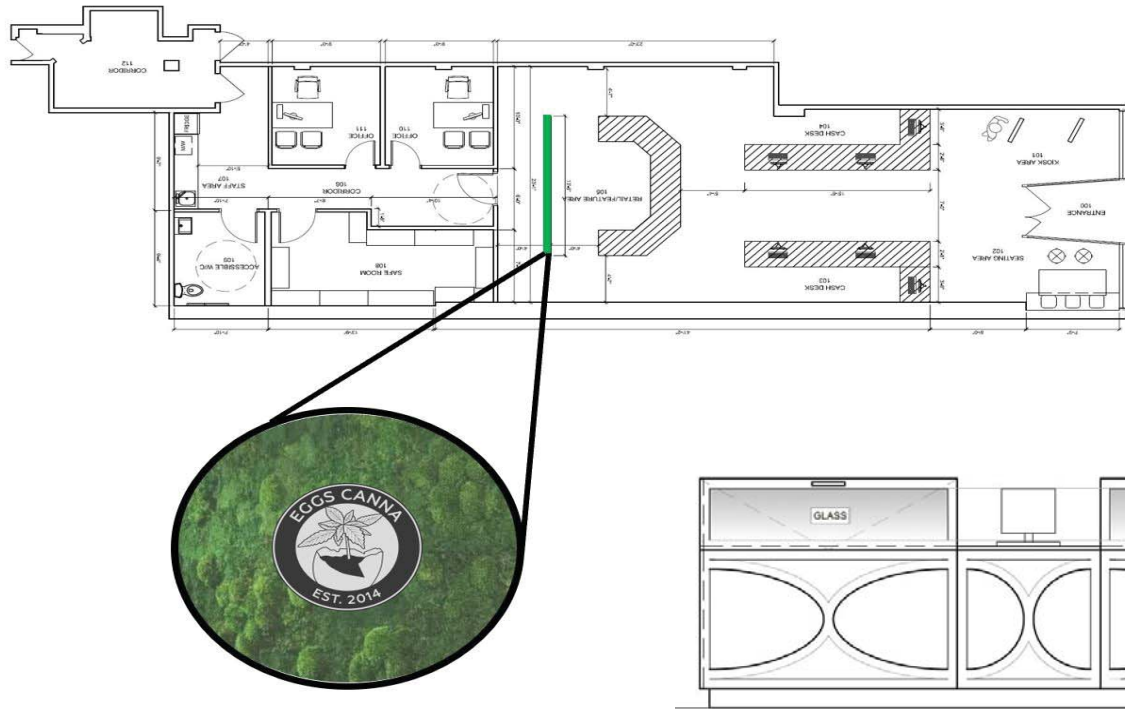
Eggs Canna beautifully marries geometric black and white shapes with soft greys and greens to give EC spaces a luxurious boutique feel. Our Victorian inspired Retail Cannabis stores evoke feelings of warmth and elegance.

Our current retail operations have given us valuable insights as to what the 'new age Cannaseur' wishes to see on the shelves and the experience they desire.

EC has opted to depart from what has become a common conceptual design amongst the new legal retailers: the wood and sterile contemporary feel was not captivating the cannabis consumer. Many consumers felt as though they were either in a "Saje" or "iPhone" store and thus not resonating as a cannabis retailer. EC has also moved away from the digital experience - instead our focus is on guiding and shaping our consumers' experiences with personal one-on-one assistance.

EC prides itself on curating a retail experience that is both unique and memorable. Our knowledgeable Eggies (Sales Ambassadors) shift old stigmas by re-framing them - our customers embark on a journey exploring cannabis history and products- creating a true retail cannabis experience.

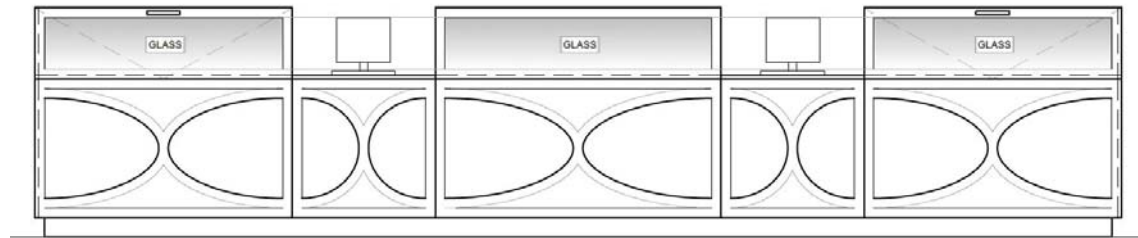
Our Eggies have a strict dress code that both compliments the design of our stores and helps to re-frame old perceptions of cannabis. Eggies are here to serve our communities in a white bottom up dress shirt, EC monogram tie and suspenders with clean dark blue jeans, and lastly, a pair of converse in any of our approved 6 colours. Our intention was to create an atmosphere of elegance and fun.



The beautiful moss-feature wall will draw the eyes when you first walk in. The neon writing in and around the logo will illuminate the space and help create a feeling of warmth and brightness.

Our storefront will feature (6) six tills & (3) three portable tills, ensuring that our guests are served in a timely manner.

We anticipate that the two stores located in Lower Lonsdale will be high volume retail outlets that must be adequately equipped to ensure that customers do not experience long wait times.



White high-gloss cabinets with black half-circle details make our store fronts feel luxurious and posh. Our play on geometric shapes and patterns, coupled with bold greenery create a truly unique shopping experience.

EC will continue to work hard and shift stigmas regarding Cannabis Retail, we are very excited and confident in our Victorian inspired design; We hope to become North Vancouver's most sought-after boutique retailer.

FLOOR PLAN & CABINETRY



2 | OUR EXECUTIVE TEAM

EC has some of the most dedicated and experienced industry professionals helping develop industry first SOP's, ensuring every aspect of the new regulatory framework is a seamless and successful transition. Our focus is just as much on compliance as it is on achieving the best possible consumer experience. We understand that the success of cannabis legalisation is hinged upon our individual success as retailers and brand ambassadors. With the world watching Canada, and all levels of government we want to ensure everyone's success as we navigate these uncharted waters together.

Our team has 50+ years of cumulative retail, legal and cannabis experience, ranging from Human Resources, cultivation and plant genetics, large scale project management to decades of hands on franchisor and franchisee know-how in one of the most competitive industries; restaurant management. This multi-faceted team has the skill set required in maneuvering the complex parameters around licensing, social responsibility and community impact, all the while safeguarding against improper handling and distribution of Cannabis.

OUR CORPORATE PARTNERSHIPS

As with any industry, strategic partnerships are pivotal to a company's continued success and growth. And as such, we have expanded our network to ensure that we are well positioned to execute from site selection to compliance and everything in-between.

i. STRATEGIC REALESTATE (SRE)

Our partners at SRE have been an integral part of our site selections, working to ensure that we meet all needed distancing requirements and in securing leases in a timely manner.

ii. SSDG DESIGN

The team at SSDG Design produce all Provincial Site and Retail Space renderings, provide complete Permit Set Drawings and act as the Project Managers for all future Canada-Wide store build-outs. Their keen eye for design ensures that our storefronts are elegant and unique spaces embodying warmth with a grand Victorian feel. SSDG is instrumental in guaranteeing that all Municipal and Provincial design specifications are met, and that projects are completed in the quoted time.



iii. TECH POS

EC has worked with TechPos in developing a comprehensive and industry specific Point of Sale (POS). This allows for seamless conversion with our accounting software (Big Chip Accounting) to meet stringent reporting requirements for the governing Provincial body. In addition to this we have developed a comprehensive encyclopedia and product log easily accessible for our team ensuring extensive product knowledge and training.

iv. MONERIS & AMEX

As cannabis retail moves into the legal landscape, we are very happy to announce that we have signed on with Moneris and Amex for our card processing needs and will be ready to offer card purchases as soon as we are ready to open. This is a huge milestone for cannabis retailers as many card processing companies are just now entering this market.

v. BENEFITS BY DESIGN

We believe that all **employees should receive full medical and dental** regardless of the industry you work in, and we are very excited to have partnered with Benefits by Design who have given us and our employees a wonderful comprehensive benefits package.

vi. ALTHING CONSULTING

As part of our continued commitment to the jurisdictions we work and live in, we have partnered with Althing Consulting to assist both the municipalities with any cannabis related questions or concerns they may have, along with providing EC with valuable direction and guidance on all legislative regulations. Jaclyn Pehota and Ian Dawkins have over 10 years of community and municipal engagement experience, having assisted over a dozen municipalities craft bylaws.

vii. BIG CHIP ACCOUNTING

We are very excited to have partnered with Big Chip for all our Corporate and Partnership stores; their user friendly and remote interface ensures that compliance is made easy. **Big Chip has years of experience is providing auditable accounting systems to serve the strict requirements needed for the private liquor industry, and their cannabis reporting systems exceed the strictest of Provincial and Federal requirements.** In addition to their regulatory platform they offer a centralized company portal where all our Provincial training modules, and EC specific training materials can be uploaded and monitored. This ensures that prior to employees working the sales floor they have completed;

- All company training and test modules (ongoing throughout term of employment);
- Provincial Worker Application Qualification Form;
- Attestation Form, and
- Witness Attestation Form.



This training and HR component guarantee EC the upmost control over all regulatory process needed to be completed for staff—it is a failsafe ensuring no employee slips through the cracks and begins working prior to meeting all qualifying factors.

viii. WESTLAND INSURANCE

Having comprehensive business insurance is important for the cannabis sector, and we are pleased to have partnered with Westland who brings experienced and recognized underwriters to this previously uninsured industry.

CO. FOOTPRINT

As a new and emerging industry, we at EC believe that we have a once in generation opportunity to do something great, we therefore strive to bring unique business practices; and have vowed to do things a little different wherever we can! Our promise is to be socially responsible and forward-thinking.



Wages

We budget for higher than normal compensation packages ensuring that all staff receive a great living wage. Our sales team wage is currently around \$18/hour, growing up to \$23/hour for Shift Lead Roles; growing exponentially higher for Management positions. Legalisation **must** drive our local economy, offering good wages is a great way to give back to the people who will pioneer this industry.



Benefits

We have negotiated and offer some of the most comprehensive medical and dental packages for our staff. We are proud to say that we are one of the few retail companies who are not union to offer full benefit packages. We expect great things from our staff, so we too need to do great things for them!



Renewable hemp products

We are always excited about anything cannabis related and as such, we cannot contain our excitement for all the new developments in renewable materials such as hemp, and its many applications. As part of our promise we will go the extra mile to ditch the plastics and add hemp to our day to day uses in and effort to reduce our carbon footprint.



Community and charitable projects

EC strives to be community leaders and is involved with many local charities and events. We all have a social responsibility to give back! We opt to work with small local charities in our continued efforts to have a positive impact directly on the communities we live and work in.



3 | OPERATIONS

EC SOP's

EC has spent the last six years working with local municipalities, and other levels of government to better understand required Standard Operating Procedures for dispensing cannabis. We inherited many of the current SOP's from industries such as privatized and government-run liquor distributors who have similar protocols and needs as cannabis distribution. We can confidentially say that we have created concise and comprehensive manuals to capture this new industry and ensure safe handling and dispensing of cannabis and its derivatives.

With four years of hands-on cannabis retail we have gained unprecedented industry insight with SOP's that have been tried, tested and true. We can easily conform to, and surpass, the strictest of regulatory processes, ensuring compliance at every level.

HOURS OF OPERATION

As per the BC Cannabis Retail Store Terms and Conditions, storefronts may operate between the hours of 9:00 am to 11:00 pm. We have taken into consideration several factors and have concluded our "standard hours of operation" in new markets, would run with the allocated maximum giving us case by case flexibility to adjust accordingly. **In the City of North Vancouver, we are purposing our hours be Sunday – Monday 9:00 am to 11:00 pm, with limited hours of 11:00 am – 7:00 pm on all Statutory Holidays except New Years Day and Christmas Day when we will be closed.**

HIRING

Hiring is completed by the Manager at each location, with Human Resources support from our partners at Big Chip Accounting and Corporate Head Office where needed. As per the Hiring Contract that all employees must sign, all offers of employment will be made void if the employee fails any of the mandatory screening and/or requirements necessary by both the Company and the Province of British Columbia to be an employee under the Cannabis Retail Store Terms and Conditions for the entire tenure of their employment with EC.



TRAINING

Initial training of all employees is conducted by the Corporate Training Team, both in store, at our licensed flagship location utilizing Big Chip's training and testing platform. Continued and subsequent training will be conducted by the Manager at each location utilizing only Corporate approved training materials on the Big Chip training and testing platform. EC has developed a proprietary training program for which all staff must pass all sets of modules, in conjunction with the Provincial training requirements for ongoing employment.

STAFFING ROLES AND RESPONSIBILITIES

Sales Ambassador (SA) aka EGGIES: Our Eggies are instrumental to our success, not only as a company but as an entire industry. Our SA's are gatekeepers, they are extensively trained for compliant cannabis distribution, coupled with risk management and mitigation giving them the needed tools to help protect the public at large from any unlawful cannabis distribution or nuisance activities. In addition to this, Eggies are expected to have extensive product knowledge and undergo regular testing as new products are added to our shelves and the Provincial regulated cannabis products' list.

Cash Out Assistant: This role is reserved for a senior SA and comprises of arriving early to open the store and complete the cashout SA's previous day/night shifts. This is a cash-handling role in the mornings, that will role over into a SA position for the remainder of their shift. We have strict cash-handling protocols that closely resemble controls set by the gaming industry, and all our systems are set up to pass both internal and external audits.

Guest Experience Team Lead (GE Team Lead): GE Team Leaders work the sales floors with the SA's and offer support and guidance and can act as intermediate supervisors when a Manager is not available or deemed unnecessary for that specific issue/instance. These are senior staff members who often will move into Managerial positions. It is imperative that we have senior staff on shift to assist with the flow of the business and to address any concerns or issues that should arise during day to day operations to maintain a controlled and compliant service environment.

Assistant General Manager (AGM): AGM's are instrumental in their roles by assisting the General Manager with the day to day operations, handling any compliance issues, ensuring standardized practices of all regulatory processes, addressing customer complaints, and managing the ongoing hiring, training, and management of employees. Not all stores may have an AGM, this additional level of management is reserved for large busy locations. **At this time, we have not included this layer of extra management in the North Vancouver Monetary and Labour Forecast.*

General Manager (GM): The General Manager is responsible for all staffing and scheduling requirements, they oversee everything from compliance, accounting, sales, training, hiring, people development, Human Resources, labour and sales forecasting and working closely with the different Executive Roles. Any operations within the four walls, and outside the storefront that can impact the individual business or the corporation, is the sole responsibility of the General Manager, therefore, this role must be approved by the Executive Team prior to making a formal offer of employment.

Staffing levels: We anticipate having a total of 15 (fifteen) employees staffing this location, with 10 full-time staff and 5 part-time staff.



- 10 Sales Associates
- 2 Cash Out Assistants / Sales Associates
- 2 Team Leads
- 1 General Manager

MONETARY AND LABOUR FORECASTING

Please see addendum.¹

POS | TECHPOS

As previously touched on, EC partnered with TechPos and has been working closely with their team in developing the most comprehensive and industry specific point of sales systems. We are very proud to have created this detailed platform that will assist our staff in providing the safest and most effective service possible.

Our custom industry specific POS includes features like;

- Automated tracking and totaling of maximum purchase allowances in one transaction;
- Auto prompt asking if patron has shown ID prior to purchase;
- Easy record keeping and automated tracking of waste products, significantly reducing staff responsibilities and opportunity for errors;
- Security feature will not allow employees to clock out for a break if it will leave the store understaffed and with less then two employees;
- Automated nightly inventory checks;
- Auditable sales records synced with Big Chip Accounting and,
- Easy two-step cash deposit verifications significantly mitigating counting errors.

¹ See Monetary and Labour Forecasting - Addendum pg 10



PRODUCTS SOLD

As per the Retail Cannabis Store Terms and Conditions, a retailer as of October 17, 2018 may sell the following and in one transaction, they may NOT surpass the total equivalent of 30g of dried cannabis or as stated below;

PRODUCT	MAXIMUM ALLOWED IN ONE TRANSACTION
i. Dried cannabis	(30g)
ii. Fresh cannabis	(150g)
iii. Solids containing cannabis	(450g)
iv. Non-solids containing cannabis (cannabis oil)	(2,100g)
v. Cannabis oil concentrates	(7.5g)
vi. Cannabis non-solid concentrates	(7.5g)
vii. Cannabis plant seeds	(30 seeds)

To ensure strict compliance with these guidelines our POS will automatically add up any variation of these maximum allowable purchases at time of purchase and if one purchase has been rung in over the limits as stated above, that transaction will automatically be flagged and a manager code will need to be entered, any item(s) over the allowable limit are to be removed prior to the sale being made. This will guarantee that no sales over the limit as shown above could be processed at any of our establishments. These thresholds will be modified in the POS as new limits are established and new items are added.

In addition to cannabis and its derivatives, all EC locations will also sell cannabis accessories such as rolling papers, wraps, holders, pipes water pipes, bongs and vaporizers.

EC locations will NOT sell, e-juice, e-liquid, snacks, tobacco or any products containing nicotine, or any other products or apparel.



SAFE DISPOSAL OF CANNABIS PRODUCT

In accordance with the BC Cannabis Retail Store Terms and Conditions disposing of non-medical cannabis, EC has created compliant protocols for tracking and reporting any discarded product. Our custom-built POS software allows us to;

- i. Scan the sku for the “waste” product to ensure there are no errors in inputting the exact item that is spoiled;
- ii. Prompts Management to state why it is a waste product, i.e. “smell jars”, or “opened package”;
- iii. When in “waste” mode TechPos will prompt the Manager to weigh the spoiled product in the built-in POS scale;
- iv. TechPos will then generate a report for any wasted product, this data is then collected by Big Chip Accounting and remitted to the BC LCDB for reporting purposes and ensure compliance with provincial regulations for the disposal of any cannabis product.

This streamlined system creates error proof inputting by automating the entire process and eradicating human error. Automatic reporting also ensures that records are kept up to date and easily accessible.

In order to be rendered unfit for human consumption, the wasted product is then shredded and mixed with vinegar in a compost bin. The vinegar will completely degrade the THC and render it unsuitable for human consumption, vinegar also destroys the smell of the product in a eco-friendly and biodegradable process.

INSURANCE

As we navigate into a legal landscape it is becoming increasingly important we establish and maintain processes that will ensure the safety and well-being of our patrons, staff and neighbours.

i. **Course of Construction (Builder’s Risk)**

Though this policy is generally provided by your contractor, for an extra layer of security it is also built into our general policy. This means that if major structural changes are made during construction or renovation, the municipality can rest assured that any errors can be immediately and effectually dealt with, without concern about funding.

ii. **Property**

Alongside standard property insurance, all EC establishments carry additional insurance for wind, hail and flood to cover any instances of inclement weather damage.



iii. Comprehensive General Liability

Our comprehensive general liability covers products, personal and advertising injury, contractual (with independent contractor) coverage and motor vehicle liability. This policy protects the store, its staff and the community in general.

iv. Business Interruption

We understand that circumstances outside of our control do sometimes happen, and to ensure that all EC locations are able to reopen as well preserve the jobs of all staff we have comprehensive business interruption built into our coverage.

v. Cyber Fraud and Security

In this modern time, the safekeeping of virtual assets is just as important as the protection of physical ones, for the guarantee of privacy for all of those who are a part of our establishment and organization as a whole we carry cyber and fraud insurance.



4 | BUILD AND OPENING SCHEDULES

EC and SSDG Design have been working tirelessly on a concise and well executed build schedule, and we are very happy with not only the beautifully designed stores but the time in which we complete our builds.

CONTRACTORS

SSDG uses only trusted and nationally recognized contractors for its locations; ensuring not only the quality of work but also that projects are executed on time, on budget, without any unmitigated delays.

STORE BUILDS + WORKBACK SCHEDULES

Our corporate team works very hard in gearing up for successfully and craftly executed grand openings. We offer full store opening workback schedules² and unprecedented experience and support during all aspects of build, training and of course grand opening. Our Franchisees can count on our many years experience and hands on assistance, in ensuring their grand opening is a huge success. And likewise, the municipalities that we partner with can rest assured knowing we are a trusted, experienced and capable retailer who will open in the timeframes we have set fourth. We can appreciate the urgency in opening and offering our citizens safe and legalised cannabis access without exorbitant and lengthy delays.

BOUTIQUE INSPIRED DESIGN

Our design phase was a year in the making, and that is because we wanted to ensure we got it right! As Canada's leading cannabis retailer, we have created the ultimate guest experience through our brand and our spaces. Eggs Canna® retailers capture that wonderful nostalgia moment; inspired by our most familiar gathering spaces – the kitchen island or our favourite boutique restaurant.

² See Store Opening Workback Schedule - Addendum pg 12



5 | CANNABIS MARKETS

One of the primary motivations behind the legalization of cannabis was better control for keeping cannabis out of the hands of Canadian youth. We understand and appreciate that this can be a challenge, but rest assured that EC is committed to enforcing age restricting policies in our stores, and on a broader scale in our marketing and advertising campaigns.

i. Legal Age Verification

EC staff are trained in proper forms of ID (in accordance to BC LCDB), employees are trained to ask for TWO pieces of valid identification for any person(s) who look(s) under the age of 30 years old. TechPos also has an age verification failsafe that does not allow for the transaction to proceed without manually hitting yes to the check ID prompt. All staff are thoroughly trained in what forms of primary and secondary identification are acceptable and can act on the authenticity of the identification provided. Associates understand how to mitigate the risk of having the illegal purchase of cannabis by underage person(s), but should an incident of that nature occur we have extensive protocols properly documenting details around what happened in an effort to both prevent that person(s) from entering the premises in the future, as well as training for the staff member to ensure this does not occur again. EC corporate conducts regular secret shopper programs ensuring that these protocols are working as intended and all employees are strictly adhering to them for the tenure of their employment with us.

ii. Advertising

We understand that cannabis can be a polarizing product and pledge to advertise in such a way as to be non-offensive and unappealing to underage consumers. Our approach targets legal consumption age spaces and encourages safe and responsible use of cannabis.

iii. Target Market

In this new era the face of the modern cannabis consumer is changing. We want to appeal to every consumer, from the millennial to the baby boomer, from the connoisseur to the new customer, and we feel confident that our knowledgeable and friendly staff will be able to guide them through their shopping experience in the most exciting and memorable way.



6 | SAFE VENDING OF PRODUCT

BC CANNABIS RETAIL STORE | TERMS AND CONDITIONS

BC has created a comprehensive guide for the safe and responsible vending of cannabis. EC has the SOP's that allow for us to effectively enforce and abide by the regulations around the sale of cannabis; in addition to the Cannabis Retail Worker certification, we have developed a proprietary training program spearheaded by our Director of Staff and Training which best prepares employees for the control they will have to maintain over their space. This training program covers in further detail provincial guidelines, local municipal requirements and compliance with regulations to ensure only a positive impact is left on the community by the operating of the store.

MUNICIPAL LAWS

Each municipality has set its own rules around cannabis in accordance to what works best for its citizens. With our real time training program, we can stay on top of changes and ensure that the store always obeys the directive of its city. As pioneers in this industry, we will set the standard for what is expected from future stores in cooperating with the city to everyone's mutual benefit. EC is committed to establishing harmony between the business and the municipality.

ONGOING REGULATORY TRAINING

We have developed, using the Big Chip platform, an extensive proprietary training program for all staff. This program evolves as time goes on and further regulations or products are added to cannabis offerings. At its start this program covers:

- i. Age verifying and responsible sale of product (quantity limitations, not serving intoxicated patrons)
- ii. Extensive product knowledge (covering basic cannabis principles including the entourage effect, interactions, and potency)
- iii. Enforcement of municipal and provincial rules (including where to consume cannabis and proper storage and transportation)
- iv. Operating in a safe and responsible manner (staff on site always preserving the space around the store against nuisance, litter and security)



Our staff are prompted with their required training when they log on to view their schedule and are locked from accessing the schedule if they do not complete the training. The store manager and EC corporate also supervises the completion of this training with quizzes after each learning module and statistics on time spent completing the module.

As further regulations are added further training modules can be added without delay, ensuring all staff are always in compliance with new rules.



7 | SOCIAL RESPONSIBILITIES & IMPACT

GOOD NEIGHBOUR PRACTICES

We at EC recognize the importance of neighbourhood buy-in, and understands that this begins with us, prior to opening during operations and even after we have closed. To ensure that our neighbours know who we are, we conduct our know neighbourhood canvas and meet and greets. We have found this to be incredibly effective in building rapport and trust and offering our community⁴ an opportunity to meet us and voice any questions or concerns they may have. During business hours we also have numerous programs to help enforce non-smoking compliance, prevent loitering and littering and mitigate any nuisance behaviour.

i. **Non-Smoking Compliance and Enforcement**

Our stores and the area around them are strict non-smoking zones. We understand the nuisance that can be caused by patrons that are not properly informed of the spaces that they are not allowed to consume cannabis in. As well as educating our customers as to where they may smoke their purchases, our staff is also responsible for enforcing the smoking ban around the store. This is done through proper signage, and regular walk through rounds done around the building and within a one block radius in each direction.

ii. **Litter and Waste Pick Up**

No business ever wants to see their garbage around the neighbourhood. Though we cannot always control where people choose to throw their garbage, we do have a robust community cleanup program where staff do routine garbage checks and cleanup in their community (including not only the space around their store, but local parks and other places where people congregate). We work together to keep our cities spotless and beautiful, and free from any cannabis packaging and paraphernalia.

iii. **Nuisance and Noise Mitigation**

Spaces centered around consumption of any kind can easily become hubs attracting revelers and merry makers. And while this might be energizing for the store, we know that our neighbours don not always appreciate the noise. Music in the stores is controlled by a central hub ensuring that the volume level is always consistent with local requirements and with the smoking ban patrons are encouraged to dissipate from the space quickly if they are no longer making purchases.

³ See Good Neighbour Agreement: City of North Vancouver - Addendum pg 3

⁴ See Community Impact Statement - Addendum pg 4



iv. Odour and Esthetics

EC has always prided itself on modern, bright, welcoming stores as fit to be a jewellery shop as they are for being a retail cannabis store. Our Victorian inspired spaces and beautiful layout is a perfect addition to any neighbourhood. We understand that some might wonder about the odour from retail cannabis stores - rest assured that all products are sealed in airtight containers and product is never opened on site.

v. Community Programs and Open Houses

Being a good fit across the board is important for us, and it strengthens the bonds of a community when it's neighbours can come together to participate in events, raise donations for charity and just in general have fun with each other! We are happy to host regular open houses and welcome the locals to ask questions, voice concerns and make suggestions for what they would like to see from us. EC has also been and will continue to be an initiator of community programs like safe spaces, rubbish cleanup and food and toy drives, and charitable contributions to locally driven charities. Together we can make a difference through education, compassion and passion for the places we live and work.



8 | RISK ANALYSIS & MITIGATION PROTOCOLS

EC has worked with the experts at 3si in creating protocols that ensures the safety of our patrons, staff and the community at large. Detailed in the security plan⁸ we have completed a comprehensive and concise SOP for risk analysis and risk prevention and mitigation.

⁸ See Full Property Security Plan

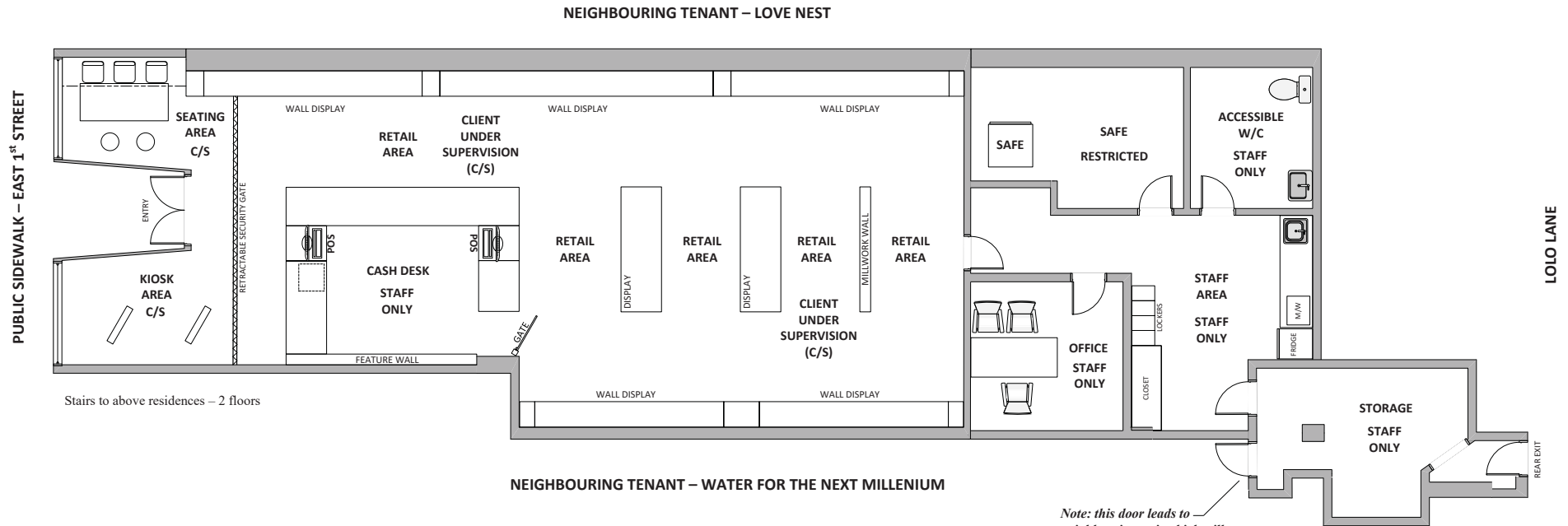


ADDENDUM

Please see the accompanying addendum to follow the Municipal Consideration and Business Plan below:

TABLE OF CONTENTS

Cannabis Retail Questions & Statistics	2
a. Parking & Traffic	2
b. Lower Lonsdale Needs Another Cannabis Retailer	3
c. Nuisance & Security	3
Eggs Canna Community Impact Statement	4
Letters of Support	6
Affidavit - Andrew Cappellano	7
Good Neighbour Agreement	8
Monetary & Labour Forecasts	9
Store Opening Workback Schedule	10
Landlord Letter	12
Property Title Search	13
Landlord Letter	14



NOV 15, 2018

jm/ct

Ver 1.0

Eggs Canna Inc.
115 East 1st Street, North Vancouver

0 ft. 9 ft. 15 ft. 30 ft.
Approximate Scale
See architectural for precise scale

3Si Risk Strategies



LEGEND

- Public
- Neighbor Tenanted Space
- Clients under Supervision (C/S)
- Admin/Staff Only
- Restricted

0 ft. 9 ft. 15 ft. 30 ft.
Approximate Scale
See architectural for precise scale



NOV 15, 2018

jm/ct

Ver 1.0

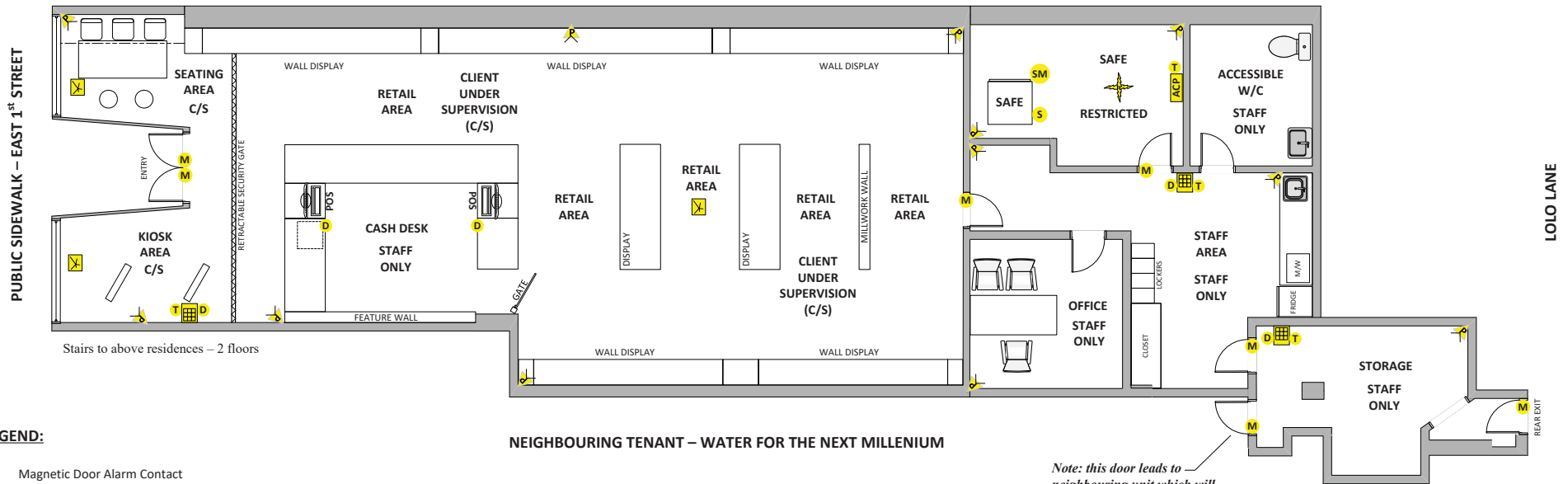
Eggs Canna Inc. SECURITY LEVELS

115 East 1st Street, North Vancouver













3Si Risk Strategies



NEIGHBOURING TENANT – LOVE NEST



LEGEND:

-  Magnetic Door Alarm Contact
-  PIR Motion Detector
-  Duress Button
-  Glass Break Sensor
-  Alarm Siren
-  Smoke Detector
-  Seismic Sensor
-  Tamper Detection
-  Alarm Control Panel
-  Card Reader
-  Alarm Keypad
-  Push button door release

NOTES: Intrusion Detection System

Alarm Contacts. Magnetic contacts N.O. specified to be compatible with alarm panel and door frame and installed as per manufacture recommendations.

All alarm zones to be home run.

Wide Angle PIR. Minimum 90 degree FOV with min 40' range on x and y axis or better.

Glass Break min. effective range of 10' by 10 ft. and compatible with alarm panel.

30 Watt outdoor rated siren. Dual Tone. Compatible with Alarm Panel.

Panel configured with duress codes at each keypad.

Alarm monitoring continuously by ULC listed monitoring company

All Users to have independent codes

Master Code only used to add and remove user codes

Codes to be changed yearly

Codes to be removed when no longer required

Full programming print-out including event buffer to be supplied by integrator (less the installer code)

System to operate on 2 independent partitions. One for the back area (south) and the other for the front area (north)

Back up battery for 4 hours operation

All troubles and tampers to be reported to Central Station

0 ft. 9 ft. 15 ft. 30 ft.
Approximate Scale
See architectural for precise scale



NOV 15, 2018

jm/ct

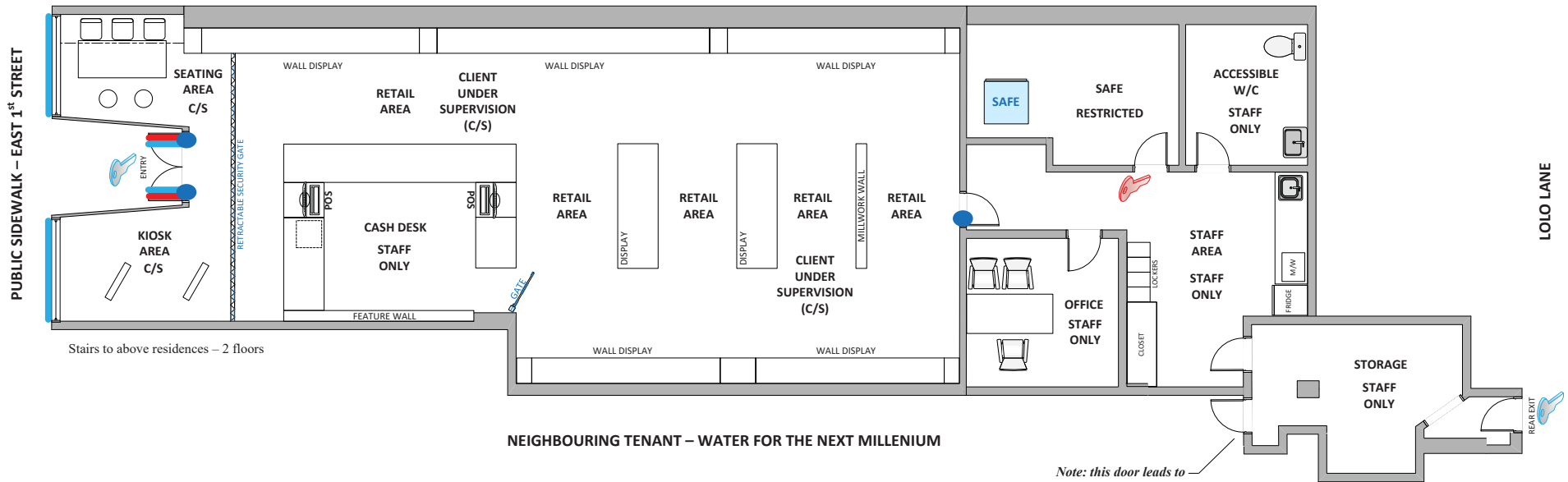
Ver 1.0

Eggs Canna Inc.
INTRUSION DETECTION
115 East 1st Street, North Vancouver

3Si Risk Strategies









NEIGHBOURING TENANT – LOVE NEST



*Note: this door leads to —
neighbouring unit which will
be occupied by Eggs Canna for
non-dispensary business use*

LEGEND:

-  Non-Removable Pins
-  Security Film
-  Full length Astragal (FLA)
-  Security bars
-  Restricted Keyway - Restricted
-  Restricted Keyway - Admin

NOTES: Mechanical

Security Film on front windows. Film rated and installed in frame. 3M Ultra600 or equivalent.

Add full length Astralgal and Security film to front entry doors.

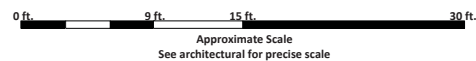
Unique keyway for Safe Room door. Unique keyways will not be on a master key system.

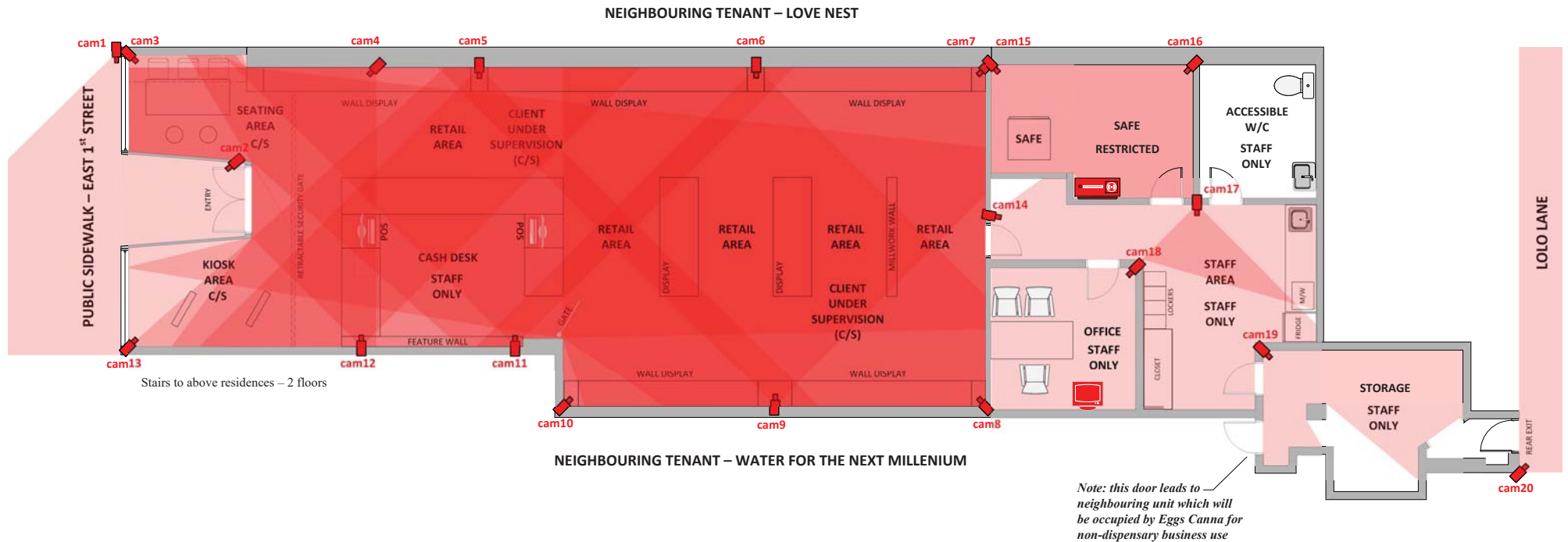
Add non-removeable pins to front entry doors and Staff Area door.






NOV 15, 2018	jm/ct	Ver 1.0
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Eggs Canna Inc.
MECHANICAL SECURITY
115 East 1st Street, North Vancouver





LEGEND:

-  Review and Download Station
-  IR Camera
-  Network Video Recorder

NOTES: Video System

220 ppm resolution at entrance door.
110 ppm minimum resolution for all other areas indicated.
Based on 4MP. FOV as shown.
Camera IR assist to operate in all light conditions (30m minimum).
Single Camera FOV
Dual Camera FOV
Triple Camera FOV
Recording at 1fps min. 7.5 fps min on motion with 5 second pre and post recording

0 ft. 9 ft. 15 ft. 30 ft.
Approximate Scale
See architectural for precise scale



NOV 15, 2018

jm/ct

Ver 1.0

Eggs Canna Inc.
VIDEO SECURITY

115 East 1st Street, North Vancouver

3Si Risk Strategies

ORDER OF THE LIEUTENANT GOVERNOR IN COUNCIL

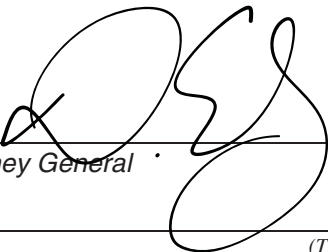
Order in Council No. 316

, Approved and Ordered June 18, 2020


Lieutenant Governor

Executive Council Chambers, Victoria

On the recommendation of the undersigned, the Lieutenant Governor, by and with the advice and consent of the Executive Council, orders that the Cannabis Licensing Regulation, B.C. Reg. 202/2018, is amended as set out in the attached Schedule.


Attorney General
Presiding Member of the Executive Council

(This part is for administrative purposes only and is not part of the Order.)

Authority under which Order is made:

Act and section: Cannabis Control and Licensing Act, S.B.C. 2018, c. 29, s. 133

Other: O.C. 528/2018

R10406443

SCHEDULE

1 Section 5 (1) of the Cannabis Licensing Regulation, B.C. Reg. 202/2018, is amended by repealing paragraph (p) and substituting the following:

- (p) the retail store must be located in a permanent building or structure;
- (p.1) the following must not be visible from outside the retail store:
 - (i) cannabis;
 - (ii) cannabis accessories;
 - (iii) packaging and labelling of cannabis and cannabis accessories; .

Date: June 18, 2020

To: All LCRB staff
All Licensees
All Industry Associations
All Local Government, First Nations, and police agencies

Re: Cannabis Licensing Regulation amendment – non-transparent walls

Previous Regulatory Requirement

A cannabis retail store must be located in a permanent building or structure and be enclosed by floor-to-ceiling walls that are not transparent.

New Regulatory Requirement

A cannabis retail store must be located in a permanent building or structure. Cannabis, cannabis accessories, and packaging and labelling of cannabis and cannabis accessories must not be visible from outside the cannabis retail store.

Explanation

The amended regulation supports the Province's commitment to protecting minors from exposure to non-medical cannabis.

Further Information

This communication contains general information about requirements under the *Cannabis Control and Licensing Act* and is not a replacement for the statute or regulations. It does not contain or constitute legal advice.

Further information regarding liquor and cannabis control and licensing in British Columbia is available on the Liquor and Cannabis Regulation Branch website at <http://www.gov.bc.ca/liquorregulationandlicensing>

If you have any questions regarding these changes, please contact the Liquor and Cannabis Regulation Branch toll free in Canada at 1-866-209-2111 or 250 952-5787 if calling from the Victoria area.

Original signed by

Mary Sue Maloughney,
Assistant Deputy Minister and General Manager



City of North Vancouver

COUNCIL POLICY

Policy Name: Recreational Cannabis Retail Policy

Policy Number: 7

POLICY

Recreational Cannabis Retail Policy

REASON FOR POLICY

The Government of Canada has announced the recreational use of cannabis will become legal in Canada on October 17, 2018. The federal Cannabis Act is the legal framework under which the production, distribution, sale and possession of cannabis, for both medical and non-medical (recreational) purposes, is regulated in Canada. Through legislation including the Cannabis Control and Licensing Act, the Province of British Columbia has created a legislative framework to supplement federal legislation and facilitate legal and controlled access to recreational cannabis in British Columbia following federal legalization.

In coordination with federal and provincial regulations, this Policy creates a framework under which recreational cannabis retail businesses will be established in the City, with a focus on the following:

1. Defining **locational criteria** for where businesses may be situated, based on land use requirements, separation from sensitive uses and geographical distribution;
2. Implementing the **application procedure** under which proposals for new cannabis retail businesses are to be submitted, assessed and approved to operate within the City.

Applicability

This policy is applicable to all rezoning applications to operate a recreational cannabis retail business in the City.

Authority to Act

The legislative framework of the Province of British Columbia, including the Community Charter, Local Government Act, and the Cannabis Control and Licensing Act, provides the basis upon which the City may regulate certain locational aspects of recreational cannabis retail businesses, as well as procedures for assessing and approving business proposals.

Policy Name: Recreational Cannabis Retail Policy

Policy Number: 7

Administration of this policy is delegated to the Planning Department.

PROVISIONS AND PROCEDURES

The following describes locational criteria for all recreational cannabis retail businesses in the City and outlines the procedure under which applications will be accepted, assessed and approved.

Locational Criteria

1. Recreational cannabis retail businesses may only be located on properties assigned with the following land use designations under the 2014 Official Community Plan (OCP) – Schedule A Land Use Map (see Schedule 1):
 - Mixed-Use Level 2 (Medium Density);
 - Mixed-Use Level 3 (Medium Density);
 - Mixed-Use Level 4A (High Density);
 - Mixed-Use Level 4B (High Density);
 - Harbourside Waterfront (Mixed-Use); and,
 - Commercial.
2. Recreational cannabis retail businesses may not be located within a 100-metre radii of the following sensitive uses (see Schedule 2):
 - Community and Recreational Centres;
 - North Shore Neighbourhood House;
 - North Shore Shelter;
 - North Vancouver School District Office; and,
 - Public elementary and secondary schools.
3. A maximum of six (6) retail businesses will be considered. Business locations are to be distributed throughout four areas in the City, in accordance with Schedule 3.

Application Procedure

1. Resources for application submission, including detailed instructions and links to the application form, submission requirements and other relevant information, will be posted on the City website at www.cnv.org/cannabis on October 1, 2018.

Policy Name: Recreational Cannabis Retail Policy

Policy Number: 7

2. The City will accept applications between November 19, 2018 at 10:00 am (PST) and November 30, 2018 at 5:00 pm (PST). All applications must include the information identified in Schedule 4 to be considered complete. Initial evaluation will be based on whether required documentation has been submitted. Through the rezoning process, a more in-depth analysis will be completed, at which point additional documents may be required.

Applications will only be accepted online through the City's file transfer service. Submissions will be deemed successfully received once displayed as a new e-mail in the inbox of the City's e-mail address. The order of which applications are received in the City's inbox will be considered the official register of applications received by the City. The City will not be liable for any application submission delay for any reason, including technological delays, or issues with either party's network or e-mail program. The City will also not be liable for any damages associated with submissions not received. The link to the file transfer service will be posted at www.cnv.org/cannabis.

The following rules apply to submissions:

- Only one submission per business/operator per area will be accepted;
 - Should the City receive multiple submissions from the same business/operator for the same location, only the first received submission will be considered;
 - A submission containing more than one application will not be considered and will be disqualified; and,
 - A submission containing more than one proposed location will not be considered and will be disqualified;
3. After the intake period, staff will review applications on a "first-come, first-serve" basis for application completeness. Applications that are deemed incomplete will not be further considered and disqualified.
 4. The City will consider a total of up to six (6) business locations, distributed throughout areas of the City in accordance with Schedule 3. The first two (2) complete applications received in Areas 1 and 2 and the first complete applications received in Areas 3 and 4 will be eligible to submit a rezoning application.
 5. All applications that proceed to the rezoning stage must submit an application fee of \$4,026.25.

Policy Name: Recreational Cannabis Retail Policy

Policy Number: 7

6. Staff will process the first set of rezoning applications as a single batch if possible. All rezoning applicants must follow standard procedures for the rezoning process, including the following:
 - a. Responding to feedback and comments from staff, applicable advisory bodies, and the public;
 - b. Organizing an open house session and conducting other applicable public notification and consultation measures; and,
 - c. Attending standard Council proceedings, including a Public Hearing, when scheduled.

Should an applicant withdraw from the rezoning process during this stage, the applicant(s) who had submitted the next complete application in the same area, as defined in Schedule 3, will be eligible to submit a rezoning application.

7. All applications will be held at Third Reading pending confirmation of approval for a Provincial Non-Medical Cannabis Retail License. Should an application at Third Reading not receive Provincial approval, the next complete application in the same area, as defined in Schedule 3, will be eligible to submit a rezoning application.
8. Upon rezoning approval, applicants may submit a business license application and, if applicable, a building permit application to the City.
9. Once operational, staff will monitor businesses to keep Council informed on the state of recreational cannabis retail in the City. After a period of two years, staff will reassess the Policy to determine whether amendments are warranted and/or if additional stores should be considered in the City, and return to Council with a recommendation for next steps. Council may request an alternative timeline for staff reassessment of the Policy at their discretion.
10. Should the City not receive the appropriate number of complete applications for each area, as defined in Schedule 3, during the timeframe outlined above in the Policy, future applications will be accepted on a 'first come, first serve' basis that are consistent with the requirements of this Policy.

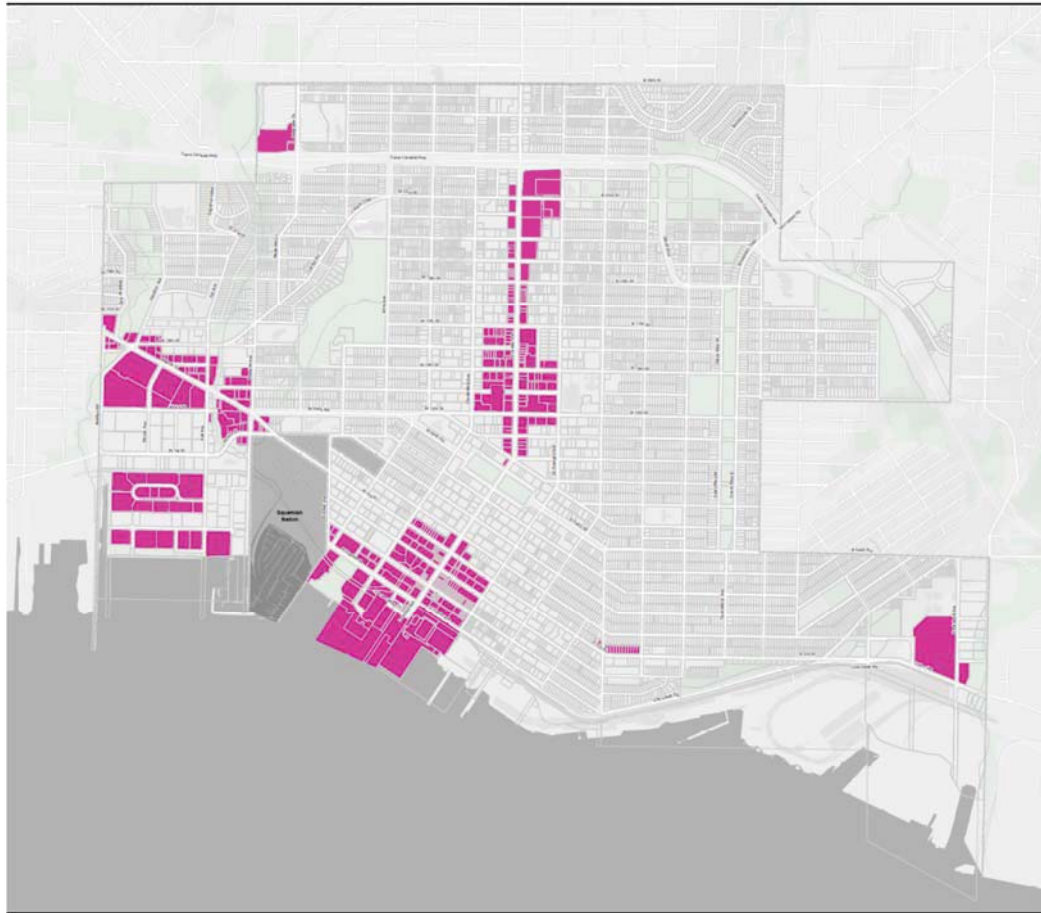
Approval date:	September 24, 2018	Approved by:	Council
Effective date:		Revision date:	

Policy Name: Recreational Cannabis Retail Policy




Policy Number: 7

Schedule 1

Permitted OCP Land Use Designations for Recreational Cannabis Retail Business Locations



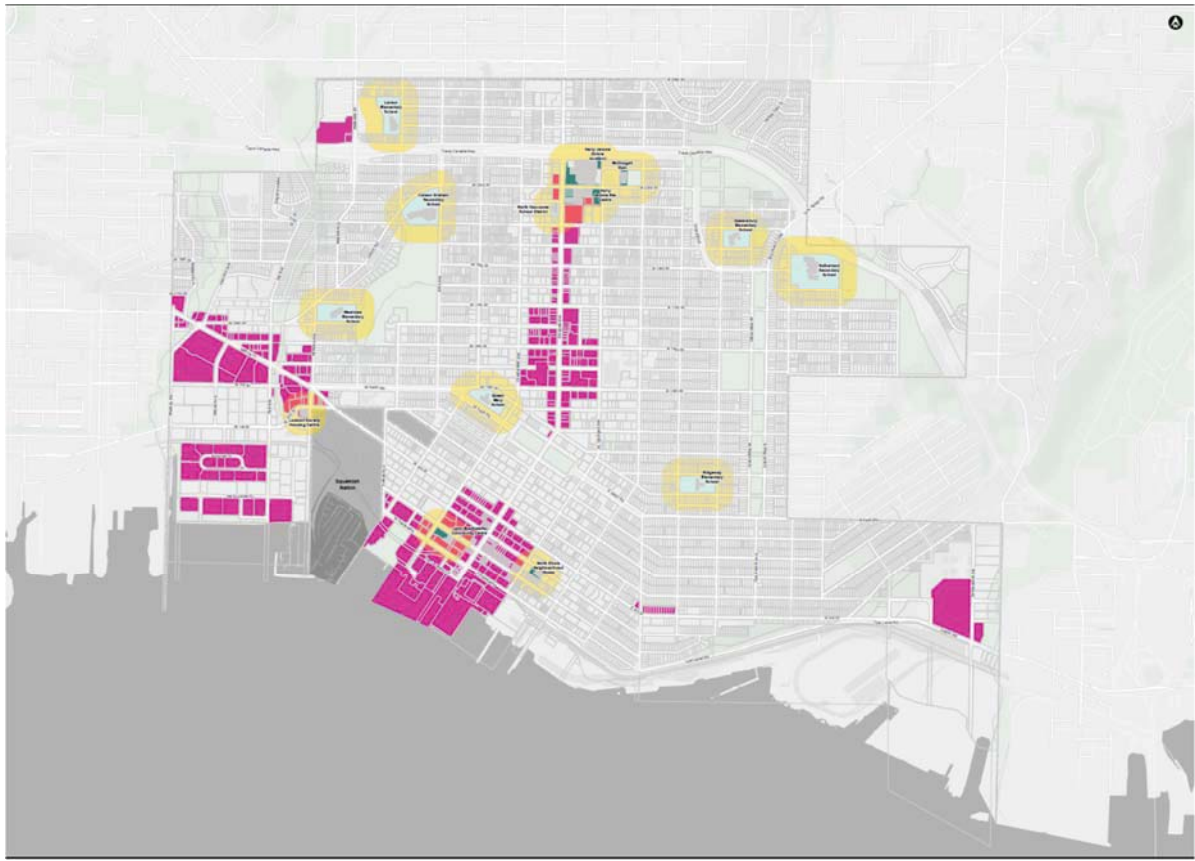
Legend

-  Permitted OCP Land Use Designations
-  Legal Parcels
-  City Boundary









Policy Name: Recreational Cannabis Retail Policy

Policy Number: 7

Schedule 2 Recreational Cannabis Retail Businesses 100-metre Radius Separation Requirement from Sensitive Uses



Legend

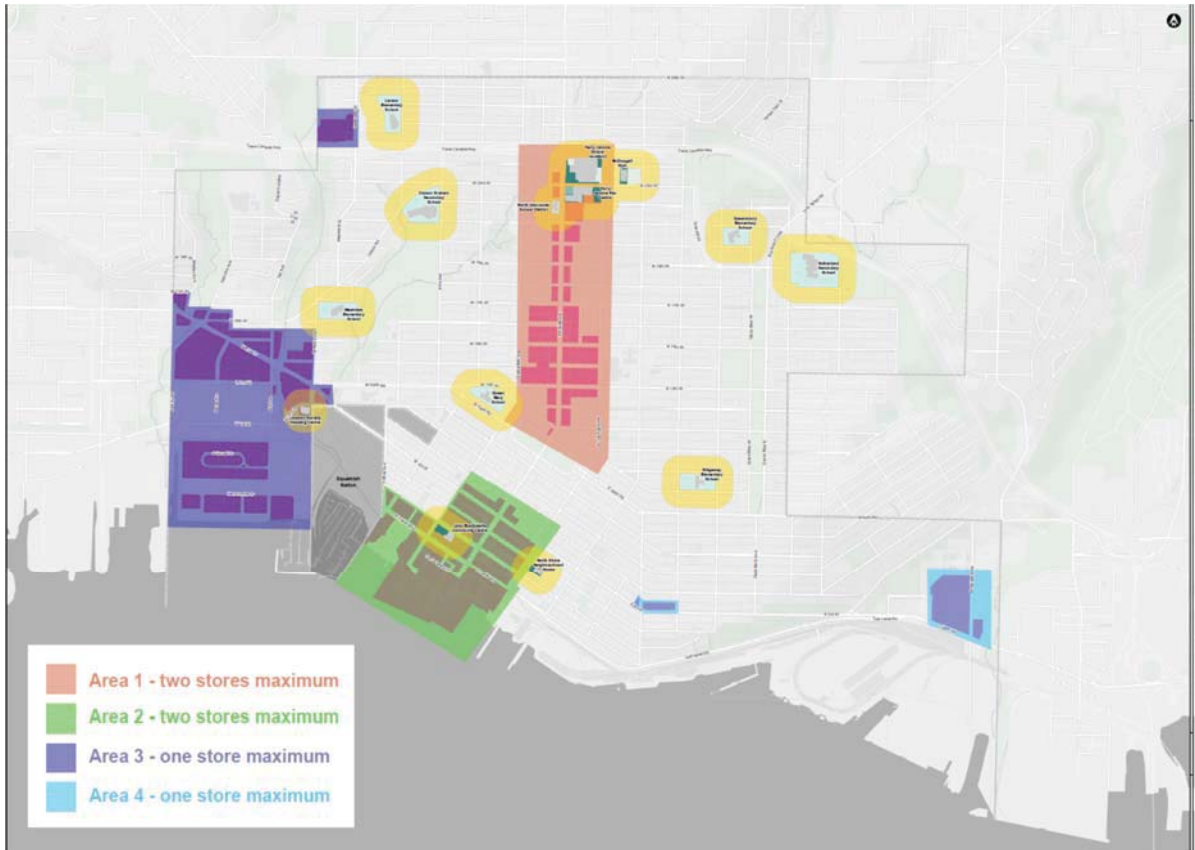
-  Sensitive Use Buildings
-  School/ School District
-  Lookout Society
-  Community/ Recreation Centres
-  100m exclusion buffer *
-  OCP Land Use Designation
-  Legal Parcels
-  City Boundary

* buffers have been measured from legal parcel boundaries

Policy Name: Recreational Cannabis Retail Policy

Policy Number: 7

Schedule 3
Recreational Cannabis Retail Businesses
Areas of Consideration



Policy Name: Recreational Cannabis Retail Policy**Policy Number: 7**

Schedule 4
Recreational Cannabis Retail Businesses - Submission Requirements

REQUIRED INFORMATION / DOCUMENTATION	YES	NO
Name and contact information of applicant(s)		
Civic address and legal description of property where the proposed business is to be located		
Letter of consent signed by the subject property strata corporation or registered owner(s) authorizing application submission and proof of an option for lease or purchase of the subject property; or a State of Title Certificate confirming the ownership of the subject property		
Confirmation that the proposed location complies with all locational requirements as per this policy		
Full description of the proposed business operation, including, at minimum, the following information: <ul style="list-style-type: none">• past business experience• corporate structure• number of staff, products sold, target market, and hours of operation• other general business information		
Letter confirming that all individuals/corporate entities associated with the proposed business are <u>not</u> currently operating any illegal recreational cannabis operations in the City of North Vancouver		
Description of the expected time frame for commencing business activities within the City, if approved		
A Community Impact Statement outlining the following: <ul style="list-style-type: none">• Potential positive and negative economic, social and environmental impacts the business may have on the immediate neighbourhood and wider community• Strategies for mitigating potential negative impacts		
A Security Plan demonstrating security features that comply or exceed Provincial requirements		
Provincial licensing referral from Liquor and Cannabis Regulation Branch (required to be received from Province)		



**115 NORTH VANCOUVER –VIRTUAL OPEN HOUSE
OCTOBER 1, 2020
HOSTED BY: EGGS CANNA**

PARTICIPANTS:

There were no participants for this meeting.

COMMENTS:

There were no comments on this proposed development application.

NOTIFICATION MATERIALS:

- a. Mailout letter was drafted & delivered via IDRS
- b. Newspaper advertisement was taken out in the North Shore News
 - i. One was posted on September 23rd, 2020
 - ii. Second ad was posted on September 30th, 2020
- c. Sign was posted in the window of site per City requirements

****copies & photos of above noted notifications have been included in this report****

COMMUNITY CONCERNS:

There were no community concerns with this application during the Virtual Open House.

SUMMARY:

Eggs Canna has done two rounds of neighbourhood notifications prior to the above noted "Virtual Open House," and prior to Covid-19. At that time Eggs Canna had gone to local business in the surrounding area on foot and introduced us and what we wanted to do with the proposed site. The second round was a similar door-to-door introduction, but we focused on the residents in and around our proposed area.

We were met with enthusiasm for the proposed development and received letters of support and endorsement that were included in the original application package.

We had comments regarding parking that we were able to satisfy by outlining that there is a parking lot across the street and additional street parking in front of the store. We also conducted an internal survey at our existing stores to understand how many of our customers either drive, walk, bus, or use other methods of transportation. We were able to conclude that two parking stalls with a maximum stay of 30-minutes would be sufficient to service our existing locations and we could confidently apply those statistics to this location.

The community was satisfied with our application and we received approval from the vast majority of businesses and residents we canvassed.

Respectfully,

Andrew Cappellano
Eggs Canna



Rezoning Application for Cannabis Retail Store 115 East 1st Street

Presented December 7, 2020
Planning Department



Background

- October 17, 2018
 - Recreational Cannabis legalized
- Federal Government
 - Cannabis Act
- Government of British Columbia
 - Cannabis Control and Licensing Act
- City of North Vancouver
 - Recreational Cannabis Retail Policy

Recreational Cannabis Retail Policy

- September 17, 2018
 - Council approved the way recreational cannabis could be distributed in the City.
 - Six retail stores maximum.
 - Recreational grade only.
 - Medicinal cannabis through Federal licensed producers only.

Recreational Cannabis Retail Policy

- Location Criteria (OCP)
 - Mixed-Use and Commercial land use designations:
 - 2 in Central Lonsdale;
 - 2 in Lower Lonsdale;
 - 1 in the West area of the City; and
 - 1 in the East area of the City.

Recreational Cannabis Retail Policy

- Separation from Sensitive Uses (100 metres).
 - Community Recreation Centers.
 - North Shore Neighbourhood House.
 - North Shore Shelter.
 - North Vancouver School District Office.
 - Public elementary and secondary schools.

Recreational Cannabis Retail Policy

- Recreational Cannabis Sales currently not allowed anywhere in the City:
 - Requires a rezoning application.
 - Would permit Cannabis Sales use on that particular property (site specific).
 - Approval or refusal of rezoning would form the Municipal response to the applicant's request for a Provincial cannabis retail license.

Urban Design – Street Front

- Federal and Provincial regulations.
- Provincial relaxation of street front requirements to allow viewing into the stores, but limit product displays.

Previous Applications

Two approved in July, 2019

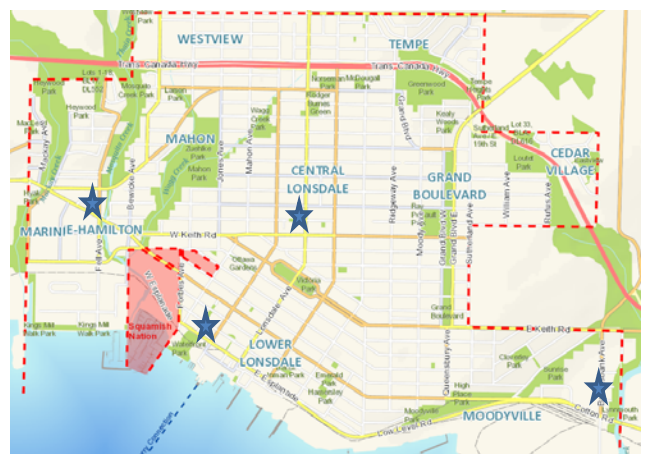
221 West 1st Street (open)

333 Brooksbank Avenue

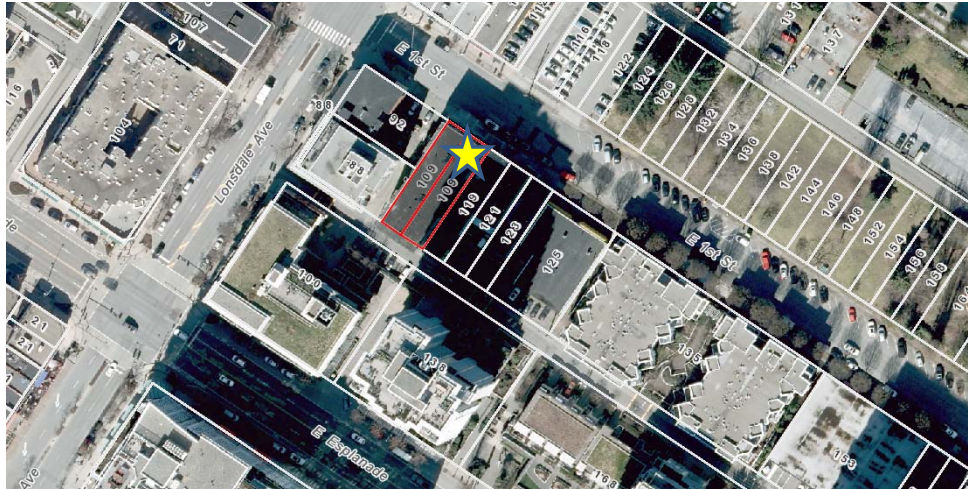
Two approved in February, 2020

820 Marine Drive (open)

1433 Lonsdale Avenue



115 East 1st Street



115 East 1st Street

- OCP designation of Mixed-Use Level 4A.
- Last location in the Lower Lonsdale area.
- 163.7 sq. metre (1,762 sq. foot) retail unit located facing East 1st Street.
- Located within a mixed-use building that was recently designated as a Heritage Building.
- Minimum of two off-street parking stalls required.
 - parking waived for this site
- Outside of the sensitive use areas.
 - John Braithwaite Community Centre (190 metres)
- Meets the OCP and the Recreational Cannabis Retail Policy.
- Needs a Zoning Bylaw amendment to add Cannabis Sales.

115 East 1st Street

- March 30, 2020
 - Council adopted a Heritage Revitalization Agreement Bylaw for the building that included zero off-street parking stalls for the property.
 - Also supported an encroachment agreement to support things like awnings to encroach onto City road allowances.

115 East 1st Street

- Community Consultation
 - Virtual Developer Information Session was held on October 1, 2020. Notifications were done in accordance with City policy.
 - No one pre-registered and no one attended the VDIS.
 - Applicant pre-canvased the area before making the application.

Recreational Cannabis Retail Policy

- The policy indicated that staff reassess the policy two years after it was adopted to determine whether amendments would be warranted.
 - Proceed with monitoring and policy review and report back to Council (Recommended);
 - Direct staff to modify the Recreational Cannabis Retail policy per Council's direction.

A photograph of a small, clear stream flowing over mossy rocks in a lush forest. The water is shallow and reflects the surrounding greenery. The text "Thank you." is overlaid on a green rectangular background in the lower-left portion of the image.

Thank you.

EGGS

NORTH VANCOUVER

MAYOR and COUNCIL PUBLIC
HEARING

DECEMBER 07, 2020

CANNA

Cannabis Boutique

PRESENTED BY **OANA NICOARA**,
PRESIDENT ECG HOLDINGS LTD.

PUBLIC HEARING

First and foremost I would like to extend a sincere thank you to Mayor and Council, and the residents for allowing us time today to discuss our proposed Cannabis Retail store.

Eggs Canna understands that operating in your city and community is a privilege, and as such we will carry on business with the communities best interest and well-being at the forefront.

I am excited to discuss our operations, parking and the design of our Cannabis Boutiques and answer any questions Council and neighbours may have.

Thank you.



ABOUT EGGS CANNA

RESPONSIBLE REGULATED DISTRIBUTION—we understand our role is to introduce cannabis to communities in a socially responsible manner.

Eggs Canna brings hands-on cannabis sales experience under a licensed Municipal and Provincial framework; this has been instrumental in developing some of the most comprehensive SOP's in-line with Provincially-mandated standards.

Eggs Canna has successfully opened and currently operates 5 licensed retail stores with no complaints or violations at any of our locations to date.

As part of our past and continued operations we foster long-lasting relationships with our communities and local businesses. EC has been involved in, and worked with; local charities, BIA's, community programs such as "car free days" and many other social gatherings.

ABOUT EGGS CANNA
P 2



COMMUNITY FIRST IS IMPORTANT TO US

Operating with great respect for our communities, Eggs Canna strives to have a lasting positive impact. EC looks to peer-reviewed studies done across the U.S. that demonstrate how legal cannabis stores add value and safety.

1. Cannabis retailers bring thorough **security measures** such as video cameras, advanced locks, safes and other means increasing guardianship ergo deterring criminal activity.¹
 - i. It has been documented in numerous crime-related studies that businesses well equipped with these added measures **increase property values by deterring crime in the area.**
2. Retail outlets **generate more foot traffic**, creating more "eyes on the street", thereby making the neighborhood safer.²
3. The presence of licensed retail stores **decrease street level drug dealing** and decrease the collateral effect on other criminal behavior in the neighbourhood.³
4. Most importantly, **access to cannabis by youth drops significantly** in areas that have adequate licensed distribution.⁴
5. Cannabis retail **brings tourism and drive sales** for surrounding businesses.

¹ Nancy Kepple and Bridget Friesthler, "Exploring the Ecological Association Between Crime and Medical Marijuana Dispensaries", 2012, Journal of Studies on Alcohol and Drugs.

² Tom Chang and Mirielle Jacobson, "Going to Pot? The Impact of Dispensary Closures on Crime", 2017, Journal of Urban Economics.

³ Yu-Wei Luke Chu and Wilbur Townsend, "The Effects of Medical Marijuana Laws on Crime", 2018, Journal of Economics Behavior and Organization.

⁴ D. Mark Gerson, Benjamin Hansen, Daniel Rees, "Association of Marijuana Laws with Teen Marijuana Use: New Estimates from the Youth Risk Behavior Studies", 2019, JAMA

COMMUNITY FIRST
P 3



SAFE RETAILING OF CANNABIS PRODUCTS

Eggs Canna understands the importance of maintaining high standards of safety and service. Here are just *some* of the ways we will dispense cannabis responsibly:

1. All employees must have their **"Selling It Right" (SIR)** to work in our establishment. SIR is a government course that teaches employees how to sell legal cannabis responsibly and gives staff the tools needed to make the right calls in all situations, including when dealing with minors or intoxicated persons.
2. Employees undergo a security screening process and must have their **"Cannabis Worker Qualification" (CWQ)** prior to employment.
3. Eggs Canna has pioneered industry-leading training for staff to ensure that laws are upheld and we serve our communities with their safety at the forefront.
4. EC staff have mandated litter and safety checks performed on a timed basis, ensuring minors are not loitering nearby, that there is no cannabis consumption in and around the site and that any litter is actively being picked up, keeping our communities clean and beautiful.
5. Eggs Canna is committed to paying its staff competitive wages alongside offering full medical and dental benefits—keeping jobs local and our communities well employed.

SAFE DISPENSING
P 4



LOWER LONSDALE NEEDS ANOTHER CANNABIS RETAILER TO MEET EXISTING and GROWING DEMAND

Extrapolating from current operations, we theorize that the current Lower Lonsdale store located at 223 W 1st St, is not able to service the growing demand and population growth in and around Lower Lonsdale. A cursory search for developments in and around the area demonstrates the enormous growth trend happening over the coming years. Here are some large projects just completed or slated for completion over the next 1-2 years:

1. 131 East 3rd Street—**56 NEW Residential Units**
2. 101-149 Lonsdale Ave—**64 NEW Residential Units**
3. 177 Victory Ship Way —**113 NEW Residential Units**
4. West Quay —**208 NEW Residential Units**
5. 119-131 West Esplanade and 120 Carrie Cates Court —**15 Storeys of NEW Residential Units**

Just this year, the Moodyville area has seen numerous multi-family redevelopments, some of these projects include:

6. Kindred —**98 NEW Residential Units**
7. Green on Queensbury —**164 NEW Residential Units**
8. Morrison —**40 NEW Residential Units**
9. Evolv —**35 NEW Residential Units**

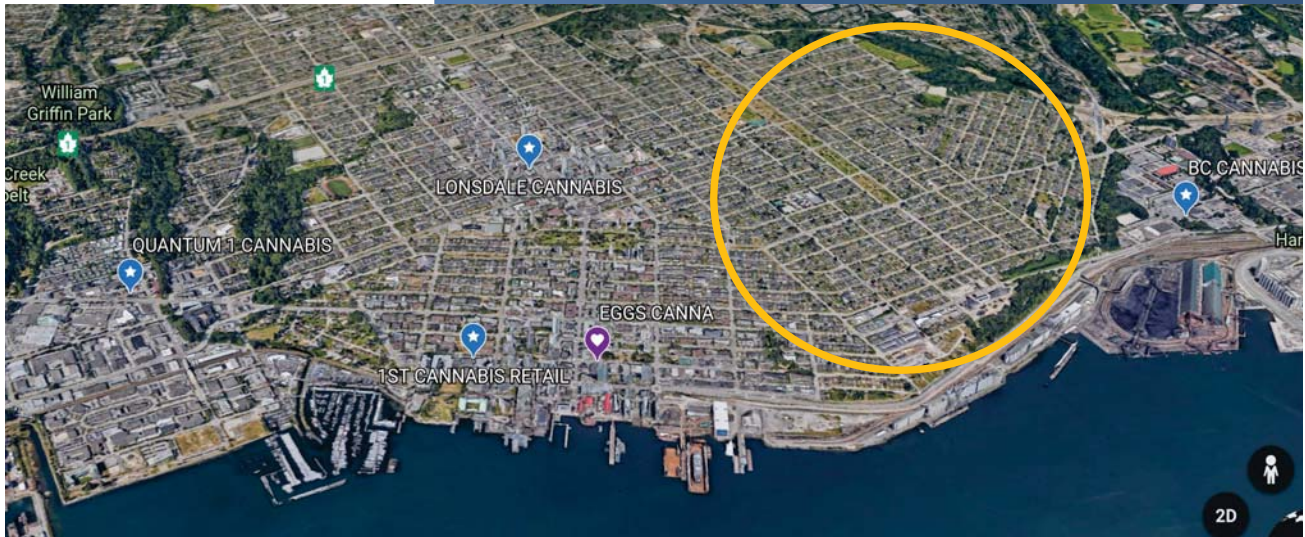
There will be a total of **778 NEW** units and an additional **15 storeys of NEW residential units** over the coming two years.

LOWER LONSDALE
P 5

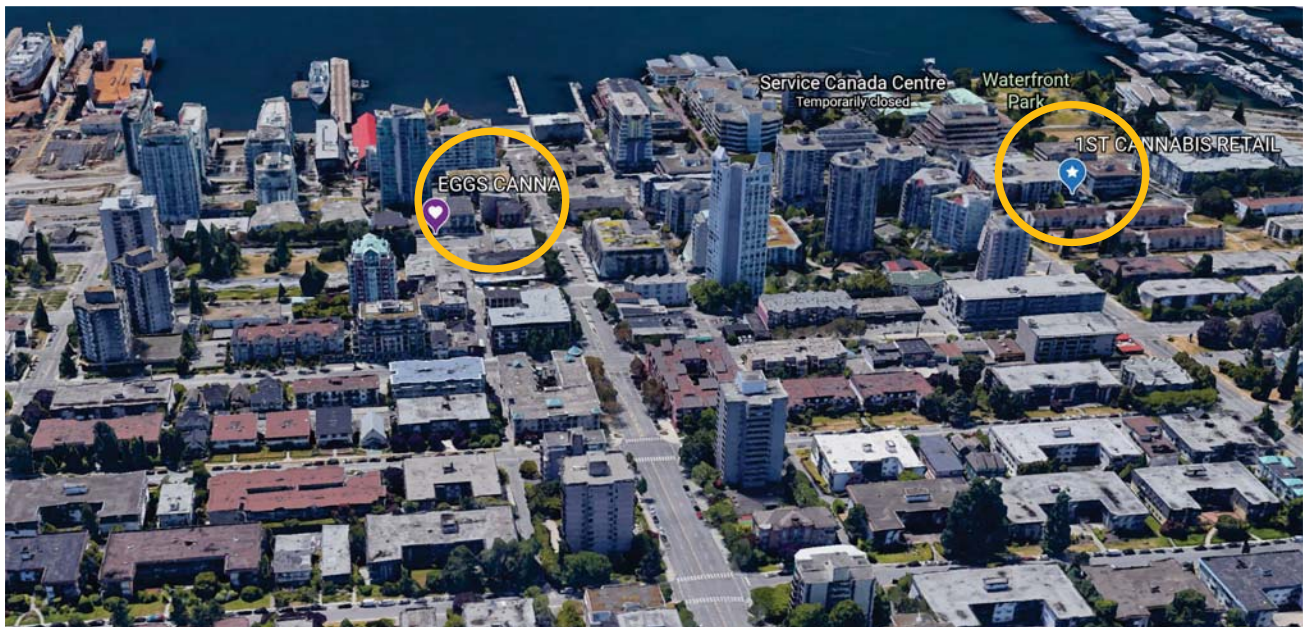


Based on our experience, the area highlighted below is too large of a surface area to be serviced by the existing cannabis retailers, especially taking into consideration 1st Cannabis only has 2 tills. Lonsdale Cannabis is located in another densely populated area of the city. From there Quantum 1 Cannabis is too far to service the highlighted area below, and the BC Cannabis store will draw customers who are commuting by car not by foot as the greenbelt and lack of sidewalks will deter anyone on foot.

By comparison, Eggs Canna will offer 6 tills with an additional 3 portable tills and cabinetry specifically built for Click & Collect; further expediting service and reducing parking requirements while virtually eliminating long lineups.



P 6



To my right you can see 1st Cannabis which has two tills and operates in an approximately 150 square foot space. 1st Cannabis is located in the Lower Lonsdale area, which is heavily populated and busy. With two tills, 1st Cannabis is presently not able to accommodate the needs of this area, let alone the future growth expected to be seen in the coming months and years.

EGGS CANNA will offer much needed support in servicing this area, especially during the busy summer months which brings tourism and other attractions.

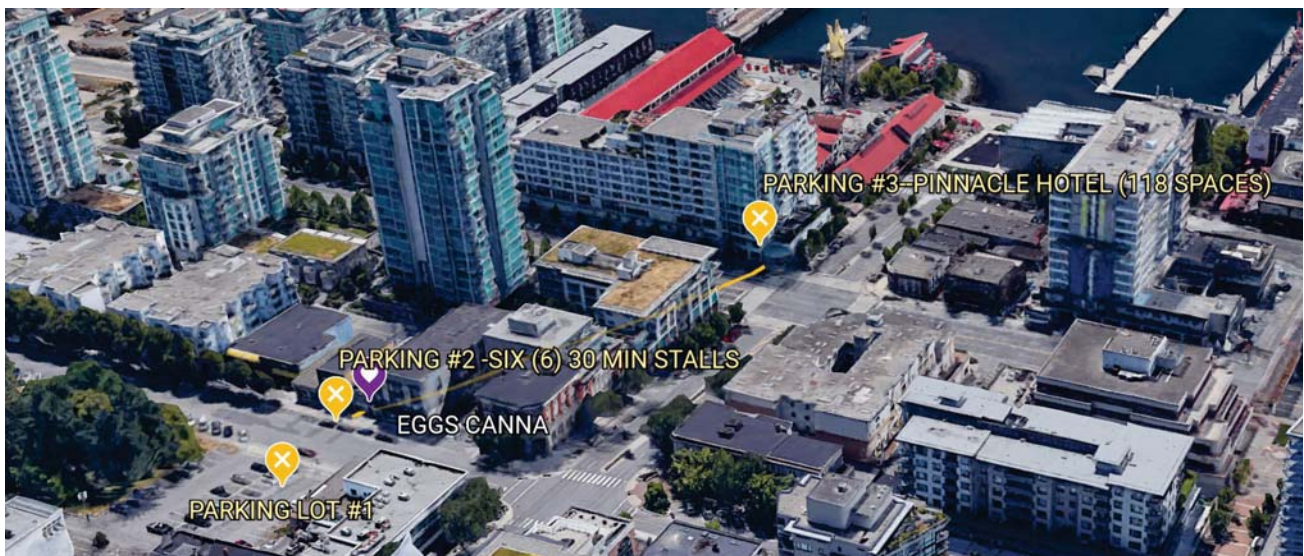
P 7

PARKING and TRAFFIC

Questions regarding parking and foot traffic have been raised by residents as well as Council, as this is a new industry there are many unknown factors at play. To shed some light on this topic, we conducted a survey using our current retail locations over the span of 3 weekends and alternating between those locations, we discovered the following trends:

1. Cannabis Retail stores bring in local customers who live or work within a 15-minute radius.
2. Stores do in fact drive foot traffic—we did not observe an exaggerated spike in car traffic when patrons were served in an efficient manner that did not cause line-ups.
3. Taking the average of those who drove, coupled with the average length of time spent in the store on a 14-hour “open” cycle, we noted approx. 1-2 driving customers every 20-25 minutes. To have adequate parking for this volume of traffic Eggs Canna would require two parking stalls with 30-minute maximum parking for each.
4. Click & Collect: With the launch of this service and up to 9-tills running at the same time, we are able to serve customers efficiently and fast; significantly reducing or eliminating lineups. EC will have the ability to grow with the area and continue to service the needs of residents for the foreseeable future with our existing location size and the number of tills.
5. Environmentally Friendly Patron Program: Eggs Canna is aware of our businesses impact on our neighbours and our carbon footprint, as such, we have created innovative and unique ways to encourage more foot traffic and less car congestion. Working closely with our partners at TechPOS, EC is excited to launch this new discount class to those who walk, bike, rollerblade etc. These discounts are validated at time of departure through our POS system with North Vancouver being the Pilot Project!

PARKING and TRAFFIC P 8

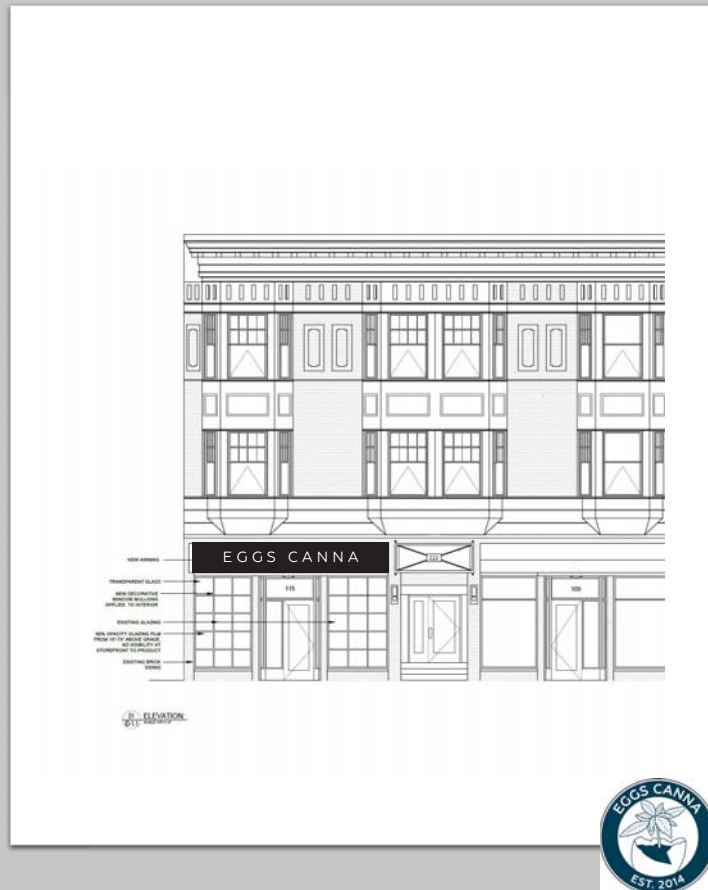


As demonstrated in this aerial view, Eggs Canna has 6 public parking stalls in front of the store, a large public parking lot across the street with the farthest lot being less than 125 meters away and offering a sprawling 4-level parkade holding 118 spaces.

Although 1st Cannabis has dedicated stalls, the physical size of the store causes excessive wait times resulting in long lineups, and having a counterintuitive effect on the parking— the overflow of customers must find other parking in and around the area.

EC will offer this area much needed relief from this congestion for those looking to purchase cannabis, especially during peak rush-hour times and the busier seasons.

LET'S TALK DESIGN!



P 10



We understand the importance of shifting old stigmas, and complementing the neighbourhoods we operate in. A timeless exterior design featuring wainscoting and mullions give a warm Victorian *je ne sais quoi* –fitting in with older and newer communities.

EC was happy to hear that mandated window coverings are no longer a Provincial requirement, and is excited to showcase our beautiful stores from the exterior and interior.

KEY DESIGN ELEMENTS

P 11





OUR **CANNASEURS** ARE
EXCITED
TO SERVE THE **NORTH SHORE!**



BEAUTIFUL DESIGN—BUILT FOR FUNCTIONALITY



P 12

GRAND OPENING MARCH 2021



If we are successful with our application, construction is slated to begin in **January 2021** with our grand opening slated for March 2021.



Eggs Canna has 8 Provincial licenses and 5 operational stores, having now **perfected our design and build-outs.**



We are very **excited and ready to serve the North Vancouver area!**

Eggs Canna has a strong proven design concept with historical ability to execute on builds in a timely manner.

GRAND OPENING
P 13



THANK YOU!

I look forward to answering all your questions.

Q and A



From: Hans1 <>
Sent: November-26-20 1:54 PM
To: Submissions
Subject: Zoning bylaw, 1995, no. 6700 amendment bylaw, 2020 no. 8808 - 115 East 1st Street

NOT IN FAVOUR

I am NOT in favour of this amendment. There is already a cannabis store on first st just 2 to 3 blocks west near Sailor Hagars brew pub. Already to many in the city.

Hans Stripp
2003 - 138 East Esplanade
North Vancouver

From: jane ablett
Sent: November-26-20 4:56 PM
To: Submissions
Subject: Proposed Cannabis Shop - 115 East 1st St

Greetings

I would like to express my opposition to the proposed cannabis shop at 115 East First Street in Lower Lonsdale.

As there is a viable option a short walk away, it seems like a poor location choice.

Regards,

Jane Ablett
138 East Esplanade
North Vancouver

From: Laurette Drolet <>
Sent: November-27-20 7:25 AM
To: Submissions
Subject: Zoning bylaw, 1995, no. 6700 amendment bylaw, 2020 no. 8808 - 115 East 1st St

NOT IN FAVOUR

I am NOT in favour of this amendment. There is already a cannabis store on first st just 2 to 3 blocks west near Sailor Hagar's. Already to many in the city.

Laurette Drolet
2003-138 East Esplanade
North Vancouver

From: Amir Chitsaz
Sent: November-28-20 3:58 PM
To: Submissions
Subject: Zoning amendment bylaw, 2020, No.8808 | Eggs Cana - 115 East 1st Street

Dear Council,

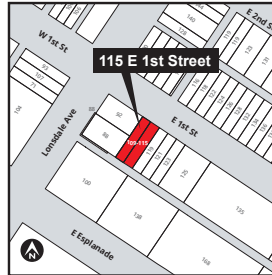
We are writing to inform you that we are voting **against** below rezoning:

- Rezoning of the subject property from Lower Lonsdale 2 (LL-2) Zone to Comprehensive Development 730 (CD-730) Zone to allow a Cannabis Retail Store on the property

Regards,

Amir Chitsaz & Golsa Hajebi
1102 - 138 East Esplanade
North Vancouver, BC V7L 4X9

WHO: Eggs Cana
WHAT: Zoning Bylaw, 1995, No. 6700,
 Amendment Bylaw, 2020, No. 8808
WHERE: **115 East 1st Street**
WHEN: Monday, December 7, 2020
 at 5:30 pm
HOW: View the meeting online at
cnv.org/LiveStreaming



Notice is hereby given that Council will consider:

Zoning Amendment Bylaw, 2020, No. 8808
 to rezone the subject property from the Lower
 Lonsdale 2 (LL-2) Zone to a Comprehensive
 Development 730 (CD-730) Zone to allow a Cannabis
 Retail Store on the property.

As City Hall remains closed to the public, the Public Hearing will be held electronically via "WebEx". All persons who believe their interest in property may be affected by the proposed bylaw will be afforded an opportunity to speak at the Public Hearing and/or by email or written submission. To ensure all submissions are available for Council at the Public Hearing, certain deadlines have been implemented.

For email submissions (preferred): Include your name and address and send to input@cnv.org **no later than 12:00 noon on Monday, Dec. 7, 2020.**

For written submissions: Include your name and address and mail or deposit into a drop-box at City Hall **no later than 4:00 pm on Friday, Dec. 4, 2020**, as documents are subject to a 24-hour quarantine period before being opened due to COVID-19.

To speak at the Public Hearing by phone:
 Pre-register by completing the online form at cnv.org/PublicHearings or provide contact information by phone to Julie Peters at 604-990-4230 **no later than 12:00 noon on Monday, December 7, 2020.**

Speakers who have not pre-registered will also have an opportunity to provide input at cnv.org/PublicHearings. Call-in details will be displayed on-screen at the Public Hearing (watch web livestream). Once all registered speakers have provided input, the Mayor will call for a recess to allow additional speakers time to phone in.

Once the Public Hearing has concluded, no further information or submissions can be considered by Council.

The proposed Zoning Amendment Bylaw, background material and presentations of staff and the applicant will be available for viewing online at cnv.org/PublicHearings on Friday, Nov. 27, 2020.

Please direct any inquiries to David Johnson, Development Planner, at djohnson@cnv.org or 604-990-4219.

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THE CORPORATION OF THE CITY OF NORTH VANCOUVER

BYLAW NO. 8808

A Bylaw to amend “Zoning Bylaw, 1995, No. 6700”

The Council of The Corporation of the City of North Vancouver, in open meeting assembled, enacts as follows:

1. This Bylaw shall be known and cited for all purposes as “**Zoning Bylaw, 1995, No. 6700, Amendment Bylaw, 2020, No. 8808**” (Eggs Cana / Oana Nicoara, 115 East 1st Street, CD-731).
2. Division VI: Zoning Map of Document “A” of “Zoning Bylaw, 1995, No. 6700” is hereby amended by reclassifying the following lots as henceforth being transferred, added to and forming part of CD-731 (Comprehensive Development 731 Zone):

Lot	Block	D.L.	Plan	
5	166	274	878	from LL-2

3. Part 11 of Division V: Comprehensive Development Regulations of Document “A” of “Zoning Bylaw, 1995, No. 6700” is hereby amended by:
 - A. Adding the following section to Section 1100, thereof, after the designation “CD-730 Comprehensive Development 730 Zone”:

“CD-731 Comprehensive Development 731 Zone”
 - B. Adding the following to Section 1101, thereof, after the “CD-730 Comprehensive Development 730 Zone”:

“CD-731 Comprehensive Development 731 Zone”

In the CD-731 Zone, permitted Uses, regulations for permitted Uses, regulations for the size, shape and siting of Buildings and Structures and required Off-Street Parking shall be as in the LL-2 Zone, except that:

- (1) In addition to the Principal Uses permitted in the LL-2 Zone, one Cannabis Sales retail store may be permitted.

READ a first time on the 16th day of November, 2020.

READ a second time on the 16th day of November, 2020.

READ a third time on the <> day of <>, 2020.

ADOPTED on the <> day of <>, 2020.

MAYOR

CORPORATE OFFICER



2021 Utility Rate Setting

Guiding Principle

Revenues collected must provide adequate funds for:

- Operating & maintenance expenditures
- Infrastructure renewal and growth
- Adequate capital reserves

2021 Single Unit Utility Rates

Utility	2020 Rate	2021 Rate	\$ Increase	% Increase
Water	\$489.00	\$514.00	\$25.00	5.1%
Sanitary Sewer	\$411.00	\$444.00	\$33.00	8.0%
Drainage*	\$107.86	\$113.25	\$5.39	5.0%
Solid Waste*	\$285.90	\$309.80	\$23.90	8.4%
Total Single Unit	\$1,293.76	\$1,381.05	\$87.29	6.7%

*Estimated

- Water and Sewer rate increases are primarily driven by:
 - Water Purchase and Sewer Levy costs from Metro
 - Capital projects to replace aging infrastructure
 - Building of reserves to offset future Metro increases
- Solid Waste rate increase is primarily driven by:
 - Reducing use of reserves as a funding source.
 - Increased tipping fees from Metro

2021 Multi-Unit Utility Rates

Utility	2020 Rate	2021 Rate	\$ Increase	% Increase
Water	\$286.00	\$301.00	\$15.00	5.2%
Sanitary Sewer	\$256.00	\$277.00	\$21.00	8.2%
Drainage*	\$48.83	\$51.27	\$2.44	5.0%
Solid Waste*	\$22.00	\$25.30	\$3.30	15.0%
Total Single Unit	\$612.83	\$654.57	\$41.74	6.8%

*Estimated

North Shore Comparative

The District of North Vancouver and the District of West Vancouver had their 2021 Utility Rates approved by their respective Councils on November 16, 2020

2021 SFH	CNV	DNV	DWV
Water	\$514.00	\$772.00	\$764.00*
Sewer	557.25*	735.80	1,186.00*
Solid Waste	309.80*	319.60	252.43
Total Single Unit	\$1,381.05	\$1,827.40	\$2,202.43

2021 SFH (change)	CNV	DNV	DWV
Water	\$25.00	\$33.25	\$42.00*
Sewer	38.39*	35.05	90.00*
Solid Waste	23.90*	4.75	20.04
Total Single Unit	\$87.29	\$73.05	\$152.04

*Estimated

Looking Back

5-year Combined Single Unit Utility Rates

	2016	2017	2018	2019	2020	2021
Water	10.0%	3.2%	3.1%	5.6%	4.2%	5.1%
Sanitary Sewer	3.3%	4.8%	11.1%	11.9%	7.1%	8.0%
Drainage*	3.0%	16.0%	3.0%	12.0%	4.0%	5.0%
Solid Waste*	(5.4%)	(2.1%)	(5.1%)	(6.0%)	(0.4%)	8.4%
Total Single Unit	2.8%	2.9%	2.9%	4.9%	4.0%	6.7%

*Estimated

Water Utility Infrastructure

- 129 km of water mains, 6,600 water connections
- Average pipe age - 51 years; average service life approx. 125 years
- Forecasting significant replacement required of small cast iron pipe
- 3.5% increase in our costs of water from Metro Vancouver; larger increases expected in future



Water Utility – 2021 Increase of 5.0%

Explanation	Increase
Water Purchases increase due to Metro rate increase of 3.5%, offset by a forecasted decrease in consumption	
Impact of Metro Rate increase: 1.8%	1.1%
Impact of reduced volume forecast: <u>(0.7%)</u>	
Net impact to water purchases 1.1%	
Capital Projects/Programs increased by \$0.4M	
Capital Projects (+\$395K): 2.6%	2.4%
Capital Programs (-\$35K) <u>(0.2%)</u>	
Net impact 2.4%	
Transfers to Reserve increased by \$119K, in order to offset future rate increases from Metro	1.4%
Water Operations and Administration increase dues to wages and financial costs	0.1%
2021 Water Utility Rate Increase:	5.0%

Sewerage/Drainage Infrastructure

- 124 km of sanitary sewer mains, 6,800 service connections
- 148 km storm sewer mains, 5,161 service connections
- Sanitary pipes average age 62 years; increased replacement rate required
- Storm system vulnerable to climate change
- North Shore WWTP currently being replaced
- Metro Sewer levy up by 11.0%; significant Metro increases coming



Sewer & Drainage Utility – 2021 Increase of 8.0%

Explanation	Increase
Metro Levy GVS&DD Charges increased by 11.0%	6.1%
<i>Reserves are used to fund large projects in order to off-set rate impacts:</i>	
Capital Projects/Programs increased \$323K	2.7%
Transfers to Reserve (net) decreased by \$138K	<u>(1.1%)</u>
	1.6%
Sanitary & Drainage Operations: Sanitary increase of \$45K, Drainage increase of \$7K	0.3%
Sanitary & Drainage Administration increase dues to wages, offset by decreased Storm Services	0.2%
Storm Drainage Levy increased revenue received through growth	(0.2%)
2021 Sewer Utility Rate Increase:	8.0%

Solid Waste Activities

- Additional multi-stream receptacles will be added to streetscape and parks in 2021 and 2022, in order to account for the increased usage of outdoor space being seen during the COVID-19 pandemic.
- In July 2020 Recycle BC took over the residential recycling on the North Shore
- Cigarette butt collection pilot program began in September of 2020
- Curbside refuse and Green Can collection was provided to 7,907 households
- 2019 refuse disposal rate of 204 kg/household, down 3.3% from 2018
- 2019 "Red Bin" dog waste program in parks diverted 67,320 Litres that would have ended up in the waste stream



2021 Proposed Solid Waste Rates

Single Unit	2020 Rate	2021 Rate	\$ Change	% Change
Refuse	\$115.00	\$119.00	\$4.00	3.5%
Green Can	\$125.00	\$138.00	\$13.00	10.4%
Eco Levy*	\$45.90	\$52.80	\$6.90	15.0%
Total Solid Waste	\$285.90	\$309.80	\$23.90	8.4%

Multi Unit	2020 Rate	2021 Rate	\$ Change	% Change
Eco Levy*	\$22.00	\$25.20	\$3.20	15.0%
Total Solid Waste	\$22.00	\$25.20	\$3.20	15.0%

* Estimate – based on assessed value

Looking Ahead

Significant Future Increases in Regional Rates:

	2021	2022	2023	2024	2025	Avg.
Bulk Water	3.5%	5.2%	6.4%	8.9%	10.8%	7.0%
GVS&DD Levy	11.0%	19.7%	20.6%	18.6%	17.0%	17.4%
Tipping Fees	3.5%	3.4%	5.8%	5.5%	5.2%	4.7%

- Water purchases from Metro Vancouver accounts for 55% of Water Expenditures and are expected to increase by nearly **40% over the next 5 years**.
- The Sewer Levy from Metro Vancouver account for over 50% of Sewer Expenditures and is expected to increase by over **120% over the next 5 years**.

Looking Ahead: 5 year Utility Rates

Option #1	2020	2021	2022	2023	2024	2025
Water	4.2%	5.1%	5.1%	5.0%	6.2%	6.1%
Sanitary Sewer	7.1%	8.0%	10.1%	12.1%	13.1%	13.1%
Drainage*	4.0%	5.0%	5.0%	5.0%	4.0%	4.0%
Solid Waste*	(0.4%)	8.4%	4.1%	4.3%	4.2%	4.3%
Total Single Unit	4.0%	6.7%	6.5%	7.2%	8.1%	8.1%

*Estimated

- Future years make use of reserve balances to offset the substantial increases coming from Metro in terms of water purchase costs and the Sewer Levy.

Looking Ahead – Rate Increase Options

➤ 5-year combined Single Unit Utility Rates.

Option #1	2020	2021	2022	2023	2024	2025
Water	4.2%	5.1%	5.1%	5.0%	6.2%	6.1%
Sanitary Sewer	7.1%	8.0%	10.1%	12.1%	13.1%	13.1%
Drainage*	4.0%	5.0%	5.0%	5.0%	4.0%	4.0%
Solid Waste*	(0.4%)	8.4%	4.1%	4.3%	4.2%	4.3%
Total Single Unit	4.0%	6.7%	6.5%	7.2%	8.1%	8.1%

Option #2	2020	2021	2022	2023	2024	2025
Water	4.2%	3.1%	5.2%	5.1%	8.1%	8.1%
Sanitary Sewer	7.1%	5.1%	12.0%	13.0%	14.1%	15.1%
Drainage*	4.0%	5.0%	5.0%	5.0%	5.0%	4.0%
Solid Waste*	(0.4%)	8.4%	4.1%	4.3%	4.2%	4.3%
Total Single Unit	4.0%	5.0%	7.1%	7.5%	9.1%	9.6%

*Estimated

2021 Single Unit Utility Rates

Option #1	2020 Rate	2021 Rate	\$ Increase	% Increase
Water	\$489.00	\$514.00	\$25.00	5.1%
Sanitary Sewer	\$411.00	\$444.00	\$33.00	8.0%
Drainage*	\$107.86	\$113.25	\$5.39	5.0%
Solid Waste	\$285.90	\$309.80	\$23.90	8.4%
Total Single Unit	\$1,293.76	\$1,381.05	\$87.29	6.7%

Option #2	2020 Rate	2021 Rate	\$ Increase	% Increase
Water	\$489.00	\$504.00	\$15.00	3.1%
Sanitary Sewer	\$411.00	\$432.00	\$21.00	5.1%
Drainage*	\$107.86	\$113.25	\$5.39	5.0%
Solid Waste	\$285.90	\$309.80	\$23.90	8.4%
Total Single Unit	\$1,293.76	\$1,359.05	\$65.29	5.0%

2021 Multi-Unit Utility Rates

Option #1	2020 Rate	2021 Rate	\$ Increase	% Increase
Water	\$286.00	\$301.00	\$15.00	5.2%
Sanitary Sewer	\$256.00	\$277.00	\$21.00	8.2%
Drainage*	\$48.83	\$51.27	\$2.44	5.0%
Solid Waste	\$22.00	\$25.30	\$3.30	15.0%
Total Single Unit	\$612.83	\$654.57	\$41.74	6.8%

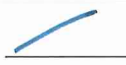


Option #2	2020 Rate	2021 Rate	\$ Increase	% Increase
Water	\$286.00	\$295.00	\$9.00	3.1%
Sanitary Sewer	\$256.00	\$269.00	\$13.00	5.1%
Drainage*	\$48.83	\$51.27	\$2.44	5.0%
Solid Waste	\$22.00	\$25.30	\$3.30	15.0%
Total Single Unit	\$612.83	\$640.57	\$27.74	4.5%

Looking Ahead

- Managing our Utility infrastructure assets to optimize service life and ensure long term system viability
 - Asset Management Strategy
 - Asset Management Plans
- Work with Metro on long range financial planning for regional systems
- Integrated Solid Waste and Resource Management Plan (ISWRMP)
 - Implementation of the region's Zero Waste Challenge
 - Beginning the process of updating the plan over the next three years.
- Working with Metro on a new organics processing contract for the North Shore





 Department Manager	 Director	 CAO
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The Corporation of **THE CITY OF NORTH VANCOUVER**
FINANCE DEPARTMENT

REPORT

To: Mayor Linda Buchanan and Members of Council

From: Ben Themens, Director of Finance

Subject: 2021 UTILITY RATES

Date: November 25, 2020 File No: 05-1820-01-0001/2021

The following is a suggested recommendation only. Refer to Council Minutes for adopted resolution.

RECOMMENDATION

PURSUANT to the report of the Director of Finance, dated November 25, 2020, entitled "2021 Utility Rates":

THAT the following bylaws be considered:

- "Water Utility Bylaw, 1994, No. 6417, Amendment Bylaw, 2020, No. 8811";
- "Sewerage and Drainage Utility Bylaw 1995, No. 6746, Amendment Bylaw, 2020, No. 8812"; and
- "Solid Waste Management Service, Bylaw 1997, No. 6920, Amendment Bylaw, 2020, No. 8813".

ATTACHMENTS

1. Rates and Levies Summary (CityDoc#[1987677](#))
2. 2021 Water Budget (CityDoc#[1987681](#))
3. 2021 Sewerage and Drainage Budget (CityDoc#[1987681](#))
4. 2021 Solid Waste Budget (CityDoc#[1987681](#))
5. Water Utility Bylaw, 1994, No. 6417, Amendment Bylaw, 2019, No. 8811 (CityDoc#[1991419](#))
6. Sewerage and Drainage Utility Bylaw 1995, No. 6746, Amendment Bylaw, 2019, No. 8812 (CityDoc#[1991432](#))
7. Solid Waste Management Service, Bylaw 1997, No. 6920, Amendment Bylaw, 2019, No. 8813 (CityDoc#[1991499](#))

SUMMARY

The purpose of this report is to provide Council with the financial impact and recommended bylaw amendments for the 2021 Utility Rates.

BACKGROUND

The City's Utility Rate Setting Process typically occurs in fall each year in order to allow preparation of utility bills for mailing in January.

Detailed information regarding the programs and projects considered in the various utility plans is provided in information reports prepared by the City's Engineering, Parks and Environment (EPE) Department which are also included in the Regular Council Agenda Package.

DISCUSSION

The City's Utility programs include water, sewerage and drainage, and solid waste (refuse, green can, and eco levy). The various utilities are self-funded through the annual utility rate setting process. The utility rates reflect the City's requirements for ongoing operations, capital programs, reserves, cost of services provided by Metro Vancouver, and other contractual costs.

Council adopted the "Development Cost Charges Bylaw, 2016, No. 8471" on July 25, 2016. The bylaw provides for the collection of DCC's for the water, sanitary sewer and drainage utilities. The fees collected under the bylaw are an important funding source for the utilities as the systems are upgraded and expanded to meet increasing service demands. Due to timing differences between DCC collection and project requirements, other reserves within the utilities may be borrowed against for the DCC portion of project costs.

The proposed 2021 flat rate utility fees for a **single unit dwelling** are as follows:

Flat Rate Utility Fees Single Unit Dwelling	2021	2020	Net Change	Net Change %
Water	\$514.00	\$489.00	\$25.00	5.1%
Sewerage & Drainage				
Sewer	444.00	411.00	33.00	7.4%
Storm Drainage (average)*	<u>113.25</u>	<u>107.86</u>	<u>5.39</u>	
Net Sewerage & Drainage	557.25	518.86	38.39	
Solid Waste				
Refuse	119.00	115.00	4.00	8.4%
Green Can	138.00	125.00	13.00	
Eco Levy (average) *	<u>52.80</u>	<u>45.90</u>	<u>6.90</u>	
Net Solid Waste	309.80	285.90	23.90	
Total Flat Rate Utility Fee	\$1,381.05	\$1,293.76	\$87.29	6.7%

The proposed 2021 flat rate utility fees for a **multi-unit dwelling** are as follows:

Flat Rate Utility Fees Multi-Unit Dwelling	2021	2020	Net Change	Net Change %
Water	\$301.00	\$286.00	\$15.00	5.2%
Sewerage & Drainage				
Sewer	277.00	256.00	21.00	8.2%
Storm Drainage (average)*	<u>51.27</u>	<u>48.83</u>	<u>2.44</u>	5.0%
Net Sewerage & Drainage	328.27	304.83	15.44	7.7%
Solid Waste				
Eco Levy (average) *	25.30	22.00	3.30	15.0%
Total Flat Rate Utility Fee	\$654.57	\$612.83	\$41.74	6.8%

* The Storm Drainage and Eco Levy are estimated. The amount is levied based on taxable assessment.

The table below shows a five year utility rate increase history for single unit dwellings:

2016	2017	2018	2019	2020	Average
2.8%	2.9%	2.9%	4.9%	4.0%	3.5%

Attachment #1 provides a summary of the proposed 2021 utility rates and levies by dwelling types. The summary also includes current year comparisons between the North Shore municipalities. It should be noted that the City's utility rates are the lowest on the North Shore and continue to be competitive through the region.

Water Utility Fund

The Water Utility provides a water distribution system for the citizens and businesses of the City of North Vancouver. The water is purchased from Metro Vancouver's Greater Vancouver Water District.

The City's 2021 Water budget is \$13.8 million, a \$0.65 million increase over 2020.

	2020	2021	Change	%
Water Operations	\$573,000	\$585,000	\$12,000	2.1%
Water Admin	1,901,600	1,915,400	13,800	0.7%
Water Purchases	7,274,000	7,427,700	153,700	2.1%
Capital Projects	1,535,000	1,930,000	395,000	25.7%
Ongoing Capital Programs	766,000	771,000	5,000	0.7%
Transfers to Reserves	183,500	694,700	511,200	278.6%
Other	957,700	514,700	(443,000)	(46.3%)
	\$13,190,800	\$13,838,500	\$647,700	4.9%

Bulk Water purchases from the Greater Vancouver Water District (GVWD) account for approximately 55% of total and 70% of operating expenses of the water utility. The price of bulk water is set to increase 3.5% over 2020, but due to forecast reduced volumes and a shift between peak and non-peak water purchases, the budgeted cost of water is estimated to increasing by only 2.1%.

Significant capital projects included in the plan include:

- \$680,000: Watermain Replacement: 100-200 Block East 13th
- \$665,000: Watermain Replacement: 200-300 Block West 27th
- \$320,000: Watermain Replacement: 2500-2600 Block Jones Avenue
- \$180,000: Watermain Replacement: 200-400 Block Gladstone

The City's 2021 Water revenue sources consist of flat rate utility fees, metered charges, use of reserves and other fees (intergovernmental, connection fees, severance fees etc.). The \$0.65 million increase in funding requirement will be provided by:

	2020	2021	Change	%
Flat Rate Sales	\$7,561,400	\$8,084,500	\$523,100	6.9%
Metered Sales	4,320,400	4,567,200	246,800	5.7%
Other	1,284,300	841,900	(442,400)	(34.4%)
Transfers from Reserves	24,700	344,900	320,200	1296%
	\$13,190,800	\$13,838,500	\$647,700	4.9%

The rates recommended in this report reflect the City's requirements for ongoing operations, capital programs, reserves and the GVWD's long range projection for water-related services.

The City utilizes seasonal metered water rates to be consistent with Metro Vancouver's model for selling their bulk water to all member municipalities by charging a higher rate from June 1 to September 30 each year. Seasonal rates are designed to create a pricing strategy that values water more in the summer when alpine lake storage is most stressed. The Water utility model assumes 35% in peak water usage and 65% in non-peak periods. The blended rate increase is consistent with the increases to other user groups.

The 2021 proposed seasonal rates are the following:

Period	Rate per cubic meter
Jan1 to May 31	1.2823
June 1 to Sept 30	1.6029
Oct 1 to Dec 31	1.2823
2021 Blended Rate	1.3945
2020 Blended Rate	1.3281
Annual Net Change %	5.0%

Attachment #2 provides a summary of the 2021 Water Utility Budget.

Sewerage & Drainage Utility Fund

The Sewerage & Drainage Utility provides a sanitary and storm water collection system for the citizens and businesses of the City of North Vancouver. This report recommends rates and transfers from reserves to meet EPE's funding requirements for ongoing operations and capital programs.

The City's 2021 combined Sewerage & Drainage budget is approximately \$14.2 million, a \$1.1 million increase over 2020. The Sewerage & Drainage Utility includes a provision for the new North Shore Wastewater Treatment Plant (NSWWTP) of \$400,000 which is equal to the amount from 2020. This provision aims to set aside funds to smooth out future rate increases associated with the construction of the NSWWTP. The current amount of funds set aside to date for the NSWWTP is \$3.4 million. Beginning in 2022, the City will draw from the funds put aside to smoothen the impact of future GVS&DD Levy increases.

	2020	2021	Change	%
Sanitary & Drainage Operations	\$516,000	\$568,000	\$52,000	10.1%
Sanitary & Drainage Admin	2,123,000	2,152,000	29,000	1.4%
GVS&DD Levy	6,771,700	7,513,100	741,400	10.9%
Capital Projects	1,560,000	1,213,000	(347,000)	(22.2%)
Ongoing Capital Programs	636,000	1,306,000	670,000	105.2%
Transfer to Reserves for NSWWTP	400,000	400,000	0	0%
Transfer to Reserves	276,500	156,000	(120,500)	(43.6%)
Other	855,100	932,100	77,000	9.0%
	\$13,138,300	\$14,240,200	\$1,101,900	8.4%

The Greater Vancouver Sewer and Drainage District (GVS&DD) levy accounts for approximately 50% of total and 70% of operating expenses of the sewerage and drainage utility. The levy is budgeted to increase 10.9% over 2020.

Significant capital work included in the plan includes:

- \$440,000 – Stormmain – Development Supported Infill
- \$225,000 – Stormwater – Treatment Facilities
- \$175,000 – Stormmain – Lonsdale at Keith Road

In addition to the capital construction projects, the plan also includes additional funding for ongoing capital programs which include:

- \$400,000 – Sanitary Renewal/Replacement by Area
- \$235,000 – Sanitary Main Capital Renewal - Relining
- \$100,000 – Sanitary IC and Connection Rehab Program

The City's 2021 Sewer and Drainage revenue sources consist of sanitary sewerage flat rate and metered charges, storm drainage levy, use of reserves and other fees (intergovernmental, connection fees, severance fees etc.). The \$1.1M additional funding requirement will be provided by:

	2020	2021	Change	%
Flat Rate Sales	\$6,660,300	\$7,322,500	\$662,200	9.9%
Metered Sales	2,953,900	3,256,700	302,800	10.3%
Storm Drainage Levy	2,430,500	2,481,500	51,000	2.1%
Transfers from Reserves	75,000	79,200	4,200	5.6%
Other	1,018,600	1,100,300	81,700	8.0%
	\$13,138,300	\$14,240,200	\$1,101,900	8.4%

The City uses a single rate billing system for metered sewer, which is consistent with user pay pricing strategy. The proposed metered sewer rate for 2021 is \$1.8123 per cubic meter, which is an 8.0% increase over the 2020 rate of \$1.6781 per cubic meter.

Attachment #3 provides a summary of the 2021 Sewerage & Drainage Budget.

Solid Waste Utility Fund

The Solid Waste Utility provides for refuse, green can, and eco levy services for the citizens of the City of North Vancouver. The rates recommended in this report are used to operate and maintain these services.

As of July 1st, 2020, Recycle BC has taken responsibility for the delivery of residential recycling services on the North Shore. As a result, the cost of the recycling collection program, contract administration, and community education programs are no longer funded directly by City residents through a Recycling Levy in the Solid Waste Utility.

Utility expenses are summarized as follows:

	2020	2021	Change	%
Refuse Operations and Admin	\$837,150	\$960,200	\$123,050	14.7%
Green Can Operations	777,200	730,000	(47,200)	(6.1%)
Recycling Operations	649,670	45,000	(604,670)	(93.1%)
Green Can Admin	340,050	339,000	(1,050)	(0.3%)
Eco Levy Ops and Admin	1,001,800	1,210,500	208,700	20.8%
Zero Waste Challenge	250,000	345,800	95,800	38.3%
Transfers to Reserve	4,282	0	(4,282)	(100%)
	\$3,860,152	\$3,630,500	\$(229,652)	(5.9%)

The additional funding will be increasing revenue as follows:

	2020	2021	Change	%
User Fees	\$1,717,432	\$1,827,000	\$109,568	6.4%
Eco Levy	1,100,000	1,265,000	165,000	15.0%
PPP Program Incentive	335,000	-	(335,000)	(100%)
Transfers from Reserve	707,720	538,500	(169,220)	(23.9%)
	\$3,860,152	\$3,630,500	\$(229,652)	(5.9%)

The City's 2021 combined Solid Waste budget is approximately \$3.6 million. The 2021 recommended rates increase Refuse, Green Can and Eco Levy rates by 3.5%, 10.4%, and 15.0% respectively, resulting in overall solid waste fee increases of 8.4% for single unit dwellings and 15.0% for multi unit dwellings. It should be noted that multi unit dwellings are responsible for their own Refuse and Green Can disposal through third party entities.

Single Unit Rates	2020	2021	Change	%
Refuse	\$115.00	\$119.00	\$4.00	3.5%
Green Can	125.00	138.00	13.00	10.4%
Eco Levy (average)*	45.90	52.80	6.90	15.0%
Net Solid Waste	\$285.90	\$309.80	\$23.90	8.4%

Multi Unit Rates	2020	2021	Change	%
Eco Levy (average)*	\$22.00	\$25.30	\$3.30	15.0%

For 2021, the Eco Levy has been increased by 15% as a result of increasing operational costs from collecting larger volumes of litter. Since the onset of the COVID-19 pandemic, the use of City parks, streetscapes and plazas has increased significantly. The additional funding will also be used to increase the number of multi-stream refuse and recycling receptacles in the public realm to help address the litter volume increases and to support the City's waste diversion goals. As for the 10.4% increase of the Green Can rate, it is the result of a 23.9% decrease in reserve use as the use of reserve funding is becoming financially unsustainable.

The provision of solid waste services in the City of North Vancouver operate under the auspices of the Solid Waste Management Service Bylaw. The current bylaw was adopted in 1997 and has been amended a number of times in recent years with the significant changes to services and delivery models, such as the introduction of the Eco Levy, residential Green Can program, bi-weekly garbage collection, and most recently, Recycle BC's responsibility for residential recycling. Given these changes, Staff are proposing amendments to the current bylaw as an interim measure, but will be drafting a new Solid Waste bylaw for consideration by Council in early 2021.

The proposed interim bylaw amendments (attachment #7) include the annual updates to the rates as well as a proposed six can (or bag/bundle) weekly limit for curbside Green Can collection, ending the City's limited commercial tag garbage collection program, and deleting the reference to the residential recycling collection program, which is now provided by Recycle BC. Additional details concerning the 2021 Solid Waste Budget are provided in attachment #4.

FINANCIAL IMPLICATIONS

To summarize, the proposed 2021 flat rate utility fees by dwelling type are the following:

Dwelling Type	2021 Net Flat Rate (proposed)	2020 Net Flat Rate	Net Change \$
Single Unit	\$1,381.05	\$1,293.76	\$87.29
Duplex	\$1,160.33	\$1,085.57	\$74.76
Multi-Unit	\$654.57	\$612.83	\$41.74

This translates in a total annual utility charge increase of 6.7% for single unit dwellings and 6.8% for multi-unit dwellings. The City's utility rates continue to be among the lowest in the region.

It should be noted that significant regional cost increases are anticipated in the coming years, driven predominately by Metro Vancouver's expanding capital programs. The following table indicates the latest forecasted percentage increases to regional rates over the next four years.

	2022	2023	2024	2025
Bulk Water	5.2%	6.4%	8.9%	10.8%
GVS&DD Levy	19.7%	20.6%	18.6%	17.1%
Tipping Fees	3.4%	5.8%	5.5%	4.7%

While the City has been planning to buffer the impacts of the NSWWTP coming on-line, if the above increases materialize it is likely that significant future rate increases, estimated at 6% to 10%, will be required in order for the City to reach the capital renewal and rehabilitation goals discussed in the accompanying information reports from the City Engineer.

Option:

The current recommendation proposes rate adjustments that provide for all programs/projects listed in EPE's 2021 budget request. The request translates in a total annual utility rate increase of 6.7% for single unit dwellings and 6.8% for multi-unit dwellings.

Alternatively, Council could instruct staff to review options to reduce the proposed increases. It should be noted that funding is included in the proposed budget to enhance asset management efforts for the purpose of generating information that will assist in better forecasting asset maintenance needs in future years.

The City has a long-standing policy of prioritizing the maintenance of existing assets over the construction of new ones. The current list of projects includes replacement and upgrade projects as well as additional new infrastructure projects. Council could request staff to review the various projects to determine if any could be postponed to future years. As an example, Council could adopt a resolution requesting staff to prioritize capital projects included in the plan to provide options to Council to reduce the total annual Utility rate increase to a certain percentage.

INTER-DEPARTMENTAL IMPLICATIONS

This report has been prepared with the input and participation of the EPE department and should be read in conjunction with the information reports provided by EPE regarding each utility. The Leadership Team reviewed and commented on the report at its November 24, 2020 meeting.

CORPORATE PLAN AND/OR POLICY IMPLICATIONS:

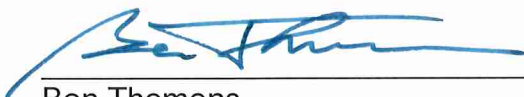
The recommended rates provide the funding to support the following goals and objectives of the 2014 Official Community Plan:

- 8.1.3 Consider stormwater management opportunities in streets, laneways and open spaces where appropriate;
- 8.1.4 Improve financial models to include cash flow projections that meet capital asset replacement needs;
- 8.1.8 Complete work underway to meet the objective of the Metro Vancouver Integrated Solid Waste and Resource Management Plan (ISWRMP) of an average 70% diversion rate by 2015 and 80% by 2020, as well as meet the requirement of the regional Liquid Waste Management Plan for a municipal Integrated Stormwater Management Plan (ISMP) by 2015;
- 8.1.9 Pursue the reduction of waste throughout the full lifecycle of production, consumption, recycling and disposal to achieve local and regional waste management goals;
- 8.2.1 Maintain or improve service levels related to water supply, sewers, transportation, communication and energy infrastructure;
- 8.2.2 Identify and monitor the condition of our infrastructure on a regular basis in order to identify the remaining operational life of individual elements, and to identify and rectify weaknesses before failure, thereby optimizing capital and maintenance program expenditures;

STRATEGIC PLAN IMPLICATIONS:

The proposed utility rates support Council's commitment to being "fiscally responsible" through rates that balance operational and capital renewal needs with the impact on the ratepayer.

RESPECTFULLY SUBMITTED:



Ben Themens
Director of Finance

The City of North Vancouver
Rates and Levies Summary

	2021 **DWV	2021 ***DNV	2020 CNV	2021 CNV	Increase \$	%
SINGLE UNIT DWELLING						
Flat Rate Water	\$ 764.00	\$ 772.00	\$ 489.00	\$ 514.00		
5% Discount			-	-		
Single Unit Net Water	764.00	772.00	489.00	514.00	25.00	5.1%
Flat Rate Sanitary Sewer	659.04	735.80	411.00	\$ 444.00		
5% Discount			-	-		
Single Unit Net Sewer	659.04	735.80	411.00	444.00	33.00	8.0%
Storm Drainage Levy (estimate)	526.96		107.86	113.25 *	5.39	5.0%
Total Single Unit Water/Sanitary/Storm	\$ 1,950.00	\$ 1,507.80	\$ 1,007.86	\$ 1,071.25	\$ 63.39	6.3%
Other Rates						
Refuse	\$ 204.38	\$ 295.90	\$ 115.00	\$ 119.00	4.00	3.5%
Green Can			125.00	\$ 138.00	13.00	10.4%
Recycling - Single Unit	-	23.70	-	\$ -	-	0.0%
Eco Levy (estimate)	48.05		45.90	\$ 52.80 *	6.90	15.0%
Total Single Unit Solid Waste	\$ 252.43	\$ 319.60	\$ 285.90	\$ 309.80	\$ 23.90	8.4%
TOTAL SINGLE UNIT ALL LEVIES	\$ 2,202.43	\$ 1,827.40	\$ 1,293.76	\$ 1,381.05	\$ 87.29	6.7%
DUPLEX Including Infills & Strata SFD)						
Flat Rate Water	\$ 611.20	\$ 657.60	\$ 410.00	\$ 431.00		
5% Discount						
Duplex Net Water	611.20	657.60	410.00	431.00	21.00	5.1%
Flat Rate Sanitary Sewer	565.65	\$ 612.20	380.00	\$ 411.00		
5% Discount						
Duplex Net Sewer	565.65	612.20	380.00	411.00	31.00	8.2%
Storm Drainage Levy (estimate)	526.96		97.78	102.67 *	4.89	5.0%
Total Duplex Water/Sanitary/Storm	\$ 1,703.81	\$ 1,269.80	\$ 887.78	\$ 944.67	\$ 56.89	6.4%
Other Rates						
Refuse	\$ 204.38	\$ 295.90	77.00	\$ 80.00	3.00	3.9%
Green Can			75.00	83.00	8.00	10.7%
Recycling - Duplex	-	23.70	-	-	-	0.0%
Eco Levy (estimate)	48.05		45.79	52.66 *	6.87	15.0%
Total Duplex Solid Waste	\$ 252.43	\$ 319.60	\$ 197.79	\$ 215.66	\$ 17.87	9.0%
TOTAL DUPLEX ALL LEVIES	\$ 1,956.24	\$ 1,589.40	\$ 1,085.57	\$ 1,160.33	\$ 74.76	7.0%
MULTI-UNIT DWELLING						
Flat Rate Water	\$ 328.52	\$ 657.60	\$ 286.00	\$ 301.00		
5% Discount			-	-		
Multi-Unit Net Water	328.52	657.60	286.00	301.00	15.00	5.2%
Flat Rate Sanitary Sewer	392.22	\$ 612.20	256.00	\$ 277.00		
5% Discount			-	-		
Multi-Unit Net Sewer	392.22	612.20	256.00	277.00	21.00	8.2%
Storm Drainage Levy (estimate)	526.96		48.83	51.27 *	2.44	5.0%
Total Multi Unit Water/Sanitary/Storm	\$ 1,247.70	\$ 1,269.80	\$ 590.83	\$ 629.27	\$ 38.44	6.5%
Recycling - Multi Unit	-	14.60	-	-	-	0.0%
Eco Levy (estimate)	48.05		22.00	25.30 *	3.30	15.0%
Total Multi Family Solid Waste	\$ 48.05	\$ 14.60	\$ 22.00	25.30	3.30	15.0%
TOTAL MULTI-UNIT ALL LEVIES	\$ 1,295.75	\$ 1,284.40	\$ 612.83	\$ 654.57	\$ 41.74	6.8%

* Average storm and eco levy based on average property assessment, plus overall increase for 2021

** DWV is fully metered; therefore, the water and sewer rates are estimates based on expected usages

*** DNV does not have a separate storm drainage levy

2021 WATER BUDGET

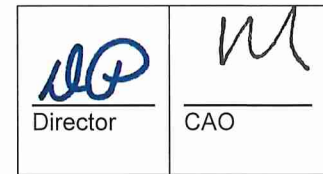
Programs	2020 Annual Budget	2021 Annual Budget	Variance 2021 Annual to 2020 Annual	% Variance	Notes
WATER REVENUE					
PERMITS SPRINKLER	1,000	1,000	0		
FEES CONNECTION-WATER/SANITARY	990,700	547,700	-443,000		Updated to reflect projected actuals - offset against expenses
FEES CONSTRUCTION SITE	20,700	12,000	-8,700		Updated to reflect projected actuals
FEES SEVERANCE	61,800	63,700	1,900		
WATER CONNECTION REUSE	49,200	50,900	1,700		
Water Operations	1,123,400	675,300	-448,100	-39.9%	
FLAT RATE SALES	7,561,391	8,084,501	523,110		Consistent with proposed rate increase
METERED SALES	4,320,362	4,567,200	246,838		Consistent with proposed rate increase
PENALTIES FLAT RATE	13,900	14,400	500		
TRANSFER FROM DNV	147,000	152,100	5,100		
Water Admin	12,042,653	12,818,201	775,548	6.4%	
TRANSFER FROM STATUTORY RESERVE	24,750	345,000	320,250		DCC reserve used for Capital project
Transfers	24,750	345,000	320,250	1293.9%	
Total Water Revenue	13,190,803	13,838,501	647,698	4.9%	
WATER EXPENSES					
WAGES AND BENEFITS	0	0	0		
GOODS AND SUPPLIES	0	0	0		
SERVICES	0	0	0		
FINANCIAL SERVICES	0	0	0		
Water Operations	573,000	585,000	12,000	2.1%	
WAGES AND BENEFITS	955,434	960,300	4,866		
GOODS	5,000	5,000	0		
WATER PURCHASES DNV	245,100	252,500	7,400		
WATER PURCHASES GVWD	7,028,900	7,175,200	146,300		3.5% increase in regional cost of bulk water - 50% of total utility budge
SERVICES	105,500	90,500	-15,000		Decreased water studies scheduled in 2021
FINANCIAL SERVICES	835,700	859,600	23,900		
Water Admin	9,175,634	9,343,100	167,466	1.8%	
Sub Total Water Operating	9,748,634	9,928,100	179,466	1.8%	
OHead Water Capital	-33,000	-33,000	0	0.0%	
TRANSFER TO CAPITAL	2,301,000	2,701,000	400,000		Planned increase for replacement of aging infrastructure
THIRD PART CONNECTIONS	990,700	547,700	-443,000		Updated to reflect projected actuals - offset against revenue
RESERVE FOR FUTURE EXPENDITURES	50,000	400,000	350,000		Increased reserve to offset future Metro costs
TRANSFER TO STATUTORY RESERVE	120,000	120,000	0		
TRANSFER TO UN/APPROPRIATED EQUITY	13,469	174,701	161,232		Increased to bring reserve to appropriate range of 5-10% of Water Expenditures.
Transfers	3,475,169	3,943,401	468,232	13.5%	
Total Water Expenses	13,190,803	13,838,501	647,698	4.9%	

2021 SEWERAGE AND DRAINAGE BUDGET

Programs	2020 Annual Budget	2021 Annual Budget	Variance 2021 Annual to 2020 Annual	% Variance	Notes
SEWER AND STORM REVENUES					
FEES CONNECTION-WATER/SANITARY	384,000	430,600	46,600		Increased to reflect projected actuals - offset against expenses
FEES SEVERANCE	57,800	60,700	2,900		
MISCELLANEOUS	15,500	16,000	500		
Sanitary Operations	457,300	507,300	50,000	10.9%	
FLAT RATE SALES	6,660,300	7,322,500	662,200		Consistent with proposed rate increase
METERED SALES	2,953,908	3,256,700	302,792		Consistent with proposed rate increase
PENALTIES FLAT RATE	9,300	9,600	300		
Sanitary Admin	9,623,508	10,588,800	965,292	10.0%	
FEES STORM CONNECTION	515,200	544,800	29,600		Increased to reflect projected actuals - offset against expenses
FEES SEVERANCE STORM	36,800	38,600	1,800		
Storm Operations	552,000	583,400	31,400	5.7%	
STORM DRAINAGE LEVY	2,430,500	2,481,500	51,000		Consistent with proposed rate increase
Storm Admin	2,430,500	2,481,500	51,000	2.1%	
Sub-Total Operating Revenues	13,063,308	14,161,000	1,097,692	8.4%	
TRANSFER FROM APPROPRIATED EQUITY	75,000	60,000	-15,000		\$75K from Capital Reserves for future expenditures
TRANSFER TO STATUTORY RESERVE	0	19,200	19,200		
Sewer and Storm Transfers	75,000	79,200	4,200	5.6%	
Total Sewer and Storm Revenue	13,138,308	14,240,200	1,101,892	8.4%	
SEWER AND STORM EXPENSES					
WAGES AND BENEFITS	122,600	148,140	25,540		Due to increased maintenance requirements on aging infrastructure
CONFERENCES, EDUCATION, TRAVEL	0	0	0		
GOODS AND SUPPLIES	17,980	21,730	3,750		
SERVICES	52,540	63,490	10,950		
FINANCIAL SERVICES	22,880	27,650	4,770		
Sanitary Operations	216,000	261,000	45,000	20.8%	
WAGES AND BENEFITS	482,664	485,300	2,636		
GOODS AND SUPPLIES	5,300	5,600	300		
SERVICE EXPENSES AND FEES	145,000	150,000	5,000		
GVS&DD LEVY	6,771,696	7,513,070	741,374		9.8% GVS&DD Sewer levy increase - 50% of total utility budget
GVRD GEOSPATIAL LEVY	7,900	8,700	800		
FINANCIAL SERVICES	476,250	522,950	46,700		
Sanitary Admin	7,888,810	8,685,620	796,810	10.1%	
WAGES AND BENEFITS	153,870	157,460	3,590		Increase due to addition of Spill Response
GOODS AND SUPPLIES	23,600	24,150	550		
SERVICES	91,330	93,460	2,130		
FINANCIAL SERVICES	31,200	31,930	730		
Storm Operations	300,000	307,000	7,000	2.3%	
WAGES AND BENEFITS	505,206	507,600	2,394		Decrease due to changes made to allocation from internal re-org
GOODS AND SUPPLIES	5,300	5,600	300		
SERVICES	195,000	140,000	-55,000		Decreased drainage studies scheduled in 2021
FINANCIAL SERVICES	308,250	334,950	26,700		
Storm Admin	1,013,756	988,150	-25,606	-2.5%	
Sub-Total Sewer and Storm Expenses	9,418,566	10,241,770	823,204	8.7%	
FINANCIAL SERVICES	-29,000	-29,000	0		
OHead Sewer Capital	-29,000	-29,000	0		
FINANCIAL SERVICES	-23,000	-23,000	0		
OHead Drainage Capital	-23,000	-23,000	0		
Sub-Total Sewer and Storm Recoveries	-52,000	-52,000	0	0.0%	
TRANSFER TO CAPITAL	2,196,000	2,519,000	323,000		Planned increase for replacement of aging infrastructure
THIRD PARTY CONNECTIONS	899,200	975,400	76,200		Adjusted to reflect projected actuals - offset against revenues
TRANSFER TO STATUTORY RESERVE	218,000	18,000	-200,000		
TFR SAME FUND TO PROJ TRKG	0	0	0		
TRANSFER TO APPROPRIATED EQUITY	58,542	138,030	79,488		
RESERVE FOR FUTURE EXPENDITURES	400,000	400,000	0		Reserve contribution for LGWWTP
Sewer and Storm Transfers	3,771,742	4,050,430	278,688	7.4%	
Total Sewer and Storm Expenses	13,138,308	14,240,200	1,101,892	8.4%	

2021 SOLID WASTE BUDGET

Programs	2020 Annual Budget	2021 Annual Budget	Variance 2021 Annual to 2020 Annual	% Variance	Notes
SOLID WASTE REVENUE					
GREEN CAN	876,000	966,100	90,100		Consistent with proposed rate increase
PPP PROGRAM INCENTIVE	335,000	0	-335,000		Reduced due to transition to Recycle BC for Recycling services
Recycling & Green Can	1,211,000	966,100	-244,900	-20.2%	
REFUSE TAGS COMMERCIAL	3,500	3,500	0		
REFUSE TAGS RESIDENTIAL	9,000	9,000	0		
Refuse	12,500	12,500	0	0.0%	
REFUSE LEVY	828,932	848,400	19,468		Consistent with proposed rate increase
Refuse Admin	828,932	848,400	19,468	2.3%	
ECO LEVY	1,100,000	1,265,000	165,000		Consistent with proposed rate increase
Eco Levy Programs	1,100,000	1,265,000	165,000	15.0%	
TRANSFER FROM APPROPRIATED EQUITY	707,720	538,500	-169,220		Reduced use of reserves as a funding source
Transfers	707,720	538,500	-169,220	-23.9%	
Total Solid Waste Revenue	3,860,152	3,630,500	-229,652	-5.9%	
SOLID WASTE EXPENSES					
SERVICES	649,670	45,000	-604,670		Reduced due to transition to Recycle BC for Recycling services
Recycling	649,670	45,000	-604,670	-93.1%	
WAGES AND BENEFITS	221,600	208,100	-13,500		Cost allocation updated between Refuse and Green Can, net increase of 6%
SERVICES	555,600	521,900	-33,700		Additional truck/staff added to collection route in 2020
Green Can	777,200	730,000	-47,200	-6.1%	
WAGES AND BENEFITS	71,200	160,700	89,500		
GOODS AND SUPPLIES	127,200	126,100	-1,100		
SERVICES	70,000	36,100	-33,900		
FINANCIAL SERVICES	196,650	189,000	-7,650		
Recycling & Green Can Admin	465,050	511,900	46,850	10.1%	
WAGES AND BENEFITS	157,100	198,950	41,850		Cost allocation updated between Refuse and Green Can, net increase of 6%
SERVICES	231,500	293,170	61,670		Additional truck/staff added to collection route in 2020
FINANCIAL SERVICES	69,400	87,890	18,490		
Refuse	458,000	580,000	122,010	26.6%	
WAGES AND BENEFITS	158,400	165,900	7,500		
GOODS AND SUPPLIES	2,000	2,000	0		
FINANCIAL SERVICES	218,750	212,300	-6,450		
TRANSFER TO APPROPRIATED EQUITY	4,282	0	-4,282		
Refuse Admin	383,432	380,200	-3,232	-0.8%	
WAGES AND BENEFITS	960,600	1,179,200	218,600		Increase due to the growth of the Eco Levy Program
GOODS AND SUPPLIES	125,000	126,100	1,100		
SERVICES	0	36,100	36,100		
FINANCIAL SERVICES	41,200	42,000	800		
Eco Levy Programs	1,126,800	1,383,400	256,600	22.8%	
Total Solid Waste Expenses	3,860,152	3,630,500	-229,652	-5.9%	



The Corporation of **THE CITY OF NORTH VANCOUVER**
ENGINEERING, PARKS AND ENVIRONMENT DEPARTMENT

INFORMATION REPORT

To: Mayor Linda Buchanan and Members of Council

From: Douglas Pope, Director, Engineering, Parks and Environment

SUBJECT: 2021 WATER UTILITY UPDATE

Date: November 25, 2020 File No: 11-5600-01-0001/2021

PURPOSE

The purpose of this report is to provide Council with information related to the operation and infrastructure management of the Water Utility. This report is a companion document to the rate setting report.

BACKGROUND

The Water Utility represents the cooperative effort of the City of North Vancouver and the Greater Vancouver Water District (GVWD) to provide a reliable supply of safe, high-quality drinking water to our businesses and residents and to meet the needs for fire protection. Implicit in this is a capital program for both organizations that reflects the best choices in rehabilitation, replacement and new construction in managing aging infrastructure and increased demands.

The City's Engineering, Parks and Environment Department (EPE) provides for the operation and management of the water distribution system. The City receives water from the Greater Vancouver Water District (Metro Vancouver) and distributes it for domestic, commercial and industrial use as well as for fire protection. The primary functions within EPE are the construction, maintenance, renewal and repair of mains, connections, valves, hydrants and related facilities.

DISCUSSION

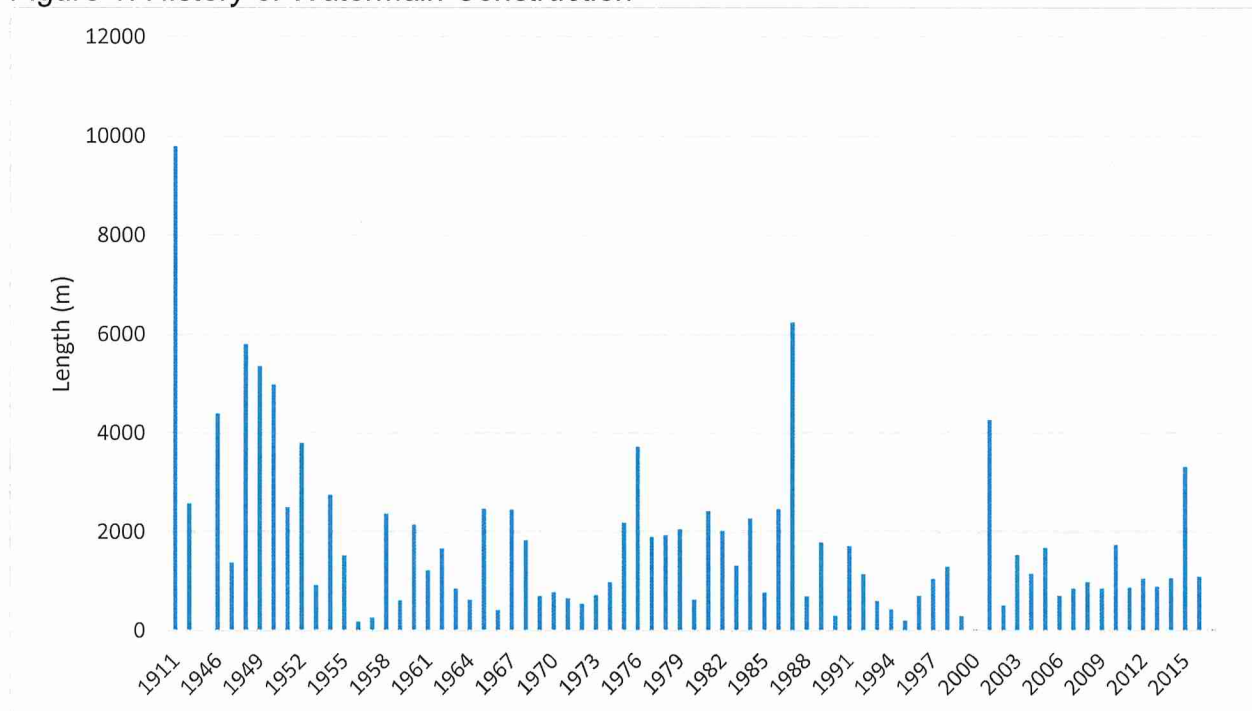
The average age of the City's water system is approximately 51 years old (by pipe length) and generally comprises the following primary components:

- 129 kilometres of watermains;

- 6,600 water service connections;
- 730 fire hydrants;
- 12 pressure reducing stations; and,
- 20 water quality sample stations.

The first segments of the City's water system were constructed in the 1910s and some are still in service today. This includes watermains running along Lonsdale, areas east and west of Lower Lonsdale, and Moodyville.

Figure 1: History of Watermain Construction



Water Utility Infrastructure Renewal

For long-term financial planning of overall water system replacement, an average annual replacement target of 0.8% (1,000 metres per year) is included in the Official Community Plan (OCP). This annual target provides a simplistic approach to plan how much of the total system should be replaced each year and is based on the assumption that the average economic life of a watermain is 125 years.

The actual period of time a watermain can provide reliable service varies based on a variety of factors such as material, ground conditions, installation practices and system demands. For example, smaller diameter cast iron pipes in corrosive ground may last 60 years or less, while large diameter, thick-walled cast iron pipes may last well over 125 years. Similarly, land-use changes create different system demands that may trigger system upgrades to increase capacity.

Looking ahead to the next 10-15 years, a large quantity of small diameter cast iron watermains will require replacement. To avoid a significant portion of the system requiring replacement all at once, staff have recommended that we gradually increase the annual

replacement rate to 1.2% (1,500 metres per year). This increases system performance and reliability, improves water quality and helps get ahead of the impending spike or backlog in renewals. Staff anticipate this rate of replacement will be needed until about 2030. Beyond 2030, the replacement rate can be adjusted to meet the needs of the system at that time. Taking these steps now proactively helps maintain high water quality, as well as reducing the risk of service interruptions caused by watermain breakages.

The 2020 project plan targeted replacement of about 0.7% (860 metres) of the system at a cost of \$1.4 million. The 2020 project plan was slightly less than typical following higher and more costly watermain construction in 2019 along the East 3rd Street and Cotton Road corridor. In 2020, a total of 790 metres of watermain was completed. For 2021, 1,230 metres of watermain replacement is proposed, representing approximately 0.95% of the system and a total capital cost of \$1.85 million.

In addition to watermain replacement, EPE administers ongoing proactive programs to replace aging water service connections, add new and replace old hydrants, and on-going capital replacement of pressure regulating stations. In the next decade, the Water Utility will need to consider larger capital upgrades or additional pressure regulating stations to meet system needs, add redundancy to improve reliability, and meet future safety regulations.

The budget also provides funding for technical studies and planning tools for the water system. Highlights for 2021 include an updated capital plan which would support the asset management plan process.

2021 Water Utility Budget

The following section summarizes the total new construction (capital) and operational budget for the Water Utility. The budget is separated into four main areas:

- Water Studies: annually funded, periodic studies, or updates to planning tools;
- Water Capital Projects: one time large construction projects for new or replacement of existing watermains and structures;
- Ongoing Capital Projects: annually funded construction of small components in the water system; and,
- Water Operations: funding for daily activities to support water utility reliability.

The overall Water Utility Operations budget has been kept constant from 2020 to 2021. The largest typical operational (or non-replacement) cost is the repair of watermain breaks. This represents about 25% of the overall budgeted operational effort for the water utility, which typically experiences about 10 watermain breaks per year. Other larger efforts include watermain flushing, water valve maintenance, and fire hydrant maintenance.

The following table presents the breakdown of the proposed 2021 water utility budget:

Table 1: 2021 Water Utility Budget

Utility Area	Description	Pre-Design Budget
Water Studies	Water Model	\$20,000
	Record Keeping/As Built Project	\$25,000
	Utility Benchmarking	\$20,000
	Water Conservation Plan	\$20,000
	Water Studies Total:	\$85,000
Water Capital Projects	Watermain Replacement - 100-200 Block East 13th	\$680,000
	Watermain Replacement - 200-400 Block Gladstone	\$180,000
	Watermain Replacement - 2500-2600 Block Jones	\$320,000
	Watermain Replacement -200-300 Block West 27th	\$665,000
	Water Projects Total:	\$1,845,000
Ongoing Capital Programs	Water Service Connection	\$300,000
	Water Valve Replacement Program	\$100,000
	Water Meter Replacement Program	\$140,000
	Water Pressure Reducing Station Capital Upgrades	\$50,000
	Water Hydrant Replacement Program	\$25,000
	Water System Sampling Stations	\$15,000
	Redundant Watermain Decommissioning	\$50,000
	Mainline Trench Restoration	\$45,000
	TFT Infrastructure Engineer	\$30,000
	Water Programs Total:	\$755,000
Water Maintenance	Watermain Repair	\$150,000
	Watermain Flushing	\$51,000
	Water Sampling	\$21,000
	Water Valve Inspection and Maintenance	\$51,000
	Water Service Installation	\$0
	Water Service Repair	\$41,000
	Water Service On/Off	\$21,000
	Water Service Inquiries	\$41,000
	Water Service Severance	\$0
	Backflow Device Inspection & Maintenance	\$26,000
	Water Meter Repairs	\$26,000
	Water Meter Reading	\$46,000
	Hydrant Repair	\$15,000
	Hydrant Preventative Maintenance	\$67,000
	PRV Station Inspection	\$29,000
	Water Maintenance Total:	\$585,000
Water Equipment	Small Tools and equipment	\$10,000
	Water Equipment Total:	\$10,000
Water Utility Total:		\$3,280,000

Connection and Severance Fees

Staff is proposing an increase in fixed fee costs of standard water connection and severance fees for single dwelling units. The 2021 servicing costs are as follows:

- \$5,300 for a new 19 millimetre diameter water service connection; and,
- \$1,200 for a 19 millimetre water severance.

The proposed water connection fee has an increase of \$200 over 2020 rates for connections and an increase of \$100 over 2020 rates for severance fees, which were not increased in 2020. These fee increases are intended to ensure the fee covers the actual cost of the work. All larger diameter sanitary sewer services are charged at cost, and all other fees remain unchanged.

Water Utility Efficiency and Incentives

The City of North Vancouver consumes approximately 450 litres per person per day which is also the regional average for Metro Vancouver. While this per capita rate of consumption is lower than our neighbouring communities on the North Shore and slightly lower than the City of Vancouver, it is higher than similar communities such as New Westminster, or metered communities such as Richmond or Surrey.

The City adopted a Water Conservation Strategy in 2015 and implementation of the strategy continues to raise awareness around water use and conservation. In 2020, the City will continue with several actions in the plan, including:

- Expanding the leak detection program to actively monitor and correlate leak locations on both the public and private systems with leak listening equipment;
- Continuing the “meter ready” program, where all new service connections and developments are required to install water meters; and,
- Supporting water and energy conservation programs, such as the BC Hydro appliance rebate program.

Given that the region can have limitations in drinking water supply, especially in the summer months, anticipated future climate change factors may require that we seek additional conservation opportunities. One potential opportunity could be to use reclaimed and treated wastewater from the North Shore Wastewater Treatment Plant (NSWWTP), currently under construction. This concept is being explored for the Harbourside neighbourhood which is the closest proposed site in the development process.

The plant will produce high quality treated and disinfected water that could be readily used for non-potable applications. The City could consider a non-potable water system sourced from the NSWWTP to supply the commercial and new residential development south of Marine Drive and in Harbourside. This type of system could be very effective in limiting outdoor water use in the summer months, which is typically 70% higher than indoor winter water use.

FINANCIAL IMPLICATIONS

The information provided in this report is intended to demonstrate the need for adequate funding for current operating and maintenance requirements, replacement and renewal projects, and that the Water Utility remains self-financing.

Details of the actual rate increase impacts are specified in the companion utility rate report.

INTER-DEPARTMENTAL IMPLICATIONS

The development of the financial plans for the Water Utility is a coordinated effort between Engineering, Parks and Environment and Finance Departments. The Major Projects Team has reviewed the report.

STRATEGIC PLAN, OCP OR POLICY IMPLICATIONS

The 2018-2022 Council Strategic Plan identifies a vision to create the Healthiest Small City in the World, through five key priorities. Provision of an affordable, safe, and sustainable water utility to meet the needs of future residents supports the priorities of:

- A City for People; and
- A Liveable City.

The annual water utility project plan provides sustainable funding for ongoing operation, asset management and renewal of our water system. The utility is fundamental for all residents and businesses in the City with overarching goals for public health and safety.

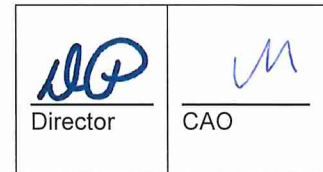
The 2014 Official Community Plan includes the guiding principles to serve the community vision for infrastructure management, which reads as follows:

- Goal 8.1 Provide the community with public infrastructure that protects the natural environment at an affordable cost;
- Goal 8.2.1 Maintain or improve service levels related to water supply, sewers, transportation, communication and energy infrastructure;
- Goal 8.2 Employ a proactive approach to infrastructure maintenance and upgrades;
- Goal 8.2.2 Identify and monitor the condition of our infrastructure on a regular basis in order to identify the remaining operation life of individual elements, and to identify and rectify weaknesses before failure, thereby optimizing capital and maintenance program expenditures;
- Goal 8.3 Use innovative planning and design to ensure infrastructure longevity.

RESPECTFULLY SUBMITTED:



Douglas T. Pope, P. Eng.
Director, Engineering, Parks and Environment



The Corporation of **THE CITY OF NORTH VANCOUVER**
ENGINEERING, PARKS AND ENVIRONMENT DEPARTMENT

INFORMATION REPORT

To: Mayor Linda Buchanan and Members of Council

From: Douglas Pope, Director, Engineering, Parks and Environment

SUBJECT: 2021 SEWERAGE AND DRAINAGE UTILITY UPDATE

Date: November 25, 2020 File No: 11-5340-01-0001/2021

PURPOSE

The purpose of this report is to provide Council with information related to the operation and infrastructure management of the Sewerage and Drainage Utility for the upcoming 2021 Sewerage and Drainage Utility Project Plan review. This report is a companion document to the rate setting report.

BACKGROUND

The Sewerage component of the Utility provides for the collection and conveyance of sewage from individual properties within the City of North Vancouver to Metro Vancouver's trunk sewers that lead to the Lion's Gate Waste Water Treatment Plant. The City's conveyance system is made up of service connection pipes, main pipes, manholes and lift stations.

The Drainage component of the Utility provides for the collection and conveyance of rainstorm water run off from individual properties and City streets to discharge locations into creeks and Burrard Inlet. The system is similarly comprised of service connections, mains and manholes, but also includes infiltration areas (wherever possible) as well as culverts, ditches, catch basins, and oil and sediment removal systems.

DISCUSSION

The Engineering Planning and Design staff work in cooperation with Operations staff to create the optimal approach for operating, maintenance and repair of these assets as well as capital replacement and renewal efforts to maximize their life and minimize the system cost impacts on the ratepayer. For each asset component to have the longest possible operational life, it must be maintained and repaired and then at end of life, be replaced.

The Sewerage and Drainage Utility was formed by Bylaw in 1995 (Bylaw 6746). It is a combined Utility for legal purposes but operated and accounted for as two separate components: one being Sewerage, the other Drainage.

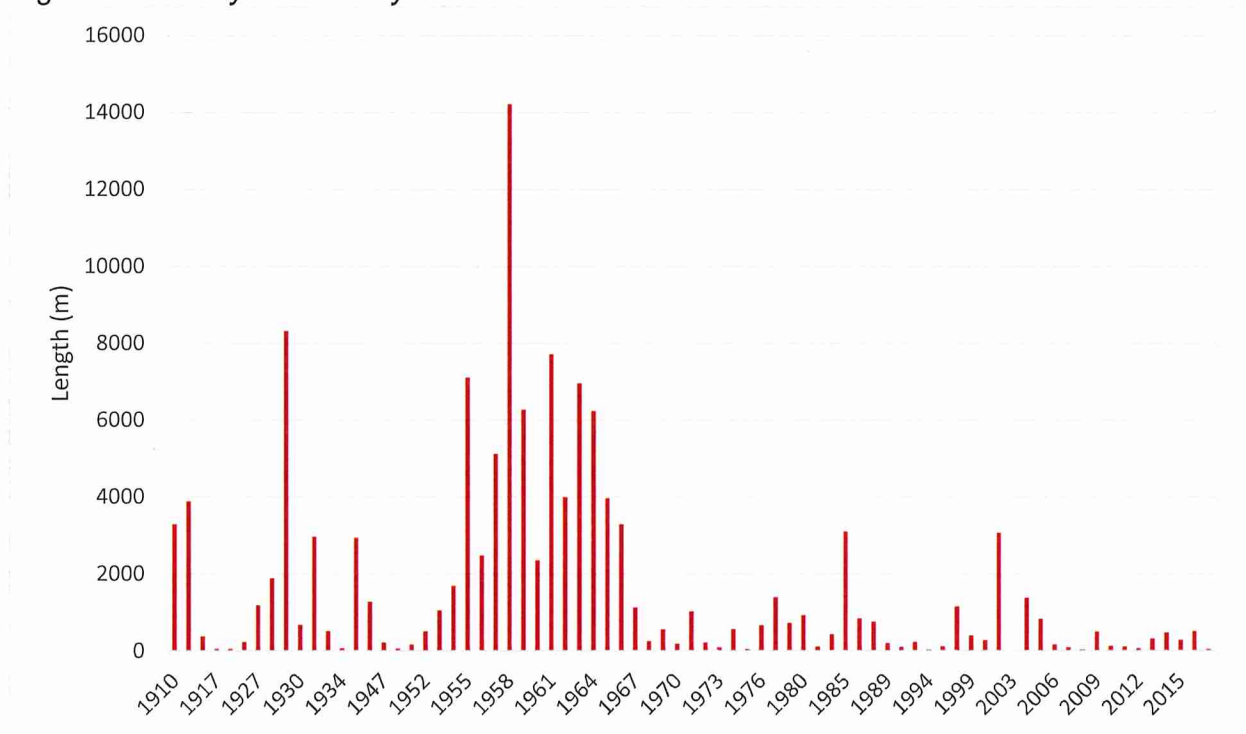
Sanitary Sewer System

The average age of the City's sanitary sewer system is approximately 62 years old, based on total pipe length, and was constructed between 1910 and the present, as shown in the following Figure 1. The peak period of sanitary sewer construction occurred during the 1960s, when many of the City's subdivisions were constructed.

Overall, the sanitary sewer system is comprised of the following primary components:

- 124 kilometres of sanitary sewer main;
- 1,800 manholes;
- 6,800 sanitary service connections; and,
- Four lift stations.

Figure 1: History of Sanitary Sewer Construction



The majority of the sanitary sewer mains are vitrified clay pipe, which was used extensively until the early 1980s. Since then, PVC pipe has been used almost exclusively.

Sanitary System Condition Assessment

In 2015, staff initiated a methodical condition assessment program of the system, with the goal of inspecting between 5-10% of the system each year starting with the oldest areas. About 6% (7,400 metres) of sanitary sewer mains are at least 100 years old.

Over the past five years, approximately 22% (27,600 metres) of the sanitary system has been inspected using CCTV to assess pipe condition, the findings of which can be summarized as follows:

- 13,000 metres are in good condition with no or marginal structural defects;
- 7,600 metres are in fair condition with low or moderate structural defects; and,
- 7,000 metres are in poor condition with poor or very poor structural defects.

The current 2020 will be starting shortly, as CCTV is best done in the wet winter months to identify and locate leaks and improper connections. It is projected that about 7% to 8% of the network will be inspected in the winter 2020 season.

Sanitary System Infrastructure Renewal

For long-term financial planning of overall sanitary system replacement, an annual replacement target of 1.0% (1,240 metres) is included in the Official Community Plan (OCP). There are two main ways to extend the life of sewer mains: to excavate and replace them, or to reline them in place without any digging. The cost to reline sewer mains is about 20% of the cost to excavate and replace them, and the goal is to structurally reline mains where possible. Relining is possible if the sewer main is in sufficiently good condition and has enough capacity for the future.

The capital renewal and upgrade program for 2020 projected approximately 550 metres of sanitary sewer relining in the Moodyville neighbourhood. To date, about 430 metres have been relined with about 450 metres planned for January 2021. A portion of the work had to be postponed due to limitations during the spring in the early months of the COVID-19 pandemic closures. The total length of sewer relined or replaced under the 2020 program will be about 0.7%.

For 2021, about 950 metres of sewer main relining (0.7%) is projected based on the current funding in the Project Plan. As asset management plans are updated in 2021 and 2022 and additional sewer inspection is completed, an increase in sewer relining is foreseen to provide a reliable sewer system and reduce leaks in the system.

2021 Proposed Sewerage Budget

Table 1 on the following page presents the breakdown of the sewerage portion of the proposed 2020 budget. The budget is comprised of four main areas: studies, one-time capital projects, annual capital projects, and operations.

Studies are capital projects that may occur annually or less frequently to support utility planning and asset management. The largest study item is the annual condition assessment and CCTV program, which is core to ongoing sewer asset management. This represents about two-thirds of sewerage study funding and was increased over 2020 levels to accelerate condition assessment.

In 2021, the capital projects focus on relining efforts and include funding for a new vehicle to support additional capacity in the sewer maintenance, inspection, and inflow and infiltration management work.

Table 1: 2021 Sewerage Budget

	Description	Pre-Design Budget
Sewerage Studies	Sanitary Model Update and Calibration	\$ 10,000
	Annual CCTV and Sanitary Sewer Condition Assessment Reporting	\$ 100,000
	Record Keeping/As Built Project	\$ 20,000
	Sewer Utility Benchmarking	\$ 20,000
	Sewerage Studies Total:	\$150,000
Sewerage Capital Projects	Sanitary Main Capital Renewal - Relining	\$235,000
	Crew Vehicle	\$100,000
	Sewerage Projects Total:	\$335,000
Ongoing Capital Programs	Sanitary Sewer System - Renewal / Replacement by Area	\$400,000
	Sanitary Sewer IC and Connection Program	\$100,000
	Sanitary Sewer Private Side - I&I Management	\$100,000
	Trench Restoration Contingency	\$45,000
	Sanitary Sewer Manhole Replacement	\$25,000
	TFT Infrastructure Engineer	\$15,000
	Sewerage Programs Total:	\$685,000
Sewerage Maintenance	Sanitary Sewer Flow Monitoring Program	\$30,000
	Capital Paving	\$5,000
	Concrete Sidewalk Construction	\$5,000
	Pump Repairs - Pump Station Maintenance	\$20,000
	Pump Station Monitoring	\$10,000
	Sanitary Main Cleaning	\$15,000
	Sanitary Main Inspection	\$10,000
	Sanitary Main Repair or Grouting	\$50,000
	Sanitary Manhole Adjust / Repair / Inspection	\$10,000
	Sanitary Service (Connection)	\$0
	Sanitary Service Repair / Blockage Assistance / Video	\$90,000
	Sanitary Severance	\$0
	Small Equipment Service	\$1,000
	Wet Well Cleaning	\$15,000
	Sewerage Maintenance Total:	\$261,000
Sew/Drge Equipment	Small Tools and equipment	\$15,000
	Sewerage & Drainage Equipment Total:	\$15,000

Sewerage Utility Total: \$1,446,000

Budget Provision for New North Shore Wastewater Treatment Plant

Over the past six budget cycles, Council has approved budget allocations to the sewer and drainage reserve to reduce future rate shock for the North Shore Waste Water Treatment Plant (NSWWTP). Council approved a \$600,000 contribution to the reserve in 2018, \$600,000 in 2019, \$400,000 in 2020, and a proposed \$400,000 in 2021. While

construction of the new NSWWTP is underway, the construction site conditions are proving to be difficult and final completion of the project is delayed. It is foreseen that the total cost of the treatment plant will be higher than estimated and that higher costs to the sewer utility rate will be needed in the coming 3-5 years.

Sanitary Sewer Connection and Severance Fees

Staff is proposing the following increase in fixed fee costs of standard sanitary sewer connection installation and removal:

- \$5,300 for standard 100 millimetre diameter sanitary service connection installation; and,
- \$1,200 for a sanitary sewer severance.

The proposed sanitary sewer connection fee has an increase of \$200 over 2020 rates for connections and an increase of \$100 over 2020 rates for severance fees. These fee increases are intended to ensure the fee covers the actual cost of the work. All larger diameter sanitary sewer services are charged at cost, and all other fees remain unchanged.

Sanitary Sewer Efficiency and Incentives

The City's sewer system is one of the older systems in the region. Many municipalities with very old sewer systems have been aggressively replacing sewers to improve performance and capacity and reduce groundwater infiltration and rainwater inflow (inflow and infiltration) into sewers that results in regional overflows. The City of North Vancouver along with the District of North Vancouver and a number of other Metro Vancouver municipalities have chronic inflow and infiltration where the peak sewer flow could be many times higher due to rain.

This inflow and infiltration into the sewer system consumes excess capacity in the City's and Metro Vancouver's sewer mains, as well as in the treatment plant. Consequently, the cost of sewer treatment is much higher than it should be and there may be a premature need to upgrade sewer pipes and increase capacity to prevent untreated sewage being released to Burrard Inlet during large rainfall events.

The City occasionally finds private properties where rainwater pipes are intentionally or unintentionally connected to the sanitary sewer. This is in contravention to the Sewer Use Bylaw and is a leading contributor to inflow and infiltration. While property owners are required to repair these issues, there is no real incentive to do so.

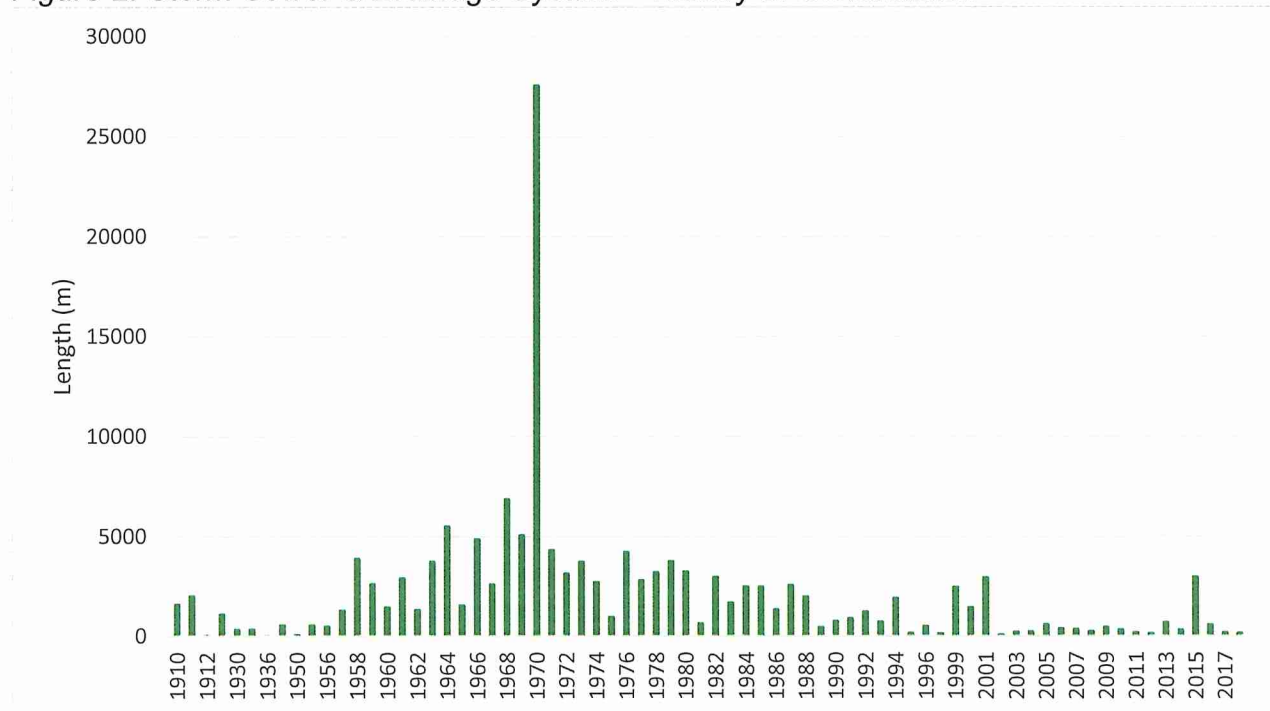
A revision to the bylaw is being proposed that would add a surcharge fee for any property with a found source of inflow from a clear combined rainwater connection. The surcharge is proposed to be 100% of the annual or metered fee, and would be applied until the issue is resolved. The property owner would have a minimum period of 6 months to repair the issue before the rate surcharge would be in effect.

This is similar to other fines currently in place in the bylaw for tampering with City facilities or unpermitted connection or severance work.

Storm Sewer and Drainage System

The average age of the City's storm sewer and drainage system is approximately 46 years, based on pipe length. While some storm sewers in Lower Lonsdale date back to the 1910s, much of the storm sewer system was constructed during the 1960s and 1970s. Figure 2 shows the history of storm sewer construction.

Figure 2: Storm Sewer & Drainage System – History of Construction



Storm sewers constructed in the late 1960s and early 1970s were often not well documented. Where the specific date was unknown, sewers were assigned a construction year of 1970, explaining the spike for that particular year.

The storm sewer system generally comprises the following primary components:

- 148 kilometres of storm sewer main;
- 1,550 manholes;
- 5,300 storm service connections;
- 3,200 catch basins; and,
- Two detention ponds (Wagg and Tempe).

The majority of the storm sewer mains are concrete pipe, but since the mid-1980s, most storm sewers have been constructed using PVC pipe, which is more efficient for construction.

The system collects precipitation and groundwater and transports it to the following receiving waters:

- Lynn Creek;
- Keith Creek;
- Mackay Creek;
- Mosquito Creek;
- Mission Creek;
- Wagg Creek;
- Thain Creek; and,
- Burrard Inlet.

Storm Sewer System Condition Assessment

Similar to the sanitary system, in 2015 staff initiated a methodical condition assessment program of the storm sewer system. Since then, approximately 12% (17,400 metres) of the storm system has been inspected using CCTV to assess pipe condition, with findings being summarized as follows:

- 10,800 metres are in good condition with no or marginal structural defects;
- 4,000 metres are in fair condition with low or moderate structural defects; and,
- 2,500 metres are in poor condition with poor or very poor structural defects.

The pipes rated in poor condition are high priority candidates to be relined or replaced in the near future.

In 2020, City Operations increased its ability to conduct pipe inspection with more capable pipe inspection cameras, and will be completing the 2020 storm sewer inspection program.

Storm Sewer and Drainage System Infrastructure Renewal

For long-term financial planning purposes, the recommended economic life of storm sewer mains, like sanitary sewers, is 100 years. However, due to land use changes and increasingly impervious urban areas, parts of the existing storm sewer system may not have the capacity to meet future system demands and will require upgrades before reaching the end of their economic lives. Conversely, some parts of the system may reach the end of their economic lives but continue to perform well and meet current demand, allowing replacement to be deferred.

Future storm system capacity planning considers the anticipated increase in total impervious (hard surface) area contained in the OCP as well as potential increased precipitation due to climate change. In March 2017, Council endorsed the Integrated Stormwater Management Plan (ISMP) which is primarily an approach to improve overall watershed health, but also serves to extend the service-capacity life of the storm sewer system. Environmental monitoring is conducted every two years in accordance with the City's Environmental Protection Plan (2000). This program has been adapted in recent years to meet regulatory requirements from Ministry of Environment and Climate Change, and is showing that ecological health of City of North Vancouver streams is either improving or is not be degraded by current development patterns.

In addition to capacity concerns of the existing system due to increased development and climate change, there are also about 30 blocks of the City that do not have storm sewers. On these blocks, drainage has historically been provided by a combination of ditches and small diameter “back-of-walk” perforated pipes or clay tiles. This combination of ditches and back-of-walk pipes does not provide an adequate level of service; therefore, new storm sewers are required. Starting in 2015, the annual budget has included funding to incrementally extend the storm sewer system to unserved blocks. In 2020, more than 1,000 metres of storm sewer has been constructed under the program in conjunction with development funding. Future project locations will be coordinated with private developments to provide service to new buildings on unserved blocks. Continuing this approach will extend service to all blocks over the next 20 to 30 years, and will also limit the rainwater inflow into the sanitary sewer.

In addition to areas with no or insufficient storm sewer servicing, 2,500 metres of the 17.4 kilometres inspected were found to have poor or very poor pipe condition rating. These sections of storm main represent almost 2% of the storm system and should be also funded for replacement.

2021 Drainage Budget

The Drainage budget, like the Sewerage budget, has a studies component for non-construction related capital costs. These costs include pipe inspection and condition assessment as well as programs for operational review and consulting services. In 2021, the studies component is primarily for pipe condition inspections and assessment.

In 2021, the Drainage Capital Plan includes the provision of additional funding for the construction of a new storm sewer at Lonsdale Avenue and Keith Road to reduce the demand on the aging Lonsdale storm sewer and direct more flow to West Keith Road and Wagg Creek. The remainder of the new drainage capital project plan is directed to the construction of smaller storm sewers in blocks that do not currently have drainage mains. This project cost shares with development contribution for detached homes and coach home type development.

Table 2 on the following page presents the breakdown of the drainage portion of the proposed 2021 budget.

Table 2: 2021 Proposed Drainage Budget

	Description	Pre-Design Budget
Drainage Studies	Record Keeping/As Built Project	\$20,000
	CCTV Inspection and data management	\$100,000
	Infrastructure Benchmarking	\$20,000
	Drainage Studies Total:	\$140,000
Drainage Capital Projects	Stormmain - Lonsdale at Keith Road	\$175,000
	Stormmain Development Supported Infill	\$440,000
	Drainage Projects Total:	\$615,000
Ongoing Capital Programs	Storm Service IC Installation and Connection Replacement	\$51,000
	Storm Manhole Installation	\$26,000
	Catch Basin Installation	\$51,000
	Driveway Culvert Installation	\$10,000
	Creek - Hazard Management	\$0
	Trench Restoration for Mainline Projects	\$45,000
	Stormwater Treatment Facilities	\$225,000
	Creek Environmental Improvements	\$100,000
	Culvert Capital Improvements	\$50,000
	TFT Infrastructure Engineer	\$15,000
	Drainage Programs Total:	\$573,000
Drainage Maintenance	Storm Main Repair	\$31,000
	Storm Main Blockage Response	\$21,000
	Storm Main Inspection	\$10,000
	Storm Main Cleaning	\$26,000
	Storm Service - New Connection	\$0
	Storm Service - Severance	\$0
	Storm Connection Blockage Assistance / Video / Clear	\$51,000
	Storm Manhole Adjust / Repair / Inspection	\$15,000
	Catchbasin Adjust / Repair / Inspection	\$26,000
	Catchbasin Cleaning	\$41,000
	Culvert Repair / Clean	\$10,000
	Ditch Maintenance	\$15,000
	Water Course Maintenance	\$10,000
	Spill Response	\$51,000
	Drainage Maintenance Total:	\$307,000

Drainage Total: \$1,635,000

Storm Sewer Connection and Severance Fees

Staff is proposing the following increase in fixed fee costs of standard storm sewer connection installation and removal:

- \$5,100 for standard 150 millimetre diameter sanitary service connection installation;
- \$1,200 for a sanitary sewer severance; and,
- \$2,500 for a new storm sewer inspection chamber.

The proposed sanitary sewer connection fee has an increase of \$300 over 2020 rates for connections and an increase of \$100 over 2020 rates for severance fees. These fee increases are intended to ensure the fee covers the actual cost of the work and are aligned with sanitary sewer increases.

A new fee for the installation of a storm sewer inspection chamber is proposed for 2021 to provide the same flexibility as the fees currently provided for sanitary connections. Inspection chambers are added to new connections but exist for older connections. This fee may be applied for a large renovation to allow inspection and maintenance of storm sewer connections proactively or in an emergency.

The City also has a storm sewer extension fee for single unit detached homes. In parts of the City, modern storm sewers were not constructed during the 1960s. These areas drain to shallow ditches or shallow segmental clay pipe and are not suitable for servicing modern homes with deep foundations and intensive site coverage. To support this development, the City created a fixed fee for storm extensions in 2016 to provide some cost certainty for homeowners, which was \$1,100/metre based on the width of property frontage in 2020. This rate is being proposed to be increased to \$1,300/metre to more closely reflect the cost of installation.

All larger diameter storm sewer services are charged at cost and all other fees remain unchanged.

Storm Sewer Efficiency and Incentives

The City's storm sewer system is a combination of old and new infrastructure with a range in size and condition. As part of the City's ISMP, a number of initiatives have been developed to improve the utility efficiency, environmental performance, and longevity. The approach of the plan is to introduce rainwater source controls to lessen the impact of development and climate change on the City's storm water utility and the natural receiving waters of Mosquito Creek, Mackay Creek, Lynn Creek, and Burrard Inlet. The strategy includes:

- All new private construction shall include rainwater infiltration or detention facilities to limit the water released from new development;
- All new City hardscape shall include opportunities to mitigate and treat the hard surfaces at the source with storage and infiltration works;
- City stormwater outfalls will incrementally be upgraded to include stormwater quality treatment works through underground treatment filters; and,
- Instream habitat and barriers to fish passage shall be incrementally improved and barriers removed.

Through implementing private and public rainwater source controls, the City can extend the useful life of the storm sewer in the face of increased intensity of land use and precipitation associated with climate change. Upgrades will be used strategically to address chronic capacity problems or sections of poor condition.

FINANCIAL IMPLICATIONS

The information provided in this report is intended to demonstrate the need for adequate funding for current operating and maintenance requirements, replacement and renewal projects, and that the Sewerage and Drainage Utility remains self-financing.

Details of the actual rate increase impacts are specified in the companion utility rate report.

INTERDEPARTMENTAL IMPLICATIONS

The development of financial plans for the utility has been a co-ordinated effort between the Engineering, Parks and Environment and Finance Departments.

The 2018-2022 Council Strategic Plan identifies a vision to create the Healthiest Small City in the World, through five key priorities. Provision of an affordable, safe, and sustainable sewer and drainage utility to meet the needs of future residents supports the priorities of:

- A City for People; and
- A Liveable City.

The annual sewer and drainage utility project plan provides sustainable funding for ongoing operation, asset management and renewal of our sanitary and storm sewers. The utility is fundamental for all residents and businesses in the City with overarching goals for public health and safety.

The 2014 Official Community Plan includes the guiding principles to serve the community vision for infrastructure management, which reads as follows:

- Goal 8.1 Provide the community with public infrastructure that protects the natural environment at an affordable cost;
- Goal 8.2.1 Maintain or improve service levels related to water supply, sewers, transportation, communication and energy infrastructure;
- Goal 8.2 Employ a proactive approach to infrastructure maintenance and upgrades;
- Goal 8.2.2 Identify and monitor the condition of our infrastructure on a regular basis in order to identify the remaining operation life of individual elements, and to identify and rectify weaknesses before failure, thereby optimizing capital and maintenance program expenditures;
- Goal 8.3 Use innovative planning and design to ensure infrastructure longevity.

RESPECTFULLY SUBMITTED:



Douglas T. Pope, P. Eng.
Director, Engineering, Parks and Environment



The Corporation of **THE CITY OF NORTH VANCOUVER**
ENGINEERING, PARKS AND ENVIRONMENT DEPARTMENT

INFORMATION REPORT

To: Mayor Linda Buchanan and Members of Council

From: Douglas Pope, Director, Engineering, Parks and Environment

Subject: 2021 SOLID WASTE UTILITY UPDATE

Date: November 25, 2020 File No: 11-5380-01-0001/2021

PURPOSE

The purpose of this report is to provide background information regarding the City's Solid Waste Utility which includes residential garbage, Green Can, and Eco Levy programs. This report is a companion document to the annual rate setting report, which proposes an overall increase to the 2021 Solid Waste Utility of 8.4% (\$23.90) for single-family units and 15.0% (\$3.30) for multi-family units.

BACKGROUND

The City's Solid Waste Utility provides for the collection and disposal of residential curbside garbage and the recycling of Green Can materials (yard trimmings and food scraps) from single-family homes and smaller multi-family buildings. As well, public realm litter collection (including parks, streetscape, and plazas) and community waste reduction initiatives are funded by the Eco Levy. Until July 1, 2020, the City also provided residential recycling collection; however, responsibility for that service now belongs to Recycle BC, the industry appointed product stewardship organization that is responsible for the recycling of packaging and paper products (PPP) in BC.

DISCUSSION

Residential Solid Waste and Green Can Collection

The City provides collection and disposal of residential garbage and Green Can materials from single-family homes and multi-family properties with nine units or less that are appropriate for curbside collection. The garbage and Green Can material collected by City staff is then transported to the North Shore Transfer Station, which is operated by

Metro Vancouver. These materials are then transported by Metro Vancouver to facilities either within or outside of the region for processing.

Each year, Metro Vancouver establishes the rate or “tipping fee” for materials received at their Transfer Stations, with separate rates for garbage and organics (Green Can). The 2021 Metro Vancouver tipping fee for municipal garbage disposal will be \$117 per tonne, representing a \$4 per tonne increase from 2020. These increased operating costs result in a 3.5% increase in the Refuse Levy for a single unit residence to \$119. The 2021 tipping fee for organics remains at \$100 per tonne, but rates are anticipated to increase in 2022.

Solid Waste Management Service Bylaw Review

The provision of solid waste services in the City of North Vancouver operate under the auspices of the Solid Waste Management Service Bylaw. The current bylaw was adopted in 1997 and has been amended a number of times in recent years with the significant changes to services and delivery models, such as the introduction of the Eco Levy, residential Green Can program, bi-weekly garbage collection, and most recently, Recycle BC’s responsibility for residential recycling. Given these changes, staff are proposing amendments to the current bylaw as an interim measure, but will be drafting a new Solid Waste bylaw for consideration by Council in early 2021.

The proposed interim bylaw amendments include the annual updates to the rates, but also include a proposed six can (or bag/bundle) weekly limit for curbside Green Can collection, ending the City’s limited commercial tag garbage collection program, and deleting reference to the residential recycling collection program, which is now provided by Recycle BC.

Unlike many municipalities, the City currently has no limit on the number of Green Can containers that can be placed each week for curbside collection. While most households routinely place one or two containers per week, there are times where larger scale residential landscape projects are completed either by owners or contractors, and the resulting green waste (occasionally as many as 20 bags) is then placed for collection by City crews. The proposed new weekly limit of six containers (or bags or bundles) is consistent with Districts of West Vancouver and North Vancouver, and appropriate for residential food scraps and routine yard waste, while larger volumes of landscape materials should be taken directly to the North Shore Transfer Station.

The City provides a Residential Garbage Tag program where residents can purchase a tag for \$4.00 permitting them to set out more than the allotted two 77 litre cans. Though the City continues to encourage waste reduction, this program has been successful in supporting residents who have a need for occasional additional garbage collection. In order to continue providing this additional service the recommendation is to increase the rate to \$6.00 to better reflect the current costs of operations and disposal rates. This increase is consistent with District of West Vancouver who also offers this service.

The vast majority of commercial properties in the City have relied on private solid waste haulers to collect their refuse and recycling for many years. However, the City has continued to offer a small-scale curbside garbage program where businesses purchase garbage tags for \$4.50 each, and the containers are then collected by City crews, as

needed. The program is currently used by only 12 businesses, which purchase at total of approximately 400 tags per year (total revenue of \$1,800). This program presents a number of challenges, and staff are recommending that it be discontinued. As the City's regular weekly collection routes are focused on residential neighbourhoods, it is inefficient to have to also check commercial areas to see if there are bags or cans that may also have been placed for pick-up. In addition, the nature of these commercial businesses places a higher risk of banned materials being comingled with the garbage, such as hazardous materials or recyclables.

Recycling Service Update

As of July 1, 2020, Recycle BC has taken responsibility for the delivery of residential recycling services on the North Shore. As a result, the cost of the recycling collection program and contract administration are no longer funded directly by City residents through a Recycling Levy in the Solid Waste Utility. By transitioning residential recycling services to Recycle BC, the City is supporting the principles of extended producer responsibility. While reference to the City's Residential Recycling Collection Program can now be removed from the Solid Waste Management Service Bylaw, the bylaw still requires that all City residents and business dispose of all waste and recyclable materials appropriately.

Recycling Drop-off Area at the North Shore Recycling and Waste Centre

The consolidated Recycling Drop-Off Area adjacent to the main North Shore Transfer Station has been in operation since January 2017. The Recycling Drop-Off Area is operated by Metro Vancouver, and accepts a wider range of recyclable items, such as metal, electronics, paints, and textiles. Additionally, the depot added an ENCORP "Return-It Express" bottle return in summer of 2020. The recycling portion of the depot is supported by the three North Shore municipalities, along with funding from product stewards under the Province's Extended Producer Responsibility programs.

Reduction in Waste Tonnages

The City continues to make progress in diverting food scraps and other organic material from the waste stream through the curbside Green Can program, along with the implementation of bi-weekly garbage collection in January 2014. Residential garbage collected by the City continues to decrease, with 204 kilograms/household in 2019, representing a 3.3% decrease from 2018. Regionally, Metro Vancouver is among the most successful jurisdictions in North America in reducing waste, with a 64% diversion rate of municipal solid waste disposal. The City has done well with an overall diversion rate of 72%.

Zero Waste Challenge

Management of the City's Solid Waste Utility is guided by the regional Integrated Solid Waste and Resource Management Plan (ISWRMP). The current ISWRMP was written in 2010 and Metro Vancouver is currently beginning the process of updating the plan over the next three years.

A key deliverable of the ISWRMP is the implementation of the region's Zero Waste Challenge. This initiative requires emphasis in the following areas:

- Education and outreach to increase waste reduction and recycling across all sectors (residential, institutional, commercial, and industrial);
- Recycling for all multi-family and institutional, commercial, and industrial buildings;
- Organics including food waste diversion for single-family, multi-family, institutional, commercial and industrial buildings;
- Increased refuse and recycling on demolition and construction sites; and,
- Acceleration of provincial Extended Producer Responsibility (EPR) programs.

Zero Waste is one of the four main themes within the City's Environment Strategy, which is currently under development. Staff are working to complete the first phase of this work and further information will be brought forward for Council's consideration in 2021. Staff are working to identify and prioritize potential policies and programs to support the transition to a more circular economy, including accelerating waste diversion in the residential, commercial, and demolition and land clearing sectors.

For many years, the City has provided a Zero Waste Coach program to support residents, schools and businesses with waste diversion. The Zero Waste Coach is available to visit buildings to assess needs and space challenges, resolve collection challenges, provide education sessions at strata meetings or lobby events, and assist with signage and posters. This program plays a key role in ensuring that City residents, schools and businesses are complying with regional bans and are maximizing waste diversion opportunities available in the City and region.

The Corporate Zero Waste Program is used within all civic facilities. The program is a key component of the City's Corporate Climate Action Plan and has been implemented at City Hall, the Operations Centre, Fire Hall, RCMP, Library, Centennial Theatre, Harry Jerome and John Braithwaite Recreation Centres, North Shore Neighbourhood House and the Community Policing Office. The program has reduced the amount of garbage sent to landfill by as much as 70% in some facilities. Staff continue to provide outreach to these facilities to further increase diversion and reduce contamination.

Eco Levy

In 2011, the City implemented an Eco Levy so that waste diversion programs are funded by both residential and non-residential sectors. The Eco Levy ensures that the City's litter program and additional waste diversion initiatives, such as the provision of public realm recycling and the dog waste collection (red bin) program, are now managed and funded through the City's solid waste utility. For 2021, the Eco Levy has been increased by 15% as a result of increasing operational costs from collecting larger volumes of litter. Since the onset of the COVID-19 pandemic, the use of City parks, streetscapes and plazas has increased significantly. The additional funding will also be used to increase the number of multi-stream refuse and recycling receptacles in the public realm to help address the litter volume increases and to support the City's waste diversion goals.

FINANCIAL IMPLICATIONS

The information in this report is provided to help ensure that the Solid Waste Utility remains self-financing, and that adequate funds for current operating requirements and reserves are provided.

Details of the actual rate increase impacts are specified in the companion rate report authored by the Finance Department.

INTERDEPARTMENTAL IMPLICATIONS

Financial planning for the solid waste programs has been a coordinated effort between the Finance, Planning and Development, and the Engineering, Parks and Environment Departments.

STRATEGIC PLAN, OCP OR POLICY IMPLICATIONS

The waste management services delivered through the Solid Waste Utility support the City's corporate and community greenhouse gas reductions strategies, as well as the Official Community Plan goal 8.1: *Provide the community with public infrastructure that protects the environment at an affordable cost.*

Waste reduction is a significant factor in reducing greenhouse gas emissions and minimising the environmental impacts from the production and consumption of energy, goods, and the delivery of services in the community. Focusing resources on this work supports Council's Strategic Plan vision to be the "healthiest small city in the world" with a priority of creating "a liveable city" that leads the way in climate action and acts as a steward of the environment for future generations.

RESPECTFULLY SUBMITTED:



Douglas T. Pope, P. Eng.
Director, Engineering, Parks and Environment

THE CORPORATION OF THE CITY OF NORTH VANCOUVER

BYLAW NO. 8811

A Bylaw to Amend “Water Utility Bylaw, 1994, No. 6417”

The Council of The Corporation of the City of North Vancouver, in open meeting assembled, enacts as follows:

1. This Bylaw shall be known and cited for all purposes as **“Water Utility Bylaw, 1994, No. 6417, Amendment Bylaw, 2020, No. 8811”**.
2. “Water Utility Bylaw, 1994, No. 6417” is amended as follows:
 - A. SCHEDULES:
 - (1) Delete Schedule “A” in its entirety and replace with the new Schedule “A” attached to this bylaw.
 - (2) Delete Schedule “B” in its entirety and replace with the new Schedule “B” attached to this bylaw.
3. This Bylaw shall be effective as of the 1st day of January, 2021.

READ a first time on the <> day of <>, 2020.

READ a second time on the <> day of <>, 2020.

READ a third time on the <> day of <>, 2020.

ADOPTED on the <> day of <>, 2020.

MAYOR

CORPORATE OFFICER

SCHEDULE A

Water Services

2021 CONNECTIONS AND SEVERANCE RATES

1. Water Service Connections

Size	Fee
Standard 19 mm	\$5300
>19 mm	At Cost, with a minimum charge equal to the standard fee.

2. Water Service Severance

For Standard 19 mm Replacement	\$1200
>19 mm service replacement	At Cost, with a minimum charge equal to the standard fee.

3. Water Connection Reuse \$2500

4. Turn on/off \$50

5. Flat fee for installation and removal of temporary meter \$400

A 50% rebate is available for standard 19mm water service replacements that are installed less than 2 years in advance of a City initiated programmed replacement of water services within the block.

SCHEDULE B

2020 Water Use Rates

1. WATER RATES

1. For Temporary Service during construction

Building Size (Total Floor Area)

46 sq. m to 460 sq. m	\$165
Over 460 sq. m but not exceeding 4,600 sq. m	\$325
Over 4,600 sq. m but not exceeding 23,000 sq. m	\$540
Over 23,000 sq. m	\$860

2. Flat Rates (un-metered premises)

Single Unit Dwelling	\$514 per year
Duplexes (including infills) per unit	\$431 per year
Multiple Unit Dwellings (including extra living units within single unit dwellings, apartments, condominiums, garden apartments and coach houses) for each dwelling	\$301 per year
Stores, offices, other commercial premises not elsewhere designated; Churches and Public Halls per fixture	\$115 per year
Water Cooling Units	\$800 per year
Schools per Classroom	\$369 per year
Minimum charge for any service	\$514 per year

2. METERED WATER RATES

For commercial and industrial properties, hospitals and schools served by water meters the following scale of charges shall apply:

1.	January 1 to May 31	\$ 1.2823 per cubic metre
	June 1 to September 30	\$ 1.6029 per cubic metre
	October 1 to December 31	\$ 1.2823 per cubic metre

A minimum bill for 30 cubic metres per month will be charged if a lesser quantity or no water is consumed.

2. Metered Charges for Mixed Use Premises

The charge for Mixed Use Premises on metered rates shall be the greater of the flat rate for Multiple Dwelling Units provided in this Schedule times the number of dwelling units or the metered rates.

3. MISCELLANEOUS CHARGES

1. Testing a Meter as Provided in Section 703

For meters 50 mm and smaller	\$350
For meters over 50 mm	\$500

4. DATE OF PAYMENT AND PENALTIES

1. FOR FLAT RATE (un-metered premises)

1. Rates set out above shall be due on the last business day in February each year.
2. A penalty of five percent (5%) shall be added to rates which are not paid on or before the last business day of February and a further penalty of five percent (5%) shall be added to rates which are not paid before the last business day of April in the year which the rates are due.

2. FOR METERED PREMISES

1. A penalty of five percent (5%) shall be added to rates which are not paid on or before the due date as indicated on metered account bills.

THE CORPORATION OF THE CITY OF NORTH VANCOUVER

BYLAW NO. 8812

A Bylaw to Amend “Sewerage and Drainage Utility Bylaw, 1995, No. 6746”

The Council of The Corporation of the City of North Vancouver, in open meeting assembled, enacts as follows:

1. This Bylaw shall be known and cited for all purposes as **“Sewerage and Drainage Utility Bylaw, 1995, No. 6746, Amendment Bylaw, 2020, No. 8812”**.
2. “Sewerage and Drainage Utility Bylaw, 1995, No. 6746” is amended as follows:
 - A. SCHEDULES:
 - (1) Delete Schedule “A” in its entirety and replace with the new Schedule “A” attached to this bylaw.
 - (2) Delete Schedule “B” in its entirety and replace with the new Schedule “B” attached to this bylaw.

READ a first time on the <> day of <>, 2020.

READ a second time on the <> day of <>, 2020.

READ a third time on the <> day of <>, 2020.

ADOPTED on the <> day of <>, 2020.

MAYOR

CORPORATE OFFICER

SCHEDULE A

Sewer and Drainage Services

2021 CONNECTION AND SEVERANCE FEES

1. SANITARY CONNECTION

Size	Fee
Standard 100mm Replacement or Relining	\$5,300
>100mm	At Cost, with a minimum charge equal to the standard fee
Inspection Chamber	\$2,500
Private Sewer Connection Inspection and Certification Deposit	\$2,500
Sanitary Severance	\$1,200

2. STORM CONNECTION

Size	Fee
Standard 150mm Replacement or Relining	\$5,100
>150mm	At Cost, with a minimum charge equal to the standard fee
Inspection Chamber	\$2,500
Storm Sewer Extension and Connection for Single Unit Developments exceeding \$150,000 and excluding subdivision	\$1,300 per linear metre based on the total width of the lot serviced
Storm Severance	\$1,200
Off-Site Stormwater Management Facility Fees	\$32 per square metre of premises lot area or at cost to provide works on public property
Storm Collection System Ditch Culvert	\$250 per linear metre based on Construction the length of the culvert pipe

SCHEDULE B

2021 Sewer and Drainage Rates

The following rates shall apply to all real property used for one or more of the following purposes and from which there is a connection to the sewer systems of the City:

1. SANITARY SEWER

2021 Rates

1. **Annual Sewer Parcel Tax** \$ 0 per year

2. Flat Rates (un-metered premises)

Single Unit Dwelling \$444 per year

Duplexes (including infills) per unit \$411 per year

Multiple Unit Dwellings (including extra living units
within single unit dwellings, apartments,
condominiums, garden apartments and coach houses)
for each dwelling \$277 per year

Stores, offices, other commercial premises
not elsewhere designated; Churches and Public Halls
per fixture \$107 per year

Minimum charge for any service is \$444 per year

Rate surcharge for combined connections \$444 per year

3. Metered Rates

For commercial and industrial properties, hospitals and schools served by water meters the following scale of charges shall apply:

\$1.8123 per cubic metre

A minimum bill for 30 cubic metres per month will be charged if a lesser quantity or no water is consumed.

Rate surcharge for combined connections \$1.8123 per cubic metre

4. GVS & DD Special Industrial User Charge

Where the owner or occupier of a parcel of real property is a special industrial user, that owner or occupier shall pay to the municipality for the use of the sewerage system the greater of:

- A. the charges payable under Schedule B, subsections .1, .2, and .3, or
- B. the GVS & DD special industrial user charge as determined by the GVS & DD for that owner or occupier, prorated if necessary for monthly or quarterly billings.

5. Charges for Contaminated Groundwater Discharges to Sanitary Sewer

Ground Water Discharges \$0.33/m³, or as determined by the GVS & DD.

6. Metered Charges for Mixed Use Premises

The charge for Mixed Use Premises on metered rates shall be the greater of the flat rate for Multiple Dwelling Units provided in this Schedule times the number of dwelling units or the metered rates.

7. Sewer Use Charges

Users having a metered water service, who return less than 40% of water purchased to the sanitary sewer system, may make application to the Director of Finance for a Flat Rate Sewer charge based on their number of plumbing fixtures discharging into the sewer outlet of the premises, together with a charge of \$400 per million dollars of taxable assessment.

Applications for flat rate sewer charges must be received prior to December 1 of the year of application, and must include a full description of water usage and the total number of fixtures on the property, as described in Schedule "B" of this Bylaw.

The Director of Finance will examine each application and the supporting documentation and may visit the site to ensure that the conditions described are representative of a complete year's usage and that the total number of fixtures is accurate.

The Director of Finance will either reject the application providing reasons, or approve a Flat Rate charge. This charge, if approved, will be billed annually.

Any user rate so adjusted will be subject to periodic review by the Director of Finance and may, at any time, be returned to a user rate based on metered water consumption and the applicant so notified.

In no case shall the annual charge for a sewer-user having a metered water connection be less than the rate for a Single Unit Dwelling.

8. Date of Payment, Penalties, and Surcharge Fees

A. FOR FLAT RATE (un-metered premises)

- 1) Rates set out above shall be due on the last business day in February each year.
- 2) A penalty of five percent (5%) shall be added to rates which are not paid on or before the last business day of February and a further penalty of five percent (5%) shall be added to rates which are not paid on or before the last business day of April in the year which the rates are due.
- 3) A 100% surcharge fee will be applied to any sewer connections with combined storm drainage inflow to a sanitary connection contrary to City bylaws. The surcharge would be applied at the discretion of the City Engineer a minimum of 6 months following notification, and would be applied to the annual bill until the combined inflow is separated.

B. FOR METERED PREMISES

- 1) A penalty of five percent (5%) shall be added to rates which are not paid on or before the due date as indicated on metered account bills.
- 2) A 100% surcharge will be will be applied to the unit rate for any sewer connections with combined storm drainage inflow to a sanitary connection contrary to City bylaws. The surcharge would be applied at the discretion of the City Engineer a minimum of 6 months following notification, and would be applied to the annual bill until the combined inflow is separated.

2. STORM DRAINAGE

1. The Storm Drainage Levy is based on the taxable assessment of a property and is charged on the following classes per thousand dollars of taxable assessment based on the BC Assessment assessed value of:

- Residential
- Utility
- Major Industry
- Light Industry
- Business
- Recreation/non-profit

and is included on the Property Tax Notice payable annually in July of each year as set by Council.

2. Storm Drainage Charges

A user, who is aggrieved by the application of Schedule B 2.1 to a specific parcel, may make application to the City Engineer, in writing, stating the basis for the complaint. The City Engineer will respond, in writing, to the applicant's concern outlining any appropriate adjustment that may be authorized.

3. Penalty

A 5% penalty will be added to the balance owed if unpaid by the end of the first working day in July, and a further 5% penalty will be added to the balance owed if unpaid by the first working day of September.

THE CORPORATION OF THE CITY OF NORTH VANCOUVER

BYLAW NO. 8813

A Bylaw to amend “Solid Waste Management Service Bylaw, 1997, No. 6920”

The Council of The Corporation of the City of North Vancouver, in open meeting assembled, enacts as follows:

1. This Bylaw shall be known and cited for all purposes as **“Solid Waste Management Service Bylaw, 1997, No. 6920, Amendment Bylaw, 2020, No. 8813”**.
2. “Solid Waste Management Service Bylaw, 1997, No. 6920” is amended as follows:
 - A. In Part 3 – Interpretation, Section 301 Definitions:
 - (1) By deleting the definition for “Garbage Collection” and replacing it with the following:

“Garbage Collection” means the system of collection and disposal of garbage.
 - (2) By deleting the definition for “Recycling Container” in its entirety.
 - B. In Part 5 – Collection of Garbage and Commercial Garbage, by deleting Section 504 Commercial Garbage Collection Service in its entirety.
 - C. In Part 6 – Collection of Recyclable Materials, Yard Trimmings, and Food Scraps:
 - (1) By replacing the title to read: “PART 6 – COLLECTION OF YARD TRIMMINGS AND FOOD SCRAPS”;
 - (2) By deleting Sections 601, 602 and 603 in their entirety.
 - D. In Part 7 – Containers, by deleting Section 702 Recycling Containers in its entirety.
 - E. In Part 8 – Requirement for Payment, by deleting Subsection 801.3 in its entirety.

- F. In Part 11 – Schedules, by deleting Schedules “A”, “B”, “C”, “D”, “E” and “F” in their entirety and replacing with the new Schedules “A”, “B”, “C”, “D” and “E” attached to this bylaw.

READ a first time on the <> day of <>, 2020.

READ a second time on the <> day of <>, 2020.

READ a third time on the <> day of <>, 2020.

ADOPTED on the <> day of <>, 2020.

MAYOR

CORPORATE OFFICER

PART 11 – SCHEDULES

SCHEDULE “A”

Schedule of User Charges for Solid Waste Management Service

Definitions

1. *Standard Residential Service* = A maximum of two (2) garbage containers per dwelling unit collected every other week.
2. *Green Can Collection Service* = Weekly collection of a maximum total of six (6) Green Can designated containers, kraft bags and/or tied bundles of yard trimmings.

Annual User Charge

GARBAGE	2021
1. Per dwelling unit for single unit dwelling, duplex and infill	\$119.00
2. Per dwelling unit for secondary suite, coach house and multi-unit dwellings greater than 2 units	\$80.00
3. Per Residential garbage tag	\$6.00
GREEN CAN	2021
1. Per dwelling unit for single unit dwelling, duplex and infill	\$138.00
2. Per dwelling unit for secondary suite, coach house and multi-unit dwellings greater than 2 units	\$83.00
ECO LEVY	
<p>The Eco Levy is based on the taxable assessment of a property and is charged on the following classes per thousand dollars of taxable assessment based on the BC Assessment assessed value of:</p> <ul style="list-style-type: none">• Residential• Utility• Major Industry• Light Industry• Business• Recreation / non-profit <p>and is included on the Property Tax Notice payable annually in July of each year as set by Council.</p>	

Penalty

1. A 5% penalty will be added to the balance owed if unpaid by the end of the first working day in July, and a further 5% penalty will be added to the balance owed if unpaid by the first working day of September.

SCHEDULE "B"

Garbage Information Tag

Garbage put out for collection and not collected will be marked with a Garbage Information Tag as follows:

CITY OF NORTH VANCOUVER
GARBAGE INFORMATION TAG

The container will not be emptied in the future because:

- ☐ Garbage can is larger than 77 litres.
- ☐ Garbage can is in poor condition and requires replacement.

The garbage was not taken because:

- ☐ Garbage must be placed at property line or curb by 7:30am.
- ☐ Over 2 can or bag limit.
- ☐ Cans or bags are overweight. (20 kg/45 lb. limit)
- ☐ Recyclables found in garbage can.
- ☐ Access to garbage must not be obstructed.
- ☐ Bungee Cords still attached.
- ☐ Contents frozen inside can.
- ☐ Other _____

GENERAL ENQUIRIES 604-987-7155
www.cnv.org/collection

SCHEDULE “C”

Residential Garbage Tag

A Residential Garbage Tag as set out below permits a garbage container to be picked up from a dwelling unit even though the garbage container limit has been reached by that dwelling unit. The cost of a Residential Garbage Tag is set out in Schedule “A” attached to this Bylaw, and can be purchased from the Treasury Department, City Hall, 141 West 14th Street, North Vancouver, BC V7M 1H9.



No. 05201

city
of north
vancouver

Residential Garbage Tag

Name: _____

Address: _____

Date: _____

This tag is good for the removal of one 77 litre bag
or can of garbage, not to exceed 20 kg in weight.

SCHEDULE “D”

Recyclable Materials Collected by the Solid Waste Management Service

Packaging and Paper Products (PPP)	
1. Mixed Paper Recyclables	Newspaper, flyers, writing/home office paper, magazines, catalogues, boxboard boxes, molded boxboard, corrugated cardboard, multi-layer paper bags, paper bags, non-foil gift- wrap, greeting cards, telephone books, shredded paper. Paper which is soiled, food contaminated, or wet should be placed into containers intended for organic material collection.
2. Glass Containers Recyclables	All clear or coloured glass food and beverage bottles or jars.
3. Non-glass Mixed Container Recyclables	All hard plastic containers, steel containers, aluminum foil and containers, cold drink cups, paper cups, gable top cartons, aseptic containers, spiral wound paper cans, empty aerosol cans, frozen dessert boxes

SCHEDULE “E”

Unacceptable Materials for Garbage Collection

Recyclable materials as described in Schedule “D”:

- corrugated cardboard
- newspapers and flyers
- mixed papers including magazines, telephone directories and boxboard
- rigid plastic containers numbered 1, 2, 4 and 5

Food Scraps and Yard Trimmings

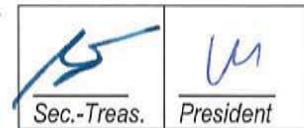
Materials subject to provincially required stewardship programs:

- beverage containers except milk and milk products
- household paints, stains and their containers including aerosols
- waste lubricating oil and its containers
- oil filters
- pesticides, solvents and flammable liquids and their containers
- prescription and non-prescription drugs and their containers
- automotive tires
- lead acid batteries
- electronic goods including computers and their peripherals, desk-top printers and televisions

Hazardous waste:

- Explosive, toxic, corrosive, caustic, hot or flammable, combustible or oxidizing substances, objects or mechanisms
- Hypodermic needles
- Bio-medical waste, sharps or infectious materials
- Dead animals, animal faeces or viscera
- Sod, rocks, gravel, soil, cement, asphalt and other similar material
- Construction and demolition material including gypsum board (gyproc)
- Derelict motor vehicles or motor vehicle parts
- Materials originating from industrial and/or agricultural operations
- Refillable propane cylinders
- Scrap metal
- Any refrigerator, freezer or other large appliance
- Waste materials not identified as acceptable for land filling pursuant to any permit or certificate issued by the Provincial Ministry of Water, Land and Air Protection
- Industrial or commercial spools

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T 604.983.7305 F 604.985.1573 E info@cnv.org
141 West 14th Street, North Vancouver BC V7M 1H9

REPORT

To: Mayor Linda Buchanan and Members of Council

From: Ben Themens, Secretary-Treasurer, Lonsdale Energy Corp.

SUBJECT: LONSDALE ENERGY CORP.
DIRECTOR NOMINATION

Date: November 24, 2020

RECOMMENDATION:

PURSUANT to the report of the Secretary-Treasurer of Lonsdale Energy Corp., dated November 24, 2020, entitled "Lonsdale Energy Corp. – Director Nomination":

THAT the Unanimous Consent Resolutions of the Shareholder of Lonsdale Energy Corp. attached to the report of the Secretary-Treasurer of Lonsdale Energy Corp. (**Attachment 1**) be endorsed;

AND THAT the Mayor and City Clerk be authorized to sign and seal the said resolution;

ATTACHMENTS:

1. Unanimous Consent Resolutions of the Shareholder of Lonsdale Energy Corp. (in lieu of the general meeting);
2. Letter from Director Themens, entitled "Lonsdale Energy Corporation Board Membership", dated May 8, 2019

PURPOSE:

This report recommends that a resolution be adopted by the City of North Vancouver, sole shareholder of Lonsdale Energy Corp. (LEC), in lieu of holding a general meeting to appoint a new Director to the LEC Board of Directors.

DISCUSSION:

In May 2019, Mr. Themens who has been a Board member since 2006, shared his request to be excused from the LEC Board due to workload and personal reasons (**Attachment 2**). The corporation has recently hired LEC's first CEO, Mr. Karsten Veng. Endorsement of the attached Unanimous Consent Resolutions of the Shareholder of Lonsdale Energy Corp. (**Attachment 1**) would replace Mr. Themens with Mr. Veng on the LEC Board.

It should be noted that Article 18.1 of the Articles of the Company specifies the officers that must be appointed to the Company as follows: a Chairperson of the Board, a Vice-Chairperson, a President and a Secretary-Treasurer. The appointment of a General Manager as Officer is allowed under the Articles but is not required.

It is planned that Mr. Veng will be appointed to the position of Secretary-Treasurer following his election to the Board.

RESPECTFULLY SUBMITTED BY:


Ben Themens, MBA, P.Eng., CGA
Secretary-Treasurer, LEC

**UNANIMOUS CONSENT RESOLUTIONS OF THE SHAREHOLDER
OF**

LONSDALE ENERGY CORP.
(the "Company")

in lieu of the general meeting

The undersigned, being the sole shareholder of the Company entitled to vote at an annual general meeting, hereby consents to and adopts in writing the following resolutions:

RESOLVED THAT:

1. the following person, who has been elected as director of the Company, at the last annual general meeting of the Company, is hereby thanked for his service as director of the Company:

Ben Themens

2. the following person, who has consented in writing to act as a director, is hereby elected as director of the Company, to succeed the above-mentioned director, hold office until the next annual general meeting of the Company or unanimous resolutions consented to in lieu of holding an annual general meeting, or until his successor is appointed:

Karsten Veng

DATED as of December 7, 2020.

**THE CORPORATION OF THE CITY OF
NORTH VANCOUVER**

Mayor Linda Buchanan

Karla Graham, City Clerk

Ben Themens, MBA, P.Eng., CPA, CGA

May 8, 2019

Ms. Leanne McCarthy, President & Chairperson of the Board
Lonsdale Energy Corporation Board
141 West 14th Street
North Vancouver, BC
V7M 1H9

Subject: Lonsdale Energy Corporation Board Membership

Ms. McCarthy:

For most of the past 15 years, I have occupied dual positions at the City of North Vancouver and Lonsdale Energy Corporation (LEC). Since September 2006, I have also filled, among other positions, the role of LEC Board Director and Secretary-Treasurer.

It has been my pleasure to fill these non-remunerated positions during those years. However, as I mentioned to you a little more than a month ago, I have been considering the challenges related to filling both positions of Executive Director, LEC and City of North Vancouver, Director of Finance /CFO given the growth of LEC and its carbon emission reduction goals.

This letter is to inform you that after careful consideration of workload and personal matters, I wish to reduce my involvement in LEC. I would like to concentrate my time and energy on my formal position of City of North Vancouver, Director of Finance /CFO.

It has been an extreme pleasure to have had the opportunity of participating in the growth of LEC from start-up to a company with more than \$4 million in sales and \$35 million in assets. For the first time since inception, the corporation will have a shareholder's equity surplus in its 2018 financial statements. The recent inter-connection of the network to cover most of the municipality's densely populated area is an achievement that will facilitate the introduction of alternative energy sources.

LEC will be presenting its 2018 Financial Statements to its shareholder, the City of North Vancouver, near the end of May or mid-June. At that time, as it is the case each year, the

shareholder will select a Board of Director for the coming year. I would appreciate it, if I could be excused from consideration to fill such a position at that time.

It has been truly an honour to have been given the opportunity to serve the Corporation.

I wish you and the Corporation all the best,



Ben Themens, MBA, P.Eng., CPA, CGA
Executive Director, Secretary-Treasurer
Lonsdale Energy Corp.

c.c.: Jessica McIlroy, Vice-Chairperson of the Board

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 Department Manager	 Director	 CAO
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The Corporation of **THE CITY OF NORTH VANCOUVER**
PLANNING & DEVELOPMENT DEPARTMENT

REPORT

To: Mayor Linda Buchanan and Members of Council

From: Renée de St. Croix, Manager, Long Range and Community Planning

Subject: METRO VANCOUVER REGIONAL INDUSTRIAL LANDS STRATEGY

Date: November 25, 2020 File No: 13-6440-01-0001/2020

The following is a suggested recommendation only. Refer to Council Minutes for adopted resolution.

RECOMMENDATION

PURSUANT to the report of the Manager, Long Range and Community Planning, dated November 25, 2020, entitled "Metro Vancouver Regional Industrial Lands Strategy Endorsement":

THAT the City of North Vancouver endorse the *Metro Vancouver Regional Industrial Lands Strategy* and direct staff to consider and recommend opportunities to implement the *Strategy's* actions, as appropriate.

ATTACHMENTS

1. Metro Vancouver Regional Industrial Lands Strategy (CityDocs [#1996027](#))
2. Metro Vancouver Regional Industrial Lands Strategy – Executive Summary (CityDocs [#1996026](#))
3. Summary of Regional Industrial Lands Strategy Recommendations and their Application to the City of North Vancouver – Appendix A (CityDocs [#1995803](#))
4. Metro 2040 Industrial and Mixed Employment Policy Review Recommendations (CityDocs [#1996108](#))

PURPOSE

The purpose of this report is to provide an update and overview of the *Metro Vancouver Regional Industrial Lands Strategy*, outline its relationship to the City of North Vancouver, describe how it relates to the *Metro Vancouver Regional Growth Strategy* Update work on the *Industrial and Mixed Employment Lands Policy Review*, and recommend that Council endorse the *Regional Industrial Lands Strategy*.

BACKGROUND

The *Regional Industrial Lands Strategy (RILS)* was created to address challenges in the Metro Vancouver Region related to industrial lands.

On July 3, 2020 the Board of Directors of the Metro Vancouver Regional District approved the *Metro Vancouver Regional Industrial Lands Strategy* (see Attachment #1). At the time of approval, the Board adopted a resolution that the *Strategy* be forwarded to member jurisdictions and to request the endorsement and implementation of the actions attributed to each specific organization.

DISCUSSION

The following sections provide:

- An overview of the *Regional Industrial Lands Strategy (RILS)*;
- The relationship of the *RILS* to the City of North Vancouver; and,
- The relationship of the *RILS* to the *Metro Vancouver Regional Growth Strategy Update*

Overview of the Regional Industrial Lands Strategy

The intent of the *Regional Industrial Lands Strategy (RILS)* is to “ensure sufficient industrial lands to meet the needs of a growing and evolving regional economy to the year 2050.”

Defining Industrial Lands

The definition of ‘Industrial’ for the purposes of the *RILS* includes light and heavy industrial production, distribution, repair, construction materials and equipment, infrastructure, outdoor storage, and wholesale. In addition to the more traditional industrial uses, the *RILS* recognizes other uses that may be compatible with industrial uses (based on a criteria), which include other employment, non-industrial, or context specific uses.

Challenges for Industrial Lands

The main challenges that Metro Vancouver’s industrial lands face include:

- Constrained land supply
- Pressure on industrial lands
- Site and adjacency issues
- Complex jurisdictional environment

Vision

Ensure sufficient industrial lands to meet the needs of a growing and evolving regional economy to the year 2050.

Big Moves and Actions

The big moves are to:

1. Protect remaining industrial lands
2. Intensify and optimize industrial lands
3. Bring the existing land supply to market and address site issues
4. Ensure a coordinated approach

The *RILS* includes 34 actions under these big moves - ten of which have been identified as 'Priority Actions' for early implementation. The 'Priority Actions' are formed, in part, by packaging together related actions from the list of 34 actions.

Metro Vancouver staff intend to continue to work on the recommendations within the *RILS* and look forward to collaborating with the various organizations and partners. In addition, Metro Vancouver staff have been directed to continue to explore the feasibility of an industrial land reserve, and other potential policy measures and initiatives, and report back on the findings.

Relationship of the *RILS* to the City of North Vancouver

"Industrial lands are crucial to supporting a prosperous, sustainable regional economy, and to providing space to accommodate the industrial services needed in our growing region."

The City of North Vancouver has a land area of 11.85km². Of that, approximately 7% is designated Industrial (excludes water lots) and 4% is designated Mixed Employment.

Table 1. City of North Vancouver Land Area

	Total Area in City of North Vancouver	Total Area in CNV Owned by the Vancouver Fraser Port Authority
Designated Industrial Use	1.19km ²	0.81km ²
Land	0.84km ²	0.63km ²
Water Lots	0.35km ²	0.18km ²
Designated Mixed Employment Use	0.43km ²	0 km ²

The Metro Vancouver Region has 113.31km² of designated industrial lands. The North Shore is home to approximately 4% of those lands, with less than 1% located within the City of North Vancouver.

The Metro Vancouver Region has 33.7km² of designated mixed employment lands. The City of North Vancouver is home to approximately 1.3% of those lands.

The *Regional Industrial Lands Strategy (RILS)* is largely focused on the more suburban / larger scale industrial lands that are primarily south of the Fraser. However, there are a number of the actions / recommendations that are pertinent to the City:

For the full summary of thirty-four Actions and their application to the City, see Attachment 3.

Table 2. Summary of Regional Industrial Lands Strategy Actions and their Application to the City

Recommendations (Actions)	Application to CNV
1. Undertake a Regional Land Use Assessment	CNV would participate as required
2. Strengthen Regional Policy (RGS)	<p>Given CNV's dense, mixed-use environment, this could have ramifications in the future should there be a desire to transition from industrial and employment designations to other designations.</p> <p>Providing there is flexibility within the RGS policies related to proximity with Frequent Transit Networks and the ability to meet other RGS objectives, these changes should be manageable.</p>
3. Define 'Industrial' Use and Create Guidelines for Primary and Secondary Uses	As part of the Regional Context Statement update, CNV would commit to review and potentially update the Zoning Bylaw using the guidelines as a resource to amend the permitted uses in the local industrial and employment lands zones.
4. Define Trade-Oriented Lands and Develop an Overlay	<p>CNV would participate as required.</p> <p>CNV would seek to ensure that the overlay does not eliminate the opportunity to intensify adjacent lands, where appropriate. In addition, the overlay should not negatively impact the desired street character designations near or within the overlay, particularly as it relates to trucking routes.</p>
5. Consider Trade-Oriented Zoning	CNV would be judicious in applying any new zoning so as not to limit flexibility and future use that this zoning could place on these lands.
7. Facilitate Intensification / Densification	CNV would review the Zoning Bylaw and other policies to determine if any opportunities to make such adjustments exist and prepare policy options for consideration.
13. Manage Land Use Conflicts through Buffering Policies and Guidelines	<p>CNV would participate as required.</p> <p>CNV would seek to ensure flexibility in the guidelines' application so that lands adjacent to light industrial and employment lands are able to reasonably redevelop.</p>
17. Ensure Transportation Connectivity – Regional Truck Route Network	<p>CNV will continue to monitor and bring forward changes as required.</p> <ul style="list-style-type: none"> • This could impact CNV's most important commercial high streets (i.e. Lonsdale Avenue, East and West Esplanade). • CNV needs to form part of the conversation concerning the route planning for commercial and heavy vehicles. • CNV and regional bodies need to explore all options to develop appropriate solutions to protect our residents and create vibrant, liveable, and healthy urban environments, while still accommodating the required heavy vehicles.
24. Reduce Environmental Impacts	CNV continues to work on this through its policy efforts including the upcoming Mobility Strategy, Environment Strategy, Economic Investment Strategy, and more.

It is important to note that the City of North Vancouver continues to be a leader in maintaining and directing regional goods movement through our community. However, given that many of our goods movement corridors are routes for other transportation users, such as cyclists, pedestrians, and transit, staff will be seeking to establish policy guidance and actions to better manage the movement of goods and services on City streets. This will enable the continued support of a prosperous regional gateway and local economy while maximizing neighbourhood livability and safety for all road users. TransLink, Metro Vancouver, and key stakeholders will be engaged to provide key input into this process through 2021. The City will also continue to work with the Gateway Transportation Collaboration Forum to implement priority trade corridor infrastructure and collaborate with neighbouring municipalities through NXSTPP to ensure that trade and goods movement infrastructure is appropriately integrated within our local context.

The City recognizes the importance of sustaining and growing employment across all sectors, including the industrial sector, and that coupled with the delivery of diverse housing types and amenities, the City will be able to reduce commutes while enhancing economic prosperity.

In addition, the City is undertaking an Economic Investment Strategy, which will also review and incorporate key components of the *RILS* and *RGS* related to mixed employment and industrial lands to support our economy into the future.

Overall, the City of North Vancouver is supportive of the *Regional Industrial Land Strategy* and staff will participate in its ongoing implementation.

Relationship of the RILS to the Metro Vancouver Regional Growth Strategy Update

The *Industrial and Mixed Employment Policy Review*, which is a component of the *Metro Vancouver Regional Growth Strategy (RGS)* update, began in early 2020 with the intent to explore: what's working, what's not working, and what's missing. In 2021, Metro Vancouver Staff will be creating new and refined policy language for this component as a part of Phase 2 of the *RGS* update.

At the Metro Vancouver Regional District Board meeting of October 30, 2020, the Board received for information the *Metro 2040 Industrial and Mixed Employment Policy Review Recommendations* report (see Attachment #4) outlining the directions that will guide the new and refined policies for this component of the *RGS* update.

The directions outlined in the report were shaped using the approved *Regional Industrial Lands Strategy (RILS)*. The *RILS* contains specific recommendations for updating the Industrial and Mixed Employment policies in the *RGS*. The intent for this work is to use the *RILS* to inform and improve the policies in the *RGS*.

The key issues for the City to consider as the *RGS* update progresses include:

- Direction #2a – The proposed minor amendment to the voting threshold could have ramifications in the future should there be a desire to transition from

industrial and employment designations to other designations. The City would seek to ensure flexibility for lands in proximity to Frequent Transit Networks or for lands that could achieve other *RGS* objectives.

- Direction #2b – The City would seek to ensure that the regional overlay for trade-oriented land does not eliminate the opportunity to intensify adjacent lands, where appropriate. In addition, the overlay should allow flexibility in the role and function of local streets and/or corridor types near or within the overlay to accommodate diverse needs.
- Direction #3 – Consideration should be given to allowing non-industrial uses (i.e. commercial and residential) in association with employment based uses (not industrial). This would have an impact on the Capilano Mall area and the Lower Lonsdale Area (E Esplanade), particularly as it relates to future transit investments in those areas.

Please note that these issues are similar to the comments noted in Table 2 above because this work is the implementation of those *RILS* actions that recommended changes to the *RGS*.

Staff will continue to monitor and participate in the *RGS* update and review the new and refined policies as these items come forward from Metro Vancouver. Staff will also continue to provide updates to Council at key milestones.

SUSTAINABILITY COMMENTS

A key component of both the *Regional Industrial Land Strategy* and of the *Metro Vancouver Regional Growth Strategy* update, as noted above, is to improve on climate action and other environmental considerations:

- *RILS* – Recommendation #24 states: Regional organizations and stakeholders continue to investigate and implement options designed to reduce the environmental impacts related to the transportation of goods and people in the region, through their respective plans and policies.
- *RGS* Update – Direction #6: Improve Climate Action.

In addition, the climate impacts relating to industrial and mixed employment lands will be considered in the City's upcoming Environment Strategy, Mobility Strategy, and Economic Investment Strategy.

FINANCIAL IMPLICATIONS

There are no direct financial implications to the City, however there would be a moderate amount of staff time required to participate in the *RILS* implementation and *RGS* update, which is part of existing work plans.

INTER-DEPARTMENTAL IMPLICATIONS

This report has been prepared with input from the Community and Partner Engagement department. Planning staff will continue to work with CPE staff in undertaking future actions related to mixed employment and industrial lands.

STRATEGIC PLAN, OCP OR POLICY IMPLICATIONS

This work supports the following *Official Community Plan* objectives:

- 7.2.6 Maintain the City's industrial land base and enhance the contribution of the Port to the local economy by supporting improved rail access, goods movement and attainable housing for workers in addition to identifying potential opportunities for local businesses to supply more goods and services to the Port operators;
- 7.2.7 Maintain the City's mixed employment areas which provide light industrial and service commercial uses in the City;
- 7.2.9 Provide a high level of public services and infrastructure for commercial and industrial lands; and,
- 7.2.12 Ensure that permitted uses on designated industrial lands are most appropriate for an industrial location (as opposed to being better suited to a commercial location), allow for intensification of industrial lands and prohibit residential as a principal use.

In addition, this work supports the *2018-2022 Council Strategic Plan* priority of "A Prosperous City supports a diverse economy by creating an environment where new and existing businesses can grow and thrive."

RESPECTFULLY SUBMITTED:



Renée de St. Croix
Manager, Long Range and Community Planning



Metro Vancouver Regional Industrial Lands Strategy

June 2020

ACKNOWLEDGEMENT OF APPRECIATION

Metro Vancouver would like to thank the following for participating in the development of the Regional Industrial Lands Strategy.

The Industrial Lands Strategy Task Force provided core input and guidance. The Task Force was chaired in turn by Mayor George Harvie, City of Delta and Mayor Richard Stewart, City of Coquitlam. Task Force representatives included:

- Elected officials from within Metro Vancouver
- BC Ministry of Jobs, Trade and Technology
- Port of Vancouver
- TransLink
- BC Chamber of Commerce
- Agricultural Land Commission
- Urban Development Institute
- Beedie Group
- Value Property Group

Hemson Consulting guided much of the engagement and prepared the draft Strategy. Metro Vancouver also acknowledges the individuals who participated in stakeholder workshops and the Subject Matter Expert Panel, as well as the Greater Vancouver Urban Freight Council, Greater Vancouver Gateway Council, and NAIOP Commercial Real Estate Development Association.

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EXECUTIVE SUMMARY

Industrial lands are crucial to supporting a prosperous and sustainable regional economy. Industrial lands accommodate over one-quarter of the region's total employment, and contribute to the region's economic well-being, along with important linkages to transportation, trade, and taxation matters. Across the region, Metro Vancouver's industrial lands serve as home to a wide range of employment activities that, in turn, play a crucial role in supporting the broader regional, provincial, and national economies. From providing services to other local businesses, offering a broad range of well paying jobs, to enabling trade between Canada and the rest of the world, industrial lands are a key component to accommodating Metro Vancouver's continued growth.

The Metro Vancouver region is facing a critical shortage of industrial land. Years of steady population and economic growth in the region, ongoing conversion and non-industrial uses of industrial lands, and the constrained geography of the region, have precipitated the challenges facing Metro Vancouver's industrial land supply. It is important to align industrial land use planning with the region's economic interests. The productivity of industrial land is inherently tied and linked to the larger system of buildings, transport infrastructure, and access to labour, which all contribute to the vitality and viability of the system.

Demand for industrial land continues to increase. Land values have increased significantly over the past few years and vacancy rates are at record lows. As a result of all these factors, the limited supply of vacant industrial land across the region is anticipated to face a worsening shortage over the coming years. With continued pressure for growth, limited options for expansion and increasing competition between other land uses, these issues now threaten not only new opportunities for growth, but also existing economic and employment activities.

The Challenge

Through canvassing industrial stakeholders and users around the region, the four main challenges facing Metro Vancouver's industrial lands are:

1. A Constrained Land Supply

Bounded by the ocean, mountain ranges and an international border, the region is geographically constrained with limited options to expand the land base. As the region's population and economy continue to grow, this results in increasing demand for, and competition amongst, various land uses including residential, commercial, recreational, agricultural and industrial. With a limited supply of vacant industrial land remaining, there are fewer opportunities to accommodate both new industrial businesses and those businesses that are seeking to expand their operations, particularly for uses that require larger parcel sizes with the necessary access to services and transportation infrastructure.

2. Pressures on Industrial Lands

As other land uses also compete for the limited amount of land available in the region, industrial lands are facing increased speculation and other market pressures to convert to non-industrial uses, such as commercial and residential. These other uses typically are higher value; often pricing-out industrial land uses when a range or mix of land uses are permitted in industrial areas.

In addition, there is an increasing trend toward permitting more accessory and non-industrial uses on lands intended for industrial activities. These employment uses, although important to the regional economy, may displace and create location challenges for more traditional industrial uses.

3. Site and Adjacency Issues

In some cases, the high cost of delivering servicing to undeveloped industrial areas has proven prohibitive. Similar issues related to environmental constraints, lack of transportation and public transit infrastructure and the encroachment of nearby sensitive uses have discouraged or prevented industrial development where it might otherwise be accommodated. Similarly, the encroachment of sensitive land uses often result in conflicts with industrial operations (i.e. truck traffic, noise, odours) which can prove disruptive to existing and prospective industrial uses.

4. A Complex Jurisdictional Environment

The issues facing Metro Vancouver's industrial land supply involve multiple stakeholders, including the public sector, crown corporations, private sector, industry associations, chambers of commerce and boards of trade. In some cases, these challenges are compounded by overlapping jurisdictions amongst government agencies, leading to a fractured regulatory landscape and competing mandates. Similarly, varying approaches to industrial land use (including permitted activities and intensities) may be in conflict with the aspirations of the region and neighbouring jurisdictions.

The Response

In response to the challenges facing industrial lands and interests of industrial stakeholders in the region, the Metro Vancouver Regional District (Metro Vancouver) struck an Industrial Lands Strategy Task Force (the Task Force) comprising appointed MVRD Board Directors and representatives from other stakeholder agencies and organizations.

The Task Force's mandate was to guide the development of a Regional Industrial Lands Strategy to address the challenges noted. This has been a collaborative process involving a range of stakeholders with an interest in the current and future role that industrial lands should play in the region. This includes understanding how current industry sectors in Metro Vancouver use land for different types of industrial activities, challenges facing the development and operation of industrial lands within the region, and how the changing nature of work and industry could impact the future demand for land, along with associated transportation implications. Despite uncertain impacts on the future, all indications are that demand for industrial land will continue to grow.

Vision

The Regional Industrial Lands Strategy seeks to:

Ensure sufficient industrial lands to meet the needs of a growing and evolving regional economy to the year 2050

The vision statement speaks to both the timeframe for the Strategy as well as acknowledging the challenges posed by the changing nature of work and potential impacts on the regional economy and industrial land use and demand.

The Regional Industrial Lands Strategy supports and advances the policy objective to protect and intensify industrial lands in the region as articulated through a series of plans, including *Metro Vancouver 2040: Shaping our Future (Metro 2040)*, the regional growth strategy, TransLink's Regional Goods Movement Strategy, the Port of Vancouver's Land Use Plan, and local Official Community Plans.

The 4 'Big Moves' and 10 Priority Actions

The challenges facing Metro Vancouver's industrial lands are complex and interconnected, and no single action or stakeholder will resolve them alone. Market forces and types of industrial activity vary significantly by sub-region, such that for example, forms of densification that may be possible in certain urban locations may not be appropriate in other locations. There are a wide range of national, regional and local serving industrial activities in the region, which have different site and location needs, and which, as a result require different policy responses. Accordingly, recommended actions need to balance regional objectives, while addressing local contexts – 'one size' does not fit all.

To respond to the challenges noted above, the Strategy identifies **34 recommendations** with **10 priority actions** organized around **4 Big Moves**:

- Protect Remaining Industrial Lands
- Intensify and Optimize Industrial Lands
- Bring the Existing Land Supply to Market & Address Site Issues
- Ensure a Coordinated Approach

The priority actions are formed, in part, by packaging together related actions in the longer list of recommendations into combined shorter-term actions, to be implemented over the first few years of endorsement of the Regional Industrial Lands Strategy.

In considering the 4 'Big Moves' and the recommendations that follow, the Strategy affirms that conversion or use of agricultural lands is not a solution to the shortage of industrial lands in the region. This principle was endorsed by the Industrial Lands Strategy Task Force and Metro Vancouver Board.

Big Move 1 - Protect Remaining Industrial Lands

Given the ongoing and projected demand for industrial land in the region, it is imperative to protect the region's remaining industrial lands and curb the threats that undermine their use for industrial activities. This means lands for trade-oriented purposes as well as for small, local serving businesses; businesses that are responding to the changing nature of industry as well as more traditional industrial activities. The priority actions for this Big Move are:

1. **Define Trade-Oriented Lands:** Trade-oriented lands are large sites associated with the transportation of goods to and through the region, such as by rail and the port, which serve a national function and are crucial to the region's economy, warranting additional attention and possible protection. A clear, consistent and collaboratively developed definition and understanding of the extent and location of these important lands will support their protection. Metro Vancouver will work with member jurisdictions to establish a definition for trade-oriented lands. (Subsequent actions identified in the 34 recommendations include consideration of a regional trade-oriented land use overlay in the regional growth strategy and municipal consideration of trade-oriented zoning.)

2. **Undertake a Regional Land Use Assessment:**

Given Metro Vancouver's constrained land base, there is significant pressure for competing, legitimate land uses across the region. Metro Vancouver will undertake a targeted or region-wide assessment of land use, looking at and beyond the existing policy framework to proactively identify the 'best' locations for different land uses based on a collaboratively developed set of criteria.

3. **Strengthen Regional Policy:** Through the update of the regional growth strategy, Metro Vancouver will explore implementation changes, such as a consistent definition for Industrial, higher voting thresholds to amend the regional Industrial land use designation, and exploration of 'no net loss' as part of the amendment criteria.

4. **Seek Greater Consistency in Local Government Zoning Definitions and Permitted Uses:**

Metro Vancouver will, in collaboration with member jurisdictions and other regional agencies, develop a consistent definition of 'industrial' and guidelines for permitted uses. Member jurisdictions, through regional context statement updates, will review and update their zoning bylaws using the established guidelines.



Big Move 2 - Intensify and Optimize Industrial Lands

Given the region's constrained land base, it is critical to encourage the most efficient use of the remaining land supply for all types of industrial users, to remove barriers to the intensification of industrial land, and investigate opportunities to optimize the location of certain land uses over time. The priority action for this Big Move is:

5. **Facilitate the Intensification / Densification of Industrial Forms Where Possible:** Municipalities will review and remove unnecessary restrictions to density or height limits where appropriate; plan space to accommodate new, smaller industrial uses when older, centrally located industrial areas densify; allow mixing of industrial with other employment uses so long as the industrial component is secured as a condition of redevelopment; and explore opportunities to encourage intensification / densification in target areas. Metro Vancouver will explore allowing mixed-use with residential on Mixed Employment lands immediately proximate to Rail Rapid Transit Stations (distance to be determined, but suggest 200 metres) as long as existing industrial space is maintained or expanded and other *Metro 2040* objectives are met (e.g. affordable, rental housing).

Big Move 3 - Bring the Existing Land Supply to Market & Address Site Issues

Certain parts of the vacant industrial land supply suffer from site-specific challenges, such as limited infrastructure support, environmental concerns, and under-sized parcels. Where vacant lands have not come to the market, local municipalities may benefit from the preparation of a bring-to-market strategy. Such a strategy would identify the issues that have prevented the development of the undeveloped or under-developed industrial land, while providing a roadmap to achieve the municipality's economic goals for its remaining industrial lands. The priority actions for this Big Move are:

6. Prepare Bring-to-Market Strategies for Vacant or Under-developed Industrial Lands:

Municipalities with vacant or under-developed industrial lands will prepare a bring-to-market strategy for their industrial land that addresses strengths, weaknesses, opportunities and challenges, to encourage reinvestment and more

intensive use, considers municipal assembly and consolidation of fragmented parcels, whether environmental remediation is required, and if local servicing and infrastructure improvements are required and can be accommodated through front-ending infrastructure investment agreements.

7. **Ensure Transportation Connectivity:** The efficient movement of goods and people is critical for many industrial businesses, both local serving and trade-oriented, in terms of where they choose to locate. The maintenance and growth of a resilient and reliable transportation network, including various modes of transit, is an integral component in planning for the effective management of the region's industrial lands. TransLink, Metro Vancouver, the Port, the Airport and municipalities will continue to work together to coordinate investment in the transportation network, implement the Regional Goods Movement Strategy, enhance the regional truck route network, support efficient container drayage, and provide transit for industrial workers.



Big Move 4 - Ensure a Coordinated Approach

Improved cooperation and data tracking among governments and other agencies and organizations is necessary to ensure effective land management and accurate monitoring into the future. Coordination also guides future alignment of policy responses to issues as they arise across Metro Vancouver and southwestern British Columbia. The priority actions for this Big Move are:

8. **Coordinate Strategies for Economic Growth and Investment:** Metro Vancouver and member jurisdictions have a shared interest in attracting new investment and supporting economic and employment activities across the region. Many municipalities have an economic development office or department that work to retain and support the expansion of local businesses. Metro Vancouver's new Regional Economic Prosperity Service provides the opportunity for a regional approach to economic development that will amplify and complement the work of member jurisdictions.
9. **Improve Data and Monitoring:** Timely access to quality data is critical for land management and the development of effective economic policy. Metro Vancouver will update the Industrial Lands Inventory to have a better understanding of the current land uses and supply, and complete a Regional Employment Survey.

10. Develop a Framework for Coordination:

Pressures on industrial lands are not limited to Metro Vancouver. Adjacent regional districts are part of the same goods movement network and commuter-shed and they are facing similar land pressures and challenges. The Province will work collaboratively with Metro Vancouver, TransLink, and municipalities on cross-boundary economic and land use planning matters and will develop a framework for economic and land use planning coordination between neighbouring regions in the broader southwestern BC economic region to support industrial land use and protection. For example, Metro Vancouver will seek to sign a memorandum of understanding with the Fraser Valley Regional District outlining shared priorities regarding the effective and efficient management, protection, and development of industrial lands within the Lower Mainland.

Implementation

Taken together, these 10 Priority Actions will ensure Metro Vancouver's industrial lands continue to provide an attractive and viable location for industrial businesses to locate, grow and prosper, while supporting the broader regional economy and community. Further details for each action will be developed during implementation, including key stakeholder engagement. Furthermore, it is recognized that there is a logical sequencing of actions to inform subsequent work.

The Regional Industrial Lands Strategy will be periodically reviewed for effectiveness. The status of policy progress and market conditions can inform adjustments to actions as needed. Additional actions will be identified for prioritization in subsequent periods through the implementation work.

SETTING THE STAGE

Metro Vancouver is facing a shortage of industrial land. With strong demand for industrial space, many industrial businesses are finding it increasingly difficult to find suitable locations, while the prevalence of conversions to non-industrial uses in some areas further reduces the remaining supply of industrial land.

Industrial lands are crucial to supporting a prosperous, sustainable regional economy and to providing space to accommodate the industrial businesses and services needed across Metro Vancouver. These lands are home to over one-quarter of the region's jobs, and are used mainly for transportation and warehousing, wholesale trade, retail trade, manufacturing, and professional and technical services.

Some industrial activities provide critical regional services, such as infrastructure maintenance and repair, while other activities contribute to a diversified regional economy. Many activities on industrial lands provide for the day-to-day needs of Metro Vancouver's residents and businesses, providing locations for services like regional utilities, vehicle repair, hotel laundry services, catering companies, couriers, breweries, goods manufacturing, and design space. These different businesses need different types of spaces to optimize their operations.

While some industries are and will continue to be land-intensive, other forms may be accommodated in high density / intensity buildings. Modern industry needs different types of space than traditional industry, and will both impact and respond to changing business, economic, employment, and transportation conditions.

Furthermore, the type of industrial activity varies greatly across the Metro Vancouver region, from urban industrial in Vancouver to logistics facilities in locations close to port terminals, and warehouses in Surrey and Langley. Each jurisdiction has unique competitive strengths and strategies for supporting international, regional and/or local demand.

Due to a constrained land base and strong demand for all types of land use, the regional supply of industrial land is under significant pressure for conversion to permit uses that can command higher market values, such as retail, commercial and residential uses. In some parts of the region, flexible zoning has allowed non-industrial businesses to occupy industrial lands, competing for and at times displacing the industrial uses that cannot easily locate elsewhere. In other areas, the encroachment of sensitive uses, like residential, next to existing industrial operations is resulting in conflicts that threaten the continued operation of the original industrial use. These challenges, along with many others, are eroding opportunities for industrial businesses to operate and grow, which in turn threatens the economic diversity and vitality of the broader regional economy.



Metro Vancouver's Industrial Lands Strategy Task Force

In response to the challenges facing the region's industrial lands, Metro Vancouver struck an Industrial Lands Strategy Task Force in March of 2018, which continued into early 2020.

The mandate of the Task Force is to guide the development of the Regional Industrial Lands Strategy. It was intended to be a collaborative process involving a range of stakeholders with an interest in the present and future role that industrial lands play in the region. This includes developing a greater understanding of how current industry sectors use land for different types of industrial activities, the challenges facing the development and operation of industrial lands, and considering how the changing nature of work and industry could impact the demand for land in the future in the region.

The Task Force includes both voting members, comprising appointed Metro Vancouver Board Directors, and non-voting members, comprising representatives from agencies and organizations with an interest in, and specialized knowledge about industrial lands. Non-voting members included representatives of the Port of Vancouver, TransLink, the Urban Development Institute, the BC Ministry of Jobs, Trade and Technology, BC Chamber of Commerce, Agricultural Land Commission, and representatives from the industrial development community. The Task Force was responsible for guiding the development of research and stakeholder consultation to support the development of the Regional Industrial Lands Strategy, before reporting back to the Metro Vancouver Board with the recommended Strategy.

Vision

One of the first efforts of the Task Force was to confirm a vision for the Regional Industrial Lands Strategy.

Ensure sufficient industrial lands to meet the needs of a growing and evolving regional economy to the year 2050

The vision statement speaks to both the timeframe for the Strategy as well as acknowledging the challenges posed by the changing nature of work and potential impacts on the regional economy and land demand.

Definition of 'Industrial'

If the objective of the Regional Industrial Lands Strategy is to ensure the sufficient supply of industrial lands, the first step is to develop a greater understanding of what the Strategy is seeking to protect lands for – i.e. how is 'industrial' defined.

Across the region, municipal policies and bylaws vary in terms of what uses are permitted within industrial and employment zoning. For example, some municipal bylaws limit permitted uses to more traditional industrial activities, such as manufacturing, warehousing and distribution logistics, while others provide more flexibility to accommodate a range of uses, such as commercial, places of worship, and indoor recreation. This flexibility of use can be beneficial towards realizing opportunities associated with the changing nature of industry and the emerging innovation economy, but it can also result in the displacement of more traditional industrial activities and increasing space and land supply challenges for industrial activities.

This is of particular concern in the context of the region's constrained land supply as it relates to the infiltration of industrial areas by non-industrial uses that could potentially locate elsewhere. The challenge is striking the right balance of narrowing the permitted uses to protect the needs of region-serving and trade-oriented activities, while remaining sufficiently flexible to allow for innovation in appropriate locations and ensure sufficient space for non-industrial and employment uses (e.g. in Urban Centres, which generally have better transit infrastructure and more amenities).

'Industrial' represents a wide spectrum of uses and intensities, ranging from large distribution and transportation lands, warehouses, manufacturing and processing facilities, to small local-serving production and suppliers, and new technology-driven businesses with integrated work spaces, which all need different types of accommodations to optimize their operations. Many regional jobs and businesses are also connected to national trade and the port, which require industrial lands for handling goods – e.g. for container storage, freight forwarding, warehouses, and other distribution functions. Towards this end, the Task Force, through consultation with regional stakeholders, developed a working definition for 'industrial' uses for the purpose of the Regional Industrial Lands Strategy.

Industrial is defined as:

- Light and heavy industrial production (e.g. cement manufacturing, food and beverage manufacturing, furniture manufacturing, metalwork and fabrication, sawmills)
- Distribution (e.g. warehousing, industrial storage, freight trucking, intermodal couriers)
- Repair (e.g. autobody shops, truck and trailer repair, consumer goods repair)
- Construction materials and equipment (e.g. building supplies and specialty trade contractors, heavy equipment rental and leasing)
- Infrastructure (e.g. public utilities such as wastewater treatment facilities and pumping stations, works yards, rail / port terminals)
- Outdoor storage activities (e.g. container storage)
- Wholesale (e.g. merchant and logistics wholesalers)

In addition to the more traditional industrial uses, the Strategy also recognizes other non-traditional industrial uses that may be compatible with industrial uses based on a number of criteria, best dealt with at the local and site levels (Tables 1 and 2).



TABLE 1: CRITERIA TO ASSIST IN DEFINING INDUSTRIAL USE & COMPATIBILITY

CRITERIA	DESCRIPTION
Compatibility with adjacent uses	Whether a business is an emitter of noise, vibration, odour or visual nuisances that make occupying areas proximate to other land uses (e.g. residential, commercial institutional) challenging
Access (to site and goods movement network)	Whether the business requires access to particular infrastructure or modes of transportation such as water, rail, airport, and highway
Space requirements and scale of business	Whether the business requires large / unique site or building characteristics
Product produced	Whether the business manufactures, distributes or repairs a physical product
Trip generation	Whether the business generates or handles relatively large amounts of truck or client traffic
Client type	Whether the business is geared toward other business or the public
Ancillary/hybrid use	Percentage of the business that is office if a hybrid use (e.g. bio-tech)

SOURCE: DEFINING INDUSTRIAL FOR THE REGIONAL INDUSTRIAL LANDS STRATEGY, METRO VANCOUVER, SEPTEMBER 2018

Through consultation with stakeholders, these criteria were used to categorize a spectrum of uses into four land use categories that align with *Metro 2040's* land use designations. These include traditional industrial activities that are appropriate for lands designated or zoned Industrial; flexible uses considered appropriate for lands designated or zoned Employment;

non-industrial uses that should not be permitted on Industrial lands; and context-specific uses that could be appropriate for Industrial or Employment lands depending on the criteria listed in Table 1. The spectrum of uses by applicable land use category are summarized in Table 2.

TABLE 2: APPLICABLE USES BY LAND USE CATEGORY

INDUSTRIAL	<ul style="list-style-type: none"> • Light and heavy industrial production • Distribution • Repair • Construction materials and equipment • Infrastructure • Outdoor storage • Wholesale
EMPLOYMENT	<ul style="list-style-type: none"> • Indoor Storage (e.g. self-storage) • Stand-alone office (e.g. law and financial offices) • Stand-alone recreation (e.g. karate studios, theatres, crossfit and climbing gyms, badminton/tennis facilities) • Retail / wholesale (e.g. big box, car dealerships, furniture, automotive parts) • High tech software development • Consulting services (e.g. engineering, architecture, environmental)
NON-INDUSTRIAL	<ul style="list-style-type: none"> • Residential and supportive housing • Artist studios and live / work space • Agriculture (soil based growing) • Places of worship • Institutional uses (e.g. schools, day cares, hospitals)
CONTEXT SPECIFIC	<ul style="list-style-type: none"> • Restaurants (i.e. serving adjacent industrial activities versus being a draw for the public) • Retail (i.e. micro-breweries depends on scale and percent retail) • Services (i.e. commercial laundry, bakery, catering depends on scale) • Research and development (i.e. high-tech, bio-tech that require 'industrial' space) • Media production studios (i.e. movie, recording, television and radio)

SOURCE: DEFINING INDUSTRIAL FOR THE REGIONAL INDUSTRIAL LANDS STRATEGY, METRO VANCOUVER, SEPTEMBER 2018

The Changing Nature of Industrial Activities

The changing nature of the economy means that business and industrial activities are evolving, as are desired space needs and location preferences, and associated employment and transportation implications. This may include new types of manufacturing, advanced technology, and integrated work spaces. In some cases, these uses may be less impactful in terms of external nuisances, and different levels of employment density.

The new types of business models may not neatly fit within the traditional paradigm of 'heavy' or 'light' industrial. The types and definitions of industrial activities are evolving and becoming more diverse. Notable new forms of industrial activity include:

- Clustering together or co-locating of related operations can support eco-industrial networks and circular economy systems, where companies collaborate to utilize each other's by-products (material loops) and share resources (peer-to-peer lending) to increase overall efficiencies. These sectors can include: innovative R&D, green / clean-tech, and smart technologies.
- Local artistry 'maker movement' craftsmanship.
- High tech and other newer forms of industry such as: e-commerce, direct-to-consumer deliveries, co-facilities, advanced technology, on-demand manufacturing, and creative / media / design.
- Significant industrial land intensification as a result of market forces, specifically new industrial uses, growth in logistics, smart warehouses, and e-commerce. The reasons are that new industrial capital investments often leverage existing and emerging technologies to either scale their output or increase value-added production.

- Logistics, distribution, global supply chains, and new forms of industrial or manufacturing activity are driving demand for industrial property.
- New forms of intense industrial development such as mega-distribution facilities and e-commerce logistics facilities have become dominant features of the major industrial real estate markets in the world. The availability of a sufficient number of large-sized land parcels to support this activity, along with labour availability, influences site selection for these activities worldwide. In certain cases, the shortage of large parcels of industrially zoned land may be an obstacle to attracting such developments of a certain size.
- Evolving business models in response to e-commerce and technology are placing increasing importance on the need for integrated space where design, manufacturing, distribution, and showroom / retail activities can occur within a single building.

Findings - Role of Industrial Lands

In setting the stage for the Regional Industrial Lands Strategy, a significant body of research was undertaken. The following findings provide an overarching summary of that work, and set the stage for the Strategy's recommendations:

1. Industrial lands play a critical role in supporting both local employment and the broader regional and national economies.
2. A broad spectrum of employment activities take place on industrial lands, many of which are unlikely or unable to locate elsewhere within the region.

3. Industrial land uses have specific spatial and infrastructural needs that must be planned and protected to ensure their efficient operation and continued growth.
4. Many industrial businesses rely on dependable access to the goods movement network to ensure the timely movement of product to and from suppliers and their clients, along with transit services to link businesses with a broader range of the region's workforce.
5. While industrial areas can benefit from a mix of complementary and secondary uses, many industrial uses struggle to compete when non-industrial uses are allowed to encroach upon and develop within industrial lands:
 - there is limited alignment on what constitutes an 'industrial' use and some local zoning have permitted distinctly non-industrial uses to locate and occupy a significant amount of industrial land.
 - the encroachment of sensitive uses, such as residential, often creates conflicts with abutting industrial activities. Without sufficient buffering to separate these uses, or mitigation measures to limit conflict, encroachment may threaten the operation and growth outlook for existing industrial operations.
6. The market for industrial land and how it is used varies across the region, with differing land values, rents, and spatial characteristics in different sub-markets.
7. There are a number of ways in which industrial uses can make more efficient use of a limited land supply, including intensification of built form where local development restrictions, geotechnical / soil conditions, and market factors permit. However, not all market areas or industrial operations are well suited to multi-level industrial buildings.
8. Innovation and the changing nature of work in industrial areas requires a careful balance between protecting for certain uses and enabling flexibility to realize emerging economic opportunities:
 - the limited supply of available industrial land, coupled with speculation and competition amongst other land uses, is resulting in rising costs and fewer options for new and existing industrial businesses.
 - without careful management of the region's remaining industrial land supply, there is a real risk that industrial activities may be displaced or may choose to (re)locate elsewhere outside of the region.
 - mismanagement of the region's industrial land supply has the potential to impact not only industrial employment and economic activity, but also the broader economic outlook for Metro Vancouver.
9. Given the diverse characteristics of Metro Vancouver's industrial sub-markets and geographies, policy options that work well in one jurisdiction or area may not apply to another. As such, policy recommendations in the Regional Industrial Lands Strategy must not be implemented in a blanket manner but instead must recognize local context and applicability.

THE ROLE OF INDUSTRIAL LANDS BY SECTOR IN THE METRO VANCOUVER ECONOMY

The Metro Vancouver regional industrial market stretches from the North Shore and Vancouver area to the Fraser Valley. The Metro Vancouver region is experiencing an acute shortage of industrial land supply, which manifests as very low vacancy rates, increasing land prices, and higher lease rates. These factors impact the industrial real estate market, spurring more development of industrial projects (within the limitations of land supply) and more intense / dense forms of industrial development.

Demand for industrial space continues to set new records as both owner-operator and investor interest in industrial assets strengthen amid low vacancy, constrained land supply, and rising rental rates throughout Metro Vancouver.

According to market reports, demand is driven by distribution of consumer goods (warehouses), food / beverage (processing), building supplies, technology users, and film production. Logistics associated with last-mile delivery and e-commerce are also in strong demand, requiring well-located urban premises. In core locations, increasing prices force a transition to more specialized light manufacturing, tech, office and showroom-type uses.

Also identified as new users of industrial space are small scale, artisanal businesses, such as custom manufacturing, small batch production, and food processing, some relating to the sharing economy, such as facilities with common kitchens. Many of these businesses require small space, and are local serving, benefiting from close proximity to the consumer population.

In recent years the rapidly growing film industry has been turning to warehouse sites to convert the structures into large production studios. As such, modern warehouse space is now becoming more attractive to both the logistics and film industry.

The anticipated growth trajectory of the Port of Vancouver's containerized cargo volumes will remain a structural driver of demand for industrial real estate in the region. The Port and associated operations require trade-oriented lands, which are large and close to transportation infrastructure.

Growth is also occurring in tech firms in urban light industrial areas, noting the diverse types: high tech, bio-tech, software, and digital. These businesses often need high ceiling, studios, labs, storage, and power, with various activities including, R&D, packaging, and distribution on site -- industrial space that meets their needs. Furthermore, different types of tech companies have different abilities to pay, in terms of both wages and accommodations; most small and young companies cannot afford high lease rents or to purchase strata space.

Economic Contribution of Industrial Lands

Industrial lands are the foundation for a significant component of the Metro Vancouver region's economic activity, representing a large amount of employment and economic activity:

- Comprising only 4% of the region's land base, industrial lands are home to nearly 27% (364,000) of the region's 1.3 million jobs, while also supporting an additional 163,000 jobs through indirect and induced impacts.
- Jobs located in industrial areas tend to be high paying, offering an average wage of \$61,100 per worker, which is roughly 10% higher than the regional average wage of \$55,000 per worker.
- In 2016, business activities on industrial lands generated \$27 billion in direct Gross Domestic Product (GDP), equating to 30% of the region's total GDP. These activities also accounted for an additional \$30 billion in indirect and induced GDP, of which \$16 billion is within the region, \$5 billion is accrued elsewhere in the province, and \$9 billion accrues elsewhere in Canada.



The Metro Vancouver industrial market continues to experience very strong demand for space and a limited supply of land. As described by one brokerage firm, "with record-low vacancy, escalating rental rates, an expensive and constrained industrial land supply and an insufficient volume of new development" there is a "ravenous appetite for industrial real estate among tenants, owner-occupiers, developers as well as private and institutional investors".

The demand is based on a growing regional population and economy, Metro Vancouver's geographic role as a transportation gateway for the nation, and evolving and emerging forms of industrial. Notable drivers of demand are: e-commerce, logistics and distribution, light manufacturing, food processing, advanced technologies, and creative and media production sectors, many of which desire urban locations to be close to their customers and workforce.

On the supply side, the quantity of available land is essentially fixed, and the amount of new industrial building floor area being built cannot keep up with absorption:

- Metro Vancouver had, as of 2015, approximately 11,300 hectares (28,000 acres) of industrial land, about 80% of which is already developed. Within this supply, local brokerages estimate there to be 210 million square feet of industrial building floor space.

- Because the development of new space has not kept pace with demand, the industrial vacancy rate of 1.4% is a record low for the region, and counts amongst the lowest in North America's major markets.
- Competition for space is resulting in increasing rental rates across the region, ranging from \$10.50 per square foot in Surrey and Delta to \$14.50 per square foot in Vancouver and North Vancouver.
- Values for vacant industrial land are also up significantly in recent years, past the \$2 million per acre mark in many areas, and much higher in the City of Vancouver.
- Purchase prices for strata industrial space have followed similar trends ranging from \$300 to \$500 per square foot depending on the sub-market, with Vancouver representing the higher end.
- Despite the development of new supply being at an all time high, with some 6 million square feet of building space under construction, absorption levels have yet to abate, indicating demand is outpacing the market's ability to provide space.

Within this market, industrial tenants are facing limited space options to accommodate their new and expanding businesses, while also experiencing increased pressure from rising market rents, maintenance costs and property taxes. Businesses are faced with either renewing their lease at notably higher rates, or relocating further away from the region's core markets, driven – or indeed, pushed – by availability and costs rather than location preferences.

Employment and Economic Activity

Of the 364,100 jobs located on the region's industrial lands, 200,400 (55%) are associated with industrial activities, involving production, distribution, repair, public infrastructure, and trade-oriented uses (employment activity located on lands associated with the Port of Vancouver and Vancouver Airport Authority). Industrial activities are significant contributors to the local economy, contributing proportionally more GDP per job than the regional average. These jobs generated an estimated \$17.3 billion in direct GDP in 2016.

Non-industrial activities, that is those jobs in sectors related to media, film and art production, research and development in professional and technical services, retail and other services, also play a significant role on the region's industrial lands. These sectors account for 163,700 direct jobs and approximately \$9.5 billion in direct GDP (Table 3), though their impact is proportionally lower on a per jobs basis than the industrial sectors. This is primarily on account of the higher economic multipliers (linkages and spending with other sectors) and higher average wages associated with activity in the industrial sectors.

TABLE 3: ECONOMIC IMPACTS OF EMPLOYMENT LOCATED ON INDUSTRIAL LANDS,
BY SECTOR, 2016

ECONOMIC IMPACT		IMPACTS:	REGION	BRITISH COLUMBIA	CANADA
EMPLOYMENT (JOBS)	Industrial Sectors	Direct	200,400	200,400	200,400
		Indirect	59,200	78,100	111,400
		Induced	48,000	62,900	87,900
		Total	307,600	341,400	399,600
	Non-Industrial Sectors	Direct	163,700	163,700	163,700
		Indirect	28,900	36,000	50,400
		Induced	28,900	33,600	45,800
		Total	219,400	233,200	259,900
	All Sectors	Total	527,100	574,600	659,500
GDP (\$ BILLIONS)	Industrial Sectors	Direct	\$ 17.3	\$ 17.3	\$ 17.3
		Indirect	\$ 5.5	\$ 7.3	\$11.2
		Induced	\$ 5.2	\$ 6.8	\$ 9.5
		Total	\$ 28.0	\$ 31.4	\$ 38.0
	Non-Industrial Sectors	Direct	\$ 9.5	\$ 9.5	\$ 9.5
		Indirect	\$ 2.5	\$ 3.2	\$ 4.6
		Induced	\$ 2.9	\$ 3.6	\$ 4.9
		Total	\$ 15.0	\$ 16.3	\$ 19.1
	All Sectors	Total	\$ 43.0	\$ 47.7	\$ 57.1

SOURCE: STATISTICS CANADA CENSUS 2016, METRO VANCOUVER AND INTERVISTAS ANALYSIS AND CALCULATIONS.

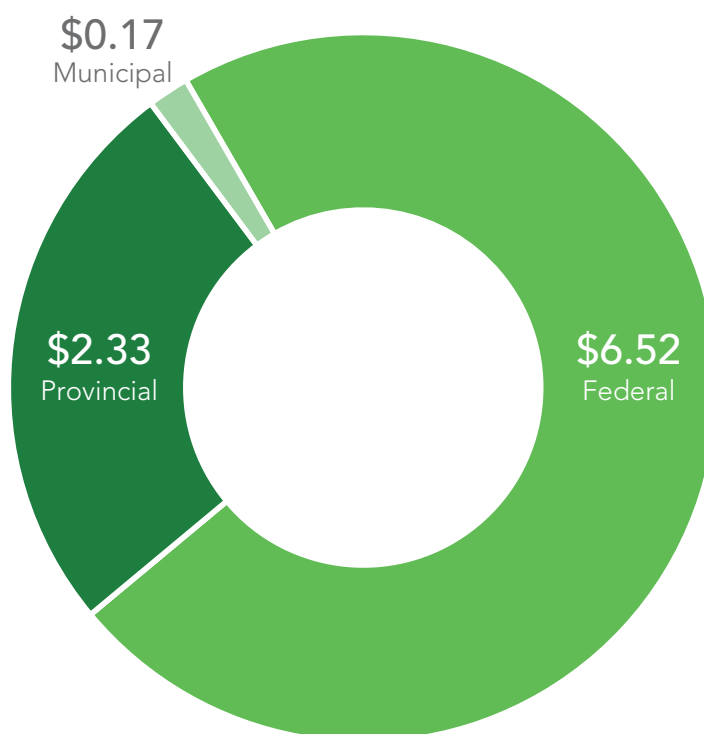
NOTE: DATA ARE ROUNDED AND MAY NOT SUM. PRICES ARE 2016 DOLLARS.

Tax Impacts

Metro Vancouver's industrial lands are estimated to generate \$6.5 billion in tax revenues and fees for the Federal government and \$2.3 billion for the Provincial government (Figure 1). Industrial sector jobs make a

greater contribution to Federal and Provincial revenues when compared to non-industrial sector jobs located in the industrial areas, primarily on account of their higher average wages for industrial related jobs.

FIGURE 1: ESTIMATED FEDERAL, PROVINCIAL AND MUNICIPAL TAX REVENUES GENERATED BY INDUSTRIAL LANDS (\$BILLIONS)



SOURCE: INTERVISTAS' TAX MODEL AND CALCULATIONS, AND ANALYSIS OF BRITISH COLUMBIA GOVERNMENT TAX BURDEN SCHEDULE 707 2016 ASSESSMENTS, TAX RATES, MUNICIPAL TAXES AND CLASS PROPORTIONS OF TAXES AND ASSESSMENTS.

METRO VANCOUVER INDUSTRIAL LANDS - ISSUES AND TRENDS

Industrial lands are crucial to supporting a prosperous and sustainable regional economy and to providing space to accommodate the industrial businesses and services needed across Metro Vancouver. Industrial lands serve both an important regional role in the economy and employment, and, as a facilitator of trade-oriented activities, a critical national role to the wider economy. Industrial lands, while only comprising 4% of the land base, accommodate over one-quarter of the region's total employment, and contribute to the region's economic well-being, along with the associated linkages to transportation, trade, and taxation matters, supporting the broader regional, provincial, and national economies.

'Industrial' represents a wide spectrum of uses and intensities, ranging from large distribution and transportation lands, warehouses, manufacturing and processing facilities, to small local-serving production and suppliers, and new technology-driven businesses with integrated work spaces. These businesses all have different needs in terms of accommodations to optimize their operations, as well as location, property and building size requirements.

Industrial lands accommodate many businesses that serve the wider economy, and provide employment opportunities close to home for residents, as well as taxation benefits to the respective local municipality. Industrial lands provide for the day-to-day needs of the population, locations for services like vehicle repair, hotel laundry, catering companies, breweries and couriers, etc., that serve and provide employment opportunities for both the region and the surrounding communities. Many regional businesses and jobs are also connected to national trade through the Port of Vancouver, which requires industrial lands for the handling of goods including: container storage, rail shunting, freight forwarding, warehouses, and other distribution functions.

The Metro Vancouver market conditions are a function of a very tight supply of industrial lands in the region, and strong demand from multiple business sectors. Strong economic fundamentals are driving demand for industrial space in the region, while a shortage of developable space (especially larger size land parcels) has kept available supply constrained. The price to lease industrial space in Metro Vancouver has increased greatly. In response, some new industrial developments are built to more intense / dense levels, while some industrial tenants are rationing space or being displaced, and in other cases are leaving the region.

This section assesses the current state of industrial lands in Metro Vancouver and provides information on trends, issues, context, opportunities and challenges for the Strategy's recommendations. Research was compiled through a comprehensive review of various market and academic sources, including a series of studies and white papers prepared for the Task Force. These studies include:

- Regional Industrial Lands Strategy – Issues and Initiatives Summary (June 2018);
- Industrial Lands and the Innovation Economy (June 2018);
- Defining Industrial for the Regional Industrial Lands Strategy (September 2018);
- Stratification of Industrial Land in Metro Vancouver (September 2018);
- Industrial Edges: Compatibility and Interface Issues in Metro Vancouver (September 2018);
- Agri-Industrial Activities in Metro Vancouver (February 2019);

- Regional Industrial Lands Strategy – Survey of Industrial Users (February 2019);
- Economic Value of Industrial Lands to the Metro Vancouver Region (February 2019); and
- The Changing Nature of Industry and Industrial Land Demand in Metro Vancouver (July 2019).

Regional plans should guide market forces in ways that respond to industrial needs while considering other community objectives. The complexity lies in balancing industrial trends, development viability, business needs, responsive regulations, and other community interests. The challenge and opportunity is how to best use and intensify industrial uses without losing the industrial function of the lands.

Challenges Facing Metro Vancouver Industrial Lands

With regional population and employment growth, and a constrained land base, the region faces a number of challenges related to the protection, management and promotion of industrial and employment lands. These challenges have been categorized into four themes:

- A. Constrained Land Supply
- B. Pressure on Industrial Lands
- C. Site and Adjacency Issues
- D. Complex Jurisdictional Environment

The challenges are complex and interconnected, and addressing them will require creative responses and collaboration among all stakeholders in the region.



A. Constrained Land Supply

The challenges facing Metro Vancouver and its industrial land supply are somewhat unique, at least in the North American context. Unlike most other North American markets, where continued demand can often be accommodated via development and growth into outer areas, Metro Vancouver is bounded both physically and politically between mountains, an ocean and an international border. While there is some potential for industrial development in the neighbouring Fraser Valley Regional District to the east, it too has a limited supply of vacant industrial land available, leaving the land supply within the Lower Mainland predominantly capped.

Between the limited supply of vacant industrial lands and high demand for new space, there is a scarcity of lands in general. This is of particular concern as it relates to larger parcel sizes (20+ acres / 8+ hectares), that are typically required for major industrial and logistical uses. This problem is further exacerbated by the fragmented nature of much of the region's remaining vacant land parcels, which results in lands that are suitable for some industrial activities, but insufficient for large-scale development, in terms of size, location, or other attributes.

With limited options through traditional expansion-based methods, Metro Vancouver will need to embrace creative options for increasing both the supply and capacity of its industrial lands. These alternative approaches are not without their own challenges, as discussed in the following examples.

In the past, it was common to expand industrial and port areas into shorelines by using fill. However, the environmental and economic costs and necessary approvals and mitigation measures can be prohibitive, resulting in the practice being used much more sparingly in recent years. The Port of Vancouver is

currently undertaking a comprehensive study to expand the Roberts Bank container terminal using this approach; though beyond that project there are few prospects for this approach to be applied elsewhere in the region.

B. Pressure on Industrial Lands

Not only are the region's industrial lands in short supply, they are also under threat of being further diminished due to a number of factors, including allowance of non-industrial uses and being out-priced due to high land values and property taxation. At the same time, industrial uses seeking other locations to suit their needs are putting pressure on the region's agricultural land.

Competition and encroachment from non-employment uses are putting considerable pressure on industrial lands. The encroachment of these sensitive land uses often result in conflicts with industrial operations (i.e. truck traffic, noise, odours) which can prove disruptive to existing and prospective industrial uses. Mixing non-industrial uses in industrial areas or converting the lands entirely runs the risk of encouraging speculation, which in turn may drive up land values and prevent industrial growth or displace existing industrial users.

Industrial areas can benefit from the presence of a number of complementary non-industrial uses, including restaurants and amenities to serve local workers, and ancillary retail and office components that are related to the primary industrial use. However, these and many other non-industrial uses can also compete for space in the industrial area. Many non-industrial uses are able to locate in other parts of the urban land supply, but given the comparatively lower cost of land and space, may prefer to locate in industrial areas if permitted to do so.

C. Site and Adjacency Issues

In addition to the macro-level challenges facing industrial lands, various site-specific issues are creating barriers to the effective development and redevelopment of the region's industrial lands:

- In the case of unserviced industrial lands, the high cost of delivering necessary servicing and infrastructure may be preventing the establishment of new industrial activity;
- A number of industrial areas have limited access to the Regional Truck Route Network and other infrastructure intended for the movement of goods and materials, including access to railways and navigable waterways;
- Recognizing that some industrial lands are located in areas that are challenging to serve efficiently with transit, limitations to transportation and transit infrastructure, coupled with a lack of local amenities, can make it difficult for some industrial businesses to attract and retain workers; and
- Regulations and policies related to industrial impacts on sensitive environmental features are limiting the development capacity of certain sites, requiring setbacks / buffers that can reduce building footprints to unusable sizes. Similar requirements for the remediation of previously occupied industrial sites have proven cost-prohibitive for some industrial developments.

D. Complex Jurisdictional Environment

Industrial lands and industrial activities play a crucial role in the region and its economy. While land use is primarily managed at the municipal level, decisions related to the effective management of industrial lands have the potential to affect neighbouring municipalities, the broader region, and even aspects of the economy at the provincial and national level.

At the same time, the issues facing industrial lands involve multiple stakeholders and overlapping jurisdictions of government, which may have their own organizational mandates. The result is a fractured regulatory landscape which can occasionally result in redundancies or competing policy objectives that stifle the effective utilization of the region's remaining industrial lands for various purposes. For example, the Port of Vancouver, with its federal mandate to accommodate and grow trade-related economic activity, is not subject to provincial or regional policy. As the Port has acquired lands to support this objective, critics have raised concern that the Port is overriding local, regional and provincial land use policies and objectives, including the protection of agricultural land or other local priorities.

In other regards, competition between neighbouring municipalities to attract business and grow their respective property tax bases may result in decisions that undermine the resiliency of the broader regional economy. This is best exemplified by the continued conversion of industrial areas to higher value land uses, despite the limited amount of industrial land remaining in the region.

Lack of collaboration and coordination between government agencies is also resulting in insufficient data sharing, and policy alignment between organizations. These gaps have resulted in policy misalignment (such as overly permissive zoning in industrial areas) and blind spots (such as infrequent reporting on industrial land take up) which make it difficult to track and respond to issues facing industrial lands in a timely manner.

Taking Stock: Finding Opportunities

Trade-Oriented Uses

Industrial businesses cover a range of activities, locational preferences and site needs. For example, a manufacturer of specialized computer components may require a far different type of space than that of a distribution and logistics warehouse operation. Much like the competition between industrial and non-industrial space users, industrial uses with different operational needs may also be able to support different values for land and space.

This disparity is of particular note when it comes to trade-oriented uses, such as the logistics, warehousing and distribution of goods. These uses tend to require specific locations with good access to port, rail or highway infrastructure, often in the form of large warehouses with ample space for the loading and unloading of trucks. However, smaller specialized industrial users can also compete for these types of sites, and may be a more financially viable form of development on high value lands, particularly if developing multi-storey and / or stratified sites. This can prove a challenge given the limited number of large sites in the region that have the characteristics needed by trade-oriented businesses.

In some cases, there can be a real or perceived tension between stratification (including small lot subdivision) and trade-oriented lands (requiring large, flat sites near the goods movement network). Nevertheless, creative solutions are possible, and a variety of uses and tenures are not mutually exclusive for large sites.

Stratification of Industrial

Another way in which Metro Vancouver is unique in the North American context is the popularity of owner-operated strata industrial space. Whereas most commercial and industrial users in North American markets tend to operate in leased space, the limited land supply and high land prices in the Metro Vancouver market, coupled with sustained periods of low interest rates, have resulted in significant demand for owner-occupied strata units.

The benefit of strata-ownership is that it allows smaller industrial users to have security of tenure over their space, providing stability while also enabling the owner-occupier to experience capital appreciation. The upfront nature (i.e. pre-sales) and high sales prices for strata property sales also enable developers to de-risk the development of more capital-intensive built forms, including multi-storey industrial projects, and can prove beneficial for users seeking space in denser inner-city locations.

However, stratified space is not conducive to all industrial users. The high cost may be prohibitive to smaller businesses and may limit the flexibility of firms looking to expand. The subdivision of individual units within buildings may also prove problematic for larger firms seeking large, cohesive spaces. This also poses a potential long-term issue for the redevelopment of existing space, requiring the consolidation of fragmented ownership within buildings. Speculation for strata may also price-out larger traditional and trade-oriented industrial land users from being able to acquire properties.

Increasing Industrial Capacity Through Intensification / Densification

With limited options to increase the land base, many industrial projects are now considering building upwards; modern warehouse distribution centres are developed to be significantly more volumetrically intensive than traditional warehousing operations. Multi-storey industrial buildings are rare in North America due to the high capital costs associated with constructing space that meets the needs of modern industrial businesses, which in turn requires a much higher rent in order to ensure a sufficient return on investment for developers. This also includes the need for sufficient truck access to enable the movement of goods, requiring ramps, freight elevators and loading bays, making site design more complex.

Traditionally, this meant that only smaller users were likely to occupy multi-storey industrial space, specifically those that needed to be located in proximity to the urban core, and were flexible in their space needs. However, as market pressures have increased, interest in multi-storey industrial projects is beginning to grow more broadly, with a number of new multi-storey projects in Metro Vancouver and similar North American markets in recent years. Some of these projects have involved a mix of uses and tenures, most commonly strata industrial space with office uses on upper floors.

Allowing for, and potentially incentivizing, the creation of multi-storey industrial space has the potential to modestly increase the supply of industrial space within the region, so long as the market conditions are sufficient to support it. It is important to recognize that such an option will be contextual within the region, as factors such as local rents, site size, industry activity, and geotechnical conditions will determine where such projects are feasible.

Other Forms of Industrial Intensification

In addition to increasing the amount of built space, there are a number of other ways to measure industrial intensity or density on the land base. These methods may not be as directly tied to land use policy, but can still accommodate increased employment and economic activity. Examples of utilization measures of industrial activity include:

- Labour activity (employees per land acre / hectare or per building sq. ft. / m²)
- Business revenue per unit (value generated per unit of land, or building floor area)
- Volume of goods produced / processed / stored per unit (per floor space, land area, employee)
- Vehicle or equipment movement per hour (trucks, loading, crane lifts)
- Quality and pay of jobs (education and pay levels)
- Value or level of equipment / technology investment (e.g. automation, racking warehouses)
- Transportation infrastructure utilization rates (goods / trips per unit)
- Building lease absorption period, vacancy rates, rental rates
- Longer hours of operation (shift work)

Industrial users are not the only ones constrained by the region's limited amount of available land, as the region's commercial, office, retail and other employment sectors also compete for space. While some of these uses are compatible with industrial activities, the market economics of these other uses may potentially undermine existing and potential industrial activity. These other employment uses typically tend to offer a higher return on investment

and higher rents on a per square foot basis, often pricing-out industrial land uses for floor space. Similarly, the rising popularity of industrial stratification, while providing the option of occupant-ownership, poses a potential disruptor to large scale industrial and trade-oriented activities, as the latter may not be able to compete with other industrial users for the remaining industrial land base.

Mixing Residential with Light Industrial Redevelopment in Specific Areas

With increasing competition for land and an overlap in regional and local land use policy objectives, some proponents are advocating for changes to zoning to allow for the development of mixed-residential uses in existing industrial areas, specifically for those in close proximity to rail rapid transit station areas. Advocates of this kind of zoning suggest that it will encourage the redevelopment of industrial space to modern standards while also addressing non-industrial policy objectives such as providing affordable housing and transit-oriented development. By mixing in higher-value residential uses, there is also the potential to cross-subsidize the redevelopment of new industrial space in an expensive urban context.

Given the conflicts that can occur between industrial and residential activities, the successful integration of these two uses is quite challenging. Generally speaking, only certain light industrial activities are compatible with sensitive residential uses in such close proximity, and even then, site design that ensures both livability and functionality for both uses can be difficult and expensive. Even with strict covenants on what uses are permitted, allowing higher value uses like residential also creates the risk of encouraging speculation on neighbouring industrial properties, and could result in the loss or displacement of the intended industrial uses over time.

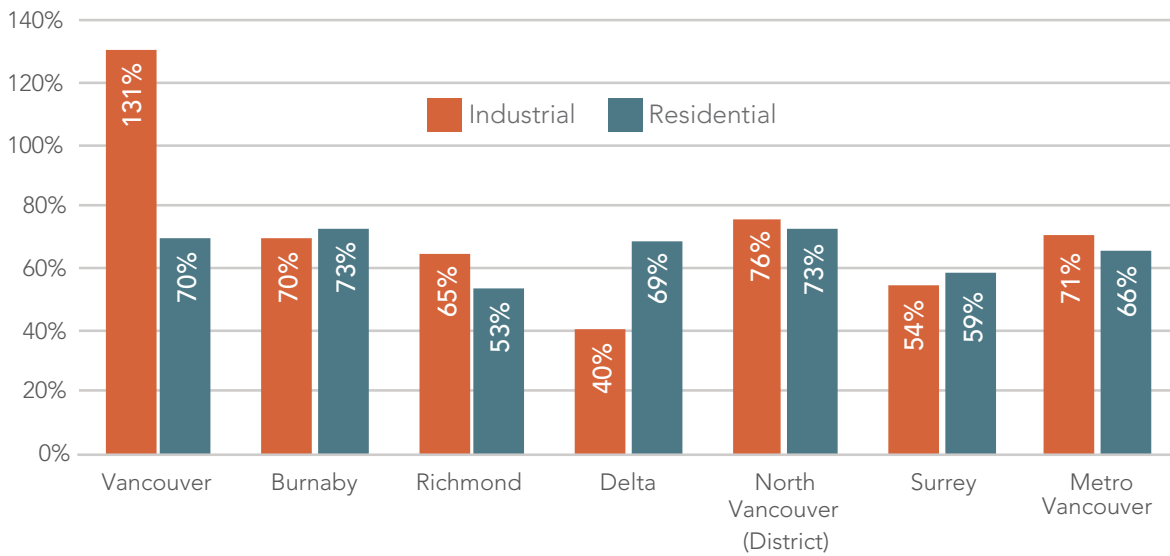
Accommodating Innovation and the Changing Nature of Work

When seeking to protect industrial lands for industrial uses, it is important to recognize that industrial activity can cover a broad range of businesses and involve the production of various types of goods. Not all industrial businesses produce goods one might otherwise associate with traditional industrial activities, such as concrete, construction goods or chemicals. As new technologies have emerged in recent decades, Metro Vancouver has benefited from the growth of numerous high-skill tech and innovation clusters. While these jobs are commonly associated with office-related employment, they also spur demand for space to accommodate the production of goods related to bio-tech, green tech, and other innovative products.

Demand for these types of spaces tends to focus on certain industrial sub-markets, most commonly found in proximity to post-secondary institutions and amenities, which in turn provide linkages to emerging high-skilled labour opportunities. These businesses often seek well-located and well-equipped facilities, which means they are often able to afford more expensive and urban industrial areas.



FIGURE 2: ASSESSMENT VALUE INCREASE, BY MUNICIPALITY, 2012-2017



SOURCE: ADAPTED FROM ANDY YAN, CITY PROGRAM, SFU, USING DATA FROM BC ASSESSMENT, 2019

Rising Land Values and the Impact of Taxation

As industrial areas continue to experience rising land values (Figure 2), this not only increases rental rates, but also taxation. Property tax in British Columbia is based on assessed land value; not just on its use as it is today, but as the 'highest and best' use of the land. As industrial areas begin to permit a broader mix of densities and uses, this means that the value of the land can rise sharply, in some cases well beyond the value being generated by the existing industrial users. For example, a small automotive repair business in an area that permits multi-storey office uses would be assessed as if it were a multi-storey office. In a five-year period between 2012 and 2017, the rate of industrial assessment growth outpaced residential assessment growth in the region, with the greatest impacts being observed in more urban municipalities.

This increase has the potential to put pressure on both landowners and leaseholders, as many leases are "triple net", meaning that the leaseholder is responsible for paying rent, along with building maintenance and property tax. As land prices and the associated assessment values rise across the region, so too can the tax payment – leading to an increase in costs that many businesses, particularly smaller ones, may not be able to absorb. These rising taxes may push businesses to relocate elsewhere in the region, or shut down entirely. As a result, this also creates pressure to convert industrial land to non-industrial use.

Industrial Edges and Buffers – Tools for Mitigating Conflict

Industrial land edges and buffers are a common means to mitigate conflict between industrial and adjacent sensitive uses, such as residential. Establishing these measures helps to ensure, for example, that residents are not negatively impacted by industrial activities, while also providing industrial users with a degree of certainty that they will be able to conduct their business without disruption.

There is no single approach among Metro Vancouver member jurisdictions in how to manage the interface between industrial and sensitive uses, though there are some common elements. These include design guidelines to manage noise, odour and light, and minimum setbacks to ensure a reasonable degree of physical separation from other uses. Most commonly these are established in a municipality's Official Community Plan (OCP) or zoning bylaw, requiring on-site mitigation measures at time of site development, but may also include more broad buffers, including transitional land uses (such as light industry or office parks) or other physical separation be located in between industrial and sensitive uses.

Depending on the approach and standard required, these measures can restrict the development of some industrial uses, or may require site plan elements that add additional cost to the design.

Addressing the Interplay between Industrial and Agricultural Land Uses

In much the same fashion of how speculation for other land uses is driving up real estate prices on industrial lands, so too is speculation impacting lands designated for agricultural uses across the region. While most of these lands are subject to provincial regulation as part of the Agricultural Land Reserve (ALR), which prevents non-agricultural uses, speculation from various land

uses is raising land prices, undermining the feasibility of using these lands for their intended agricultural use and driving pressure for conversion.

While both industrial and agricultural land bases are under pressure to convert from other land uses, there is also pressure between the two. Industrial and agricultural land uses are often posited against one another, with agricultural interests advocating that agri-industrial uses be located on industrial lands, and industrial interests advocating agri-industrial uses be allowed on agricultural lands.

Currently, ALR regulations limit industrial and commercial uses from locating on industrial land, requiring that 50% of the product involved in these activities be grown / raised on site. This "50/50 rule" plays a critical role in determining what agri-industrial activities are permitted on ALR land, barring approval from the Agricultural Land Commission. Limits to infrastructure, servicing, and transportation in both *Metro 2040* and ALR regulations also direct many agri-industrial uses to industrial areas instead. Given the comparatively high cost of industrial land, agri-industrial activities that qualify as farm use (or successfully apply to the Agricultural Land Commission for an exception) will usually locate on agricultural land instead. In a general sense, agri-industrial uses are not a significant component or threat to either land base. However, given the limited land supply and similarities between site profiles (large, flat sites) there is some pressure to convert agricultural lands to industrial uses, particularly in the case of sites that are considered under-performing or poorly located for their designated use.

The Importance of Transportation and Goods Movement

Metro Vancouver's network of trucking routes, highways, railways, and ports support both local serving businesses and the region's broader role as a gateway city between Canada and the world. From local deliveries, to the movement of materials and components for production, to importing and exporting goods to marketplaces beyond the region, having access to reliable and efficient methods of goods movement is an integral consideration for many industrial businesses.

Land use and transportation planning are complementary, although there are sometimes tensions between different types of uses and transportation implications. For example, large low-density industrial sites are typically cost ineffective to service via transit, whereas uses with greater densities of employees are more appropriately located by transit service. Furthermore, non-industrial uses in industrial areas can have negative traffic implications, such as increased transit demand that is difficult to efficiently serve and truck trips interaction with pedestrians. Accordingly, the following should be determined for appropriate locations for different types of industrial uses:

- Identifying industrial uses that benefit most from transit proximity (e.g. high employment density and person trips generation) and those that benefit least (e.g. low employment density, reliance on truck route access) to facilitate better alignment of use with transit service;
- Goods movement uses with higher goods movement needs should be located with access to the Major Road Network and Truck Route Network. This may in general apply to the more traditional heavy industrial uses; and

- More job intensive uses that generate significant person-trips are best located with access to the Frequent Transit Network (FTN), and likewise those uses that do not generate significant person trips are generally not the best use of land within closer proximity to the FTN.

Much as growth in the context of a finite land base is putting pressure on the region's industrial land base, so too does it put pressure on the region's transportation infrastructure network for moving goods and people. Concerns related to congestion, aging infrastructure, road safety, pollution and other externalities like truck parking all affect the outlook for lands throughout the region. Towards this end, planning for the effective management of industrial land is intertwined with planning for the effective management of the transportation network.

A number of organizations and agencies in the region have made strides towards supporting the effective management of the transportation network, including initiatives such as:

- TransLink's Regional Goods Movement Strategy and Regional Transportation Strategy;
- The BC Ministry of Transportation and Infrastructure's innovations and improvements in commercial vehicle monitoring and emissions standards;
- The Greater Vancouver Gateway Council's work to coordinate investment on regional infrastructure projects; and
- The Greater Vancouver Urban Freight Council's work to coordinate and champion initiatives related to goods movement.

Climate Change Vulnerability

Another issue of note impacting the long-term functionality of the industrial land supply is the risk posed by climate change. Detailed climate change projections have been completed for the Metro Vancouver region, and significant work is underway to understand the impacts, including increased flood risk.

A significant portion of the region's industrial land supply, including most major port and airport facilities, are located within low-lying areas. Many of these areas would be vulnerable to major coastal flood events in the absence of considerable adaptation measures. Should sea level rise continue as projected, significant portions of the industrial land supply may no longer be considered viable for long-term development, further reducing opportunities for growth and economic resiliency. While certain measures can be engineered to adapt to this risk, including diking and improved site design, these measures may be cost prohibitive for many industrial users to implement.

In addition to responding to the impacts of a changing climate, industrial activities also have a role to play with regards reducing regional greenhouse gas (GHG) emissions. Metro Vancouver is currently developing Climate 2050, which reflect climate issues specific to the region by addressing ten key issue areas, one of which is industrial activities. Metro Vancouver's manufacturing sector (including cement production, food processing, metal fabrication, chemical manufacturing, forest products, and petroleum refining) and construction industry contribute approximately 23% of the region's total GHG emissions combined.

As part of developing Climate 2050, the region is preparing "roadmaps" for each issue area, providing a summary analysis of current conditions and challenges facing each issue area. These roadmaps will identify regional and corporate goals and actions necessary to achieve a carbon neutral, resilient region. In the case of industrial activities, this is likely to include targeted approaches to reduce emissions by encouraging or requiring switching to low carbon fuels and adopting new technologies where appropriate.



The Future of Industry and Changing Space Needs

Like many other industrial markets around the world, the region's industrial lands have undergone a long-term transition in the nature and form of their use. A steady shift away from heavy manufacturing and natural resource sectors, particularly amongst forestry-related businesses, coupled with increasing competition with other markets in an increasingly globalized economy, have changed the balance and character of the region's industrial lands. Throughout this transition, the region has evolved a diverse sectoral mix, while also retaining its role as a leading global port and economic gateway.

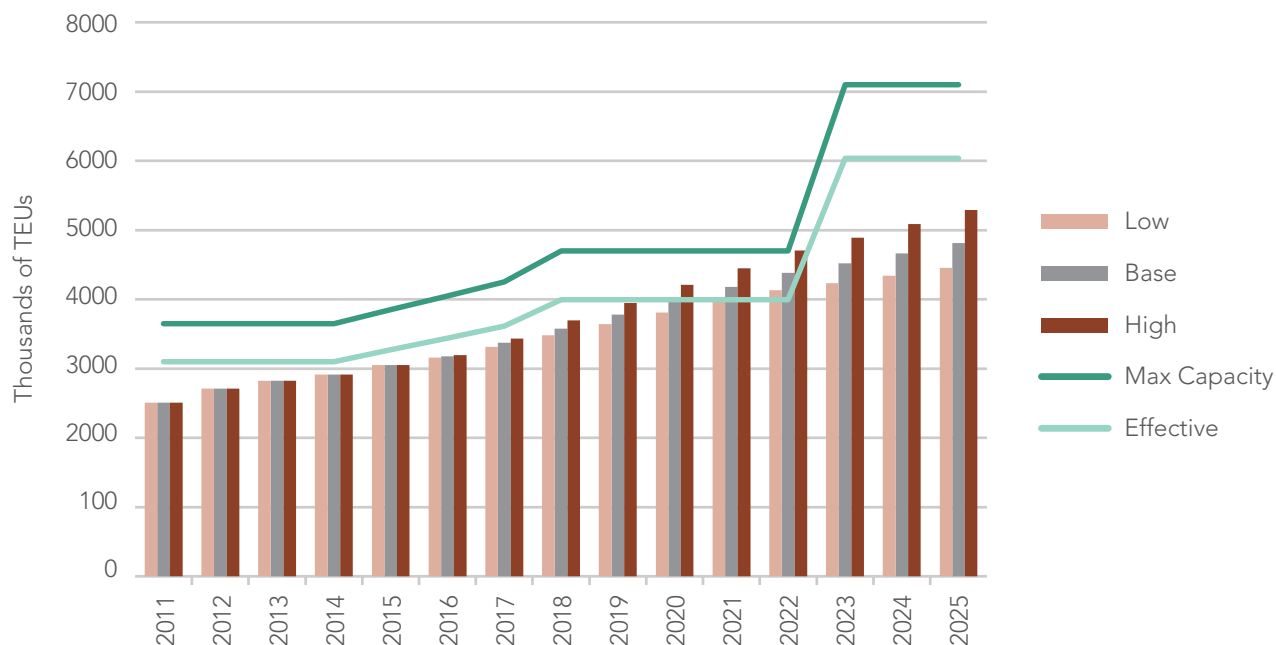
For example, locations including Granville Island, shores of False Creek, Fraser River and Burrard Inlet were once dominated by heavy industry, sawmills, large scale manufacturing, and rail operation. While a few of these uses still exist today, many industrial businesses have moved eastward or have ceased operation altogether, being replaced by predominantly commercial and residential development. In instances where employment uses have been retained, they tend to be in the form of light industrial, office and mixed commercial space.

Though some traditional waterfront-oriented industrial activities have relocated or dissipated, the core 'gateway' functions of the Port of Vancouver and Airport have continued to thrive. Continued growth in global trade have driven demand for additional space, including the need for additional container terminals, warehousing space, and logistical facilities.

In terms of growth, the Port of Vancouver has consistently outperformed other North American ports since 1990, with growth in container traffic forecasted to outpace all other ports in the Pacific Northwest for the foreseeable future. Even with the establishment of a second provincial port in Prince Rupert and improvements and expansions to local port facilities, demand is forecast to drive the need for additional port-related facilities on Metro Vancouver's industrial lands, as shown in Figure 3.

Trade-oriented uses associated with the Port and Airport represent only one part of the demand for industrial lands. Despite the changing nature of industrial activities in the region, industrial lands continue to also serve a fundamental role in facilitating city-serving activities, such as vehicle repair, food production, commercial laundry, utilities, light manufacturing and local distribution. These activities are crucial to the continued function of a range of economic activities across the region, including tourism (tour bus repair, restocking cruise ships), major office employment (printing, couriers, transit infrastructure management), and local retail businesses (food production and distribution, wholesales). At the same time, continued population growth has increased the in-region demand for goods and related services, such as e-commerce, transportation, warehousing and distribution space, particularly last mile and fulfillment centres in close proximity to major population centres.

FIGURE 3: PORT OF VANCOUVER CAPACITY AND DEMAND DEVELOPMENT TO 2025



SOURCE: CONTAINER TRAFFIC FORECAST STUDY – PORT OF VANCOUVER, 2016

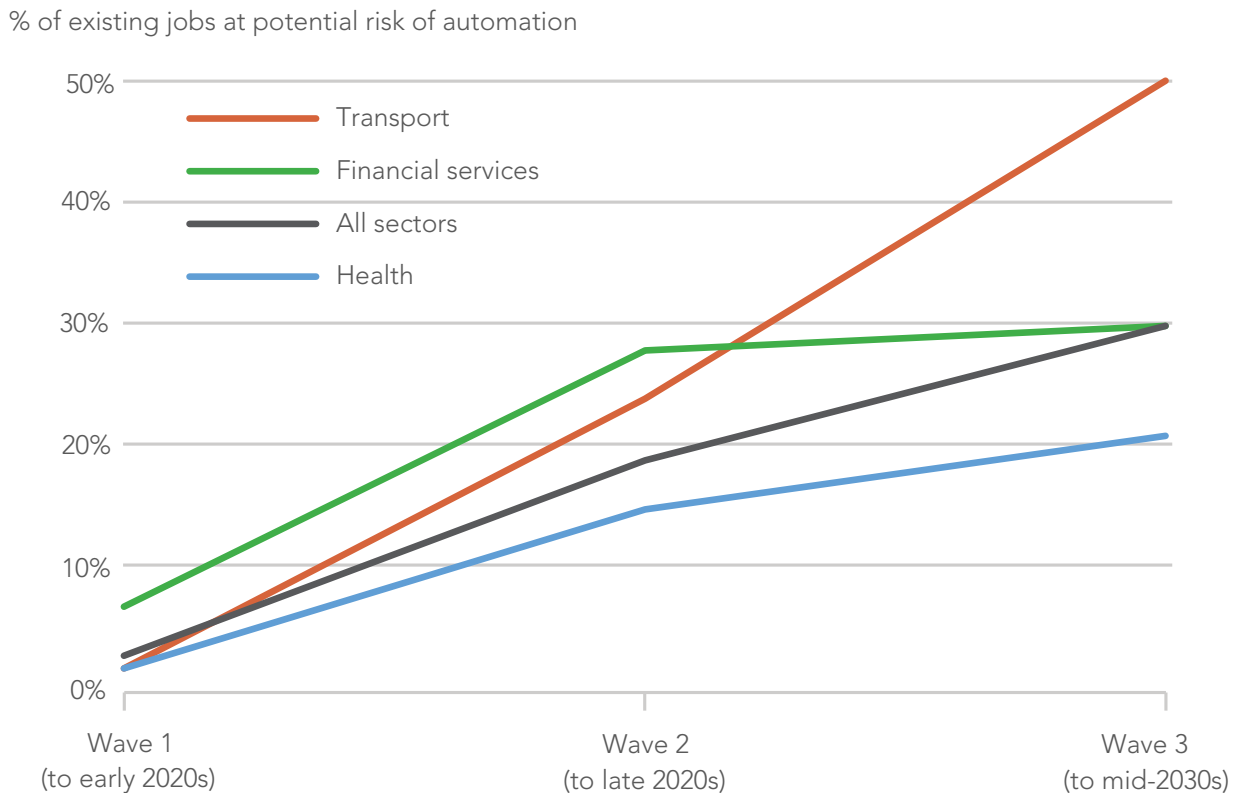
NOTE: DEMAND AND CAPACITY ARE MEASURED IN TWENTY-FOOT EQUIVALENT UNITS (TEU). DEMAND SCENARIOS PRESENT A LOW, BASE AND HIGH SCENARIO FOR EACH YEAR. THE INCREASE IN CAPACITY IN 2023 REPRESENTS THE ANTICIPATED EXPANSION OF ROBERTS BANK CONTAINER TERMINAL 2.

In addition to these city-serving functions, industrial lands are also playing a prominent role in accommodating the region's emerging innovation economy. These businesses do not fit easily into any one classic or conventional employment sector, as the nature of their work encompasses a wide range of activities including light manufacturing, media and digital entertainment production, clean-tech and bio-tech, software and hardware design, and various other uses. The space needs of these businesses can vary, but typically require a diversity of spaces at different scales, including offices, production space, and logistical facilities, many of which are most commonly associated with industrial and employment lands.

Changing industry profiles are not the only factor resulting in different land and space needs. Technological innovations are also dramatically changing the relationship between employment, productivity, and how industrial lands are occupied and used.

Automation, which has already significantly affected the manufacturing industry, is expected to continue to change how goods are produced and tasks accomplished. With the potential advent of artificial intelligence and advanced robotics, there is a real possibility that more jobs in many more sectors could be affected by automation than ever, with 42% of the

FIGURE 4: ESTIMATED POTENTIAL JOB AUTOMATION RATES
BY INDUSTRY ACROSS WAVES

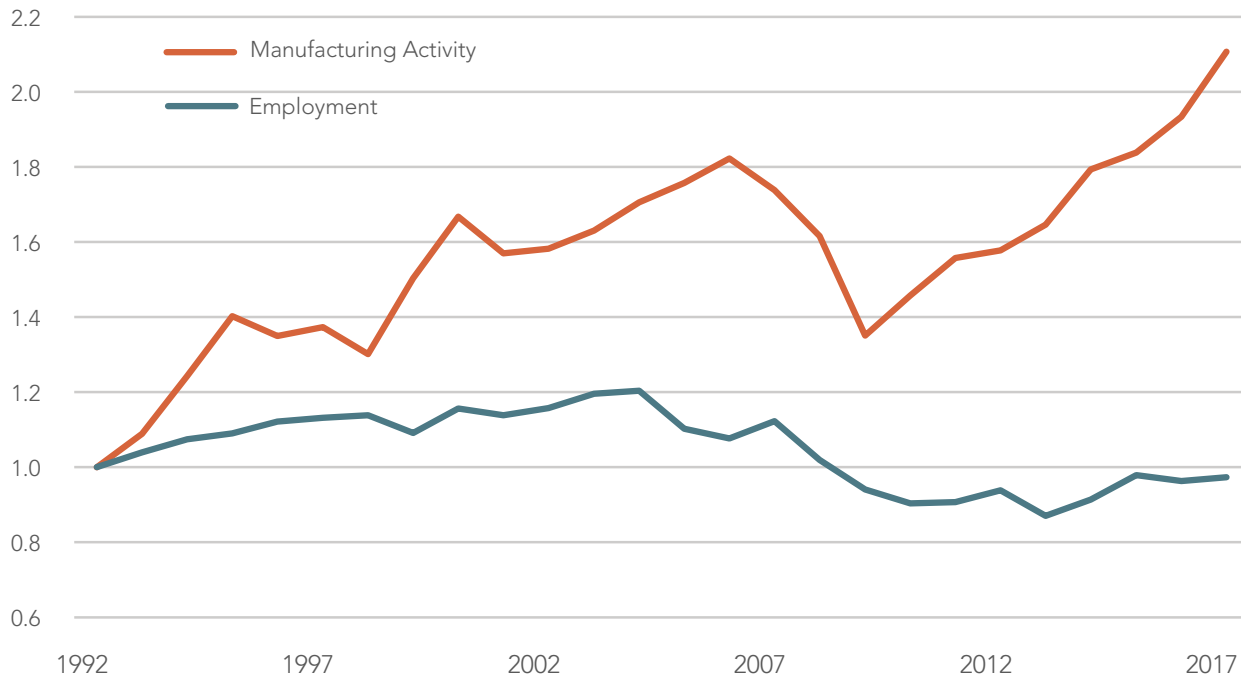


SOURCE: 'WILL ROBOTS REALLY STEAL OUR JOBS?', PWC, BASED ON OECD PIAAC DATA, 2018

Canadian labour force at high risk of being affected by automation in the next decade or two. As shown in Figure 4, artificial intelligence, advanced robotics, and automation will have a different impact on different sectors over time, with some sectors being susceptible to a significantly reduced need for labour as different waves of technological advancement occur over the next few decades.

This change has the potential to affect the demand for labour in different sectors, but it does not necessarily mean that there will be any less demand for industrial land. While automation can lead to a number of operational efficiencies, it typically still requires space to conduct the business activity (e.g. production and warehousing of goods). As shown in Figure 5, manufacturing productivity in British Columbia has continued to grow, despite declines in total manufacturing employment.

FIGURE 5: COMPARISON OF ANNUAL MANUFACTURING ACTIVITY (DOLLARS)
AND EMPLOYMENT, PROVINCE OF BRITISH COLUMBIA, 1992-2017



SOURCE: HEMSON CONSULTING LTD. USING DATA FROM STATISTICS CANADA TABLES 304-0015 & 14-10-0023-01

NOTE: FIGURES HAVE BEEN INDEXED (1992 = 1)

The separation between manufacturing activity and employment has been occurring for quite some time in North America; however, this trend began to accelerate most notably following the 2008-2009 Global Recession. While manufacturing employment has yet to recover to its pre-recession levels, manufacturing activity has continued to grow.

Over this same period (2008-2018), Metro Vancouver increased its industrial floor space by over 30 million square feet. This translates into an average growth rate of nearly 1.7% per year, significantly higher than the average annual industrial employment growth rate of 0.5% per year. While the amount of land take-up associated with this growth has declined over time (due to limited supply), suggesting denser industrial buildings are being built, the demand for space is still resulting in a net demand for land.

Industrial Land Demand Forecast to 2030 and 2050

Forecasting the longevity of the industrial land supply is a particularly challenging task in Metro Vancouver because of its unique situation as a region with a fixed land supply. While not quantifiable for the overall market, there is ample evidence that industrial development and land absorption is being limited by the availability of land for development. Some larger distribution and manufacturing facilities are already locating outside of Metro Vancouver simply because there are few large industrial sites available today or likely to be available in the future.

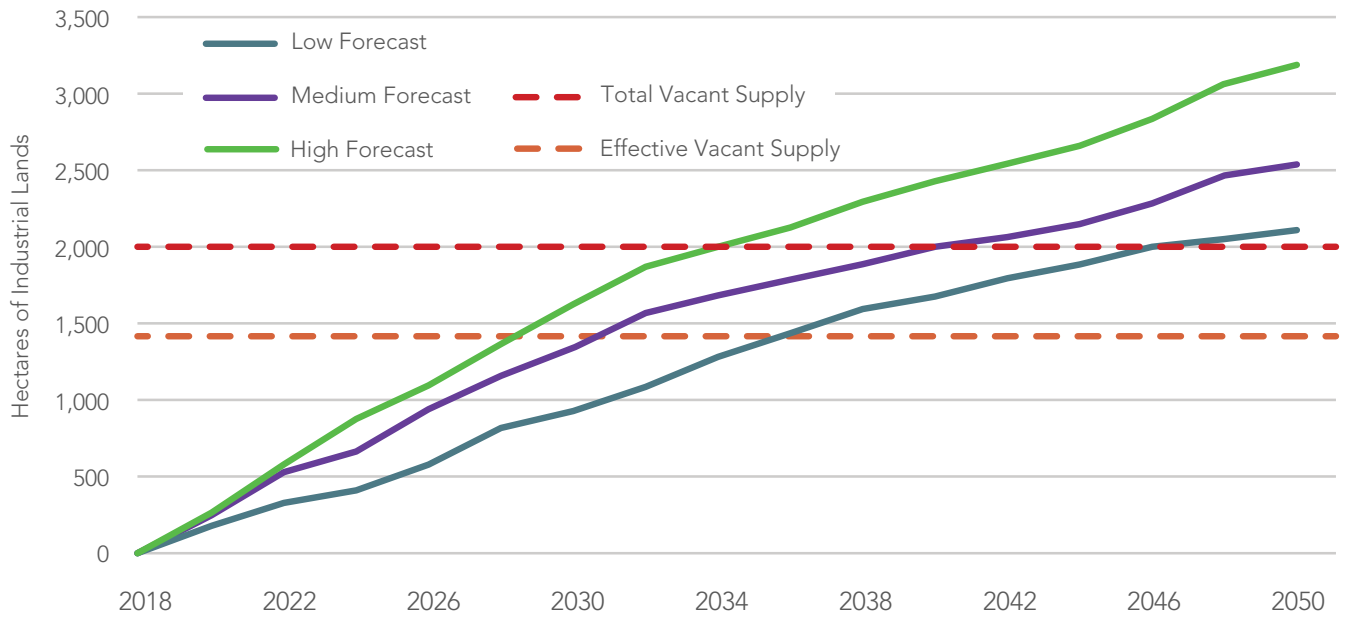
The most recently prepared forecast of regional industrial land demand used an absorption method with a range of 80 to 110 ha per year (200 to 275 acres per year). Applied to the effective supply of 1,820 ha (4,500 acres) identified in its analysis, full development would be reached between 2035 and 2045. Of course, land development does not proceed in a linear fashion until the last parcel is consumed, rather it nearly always tails off because the remaining land is of limited viability. This means that there is demand that cannot be met by the available supply well before a point of build out is reached.

These unconstrained land demand forecasts are shown in Figure 6. Depending on the scenario, the region is anticipated to absorb the last of its effective supply sometime between 2028 and 2035, with the total inventory being absorbed sometime between 2035 and 2047. However, as previously noted, absorption is likely to tail off as supply dwindles. Figure 7 demonstrates what the demand and land absorption is likely to look like when factoring in the effects of a constrained land supply.

In the absence of additional land to meet the needs of continued demand, the rate of land absorption will tail off until near full development is reached. This decline in absorption will not be due to a lack of interest, but instead will be the result of businesses and jobs having relocated to markets that are more readily able to meet their needs. The combination of a constrained land supply and steady demand will keep the price of industrial land high, likely even higher than experienced today. This will be a limiting factor for some industrial activities, but will likely also force more intensive use of land where possible.

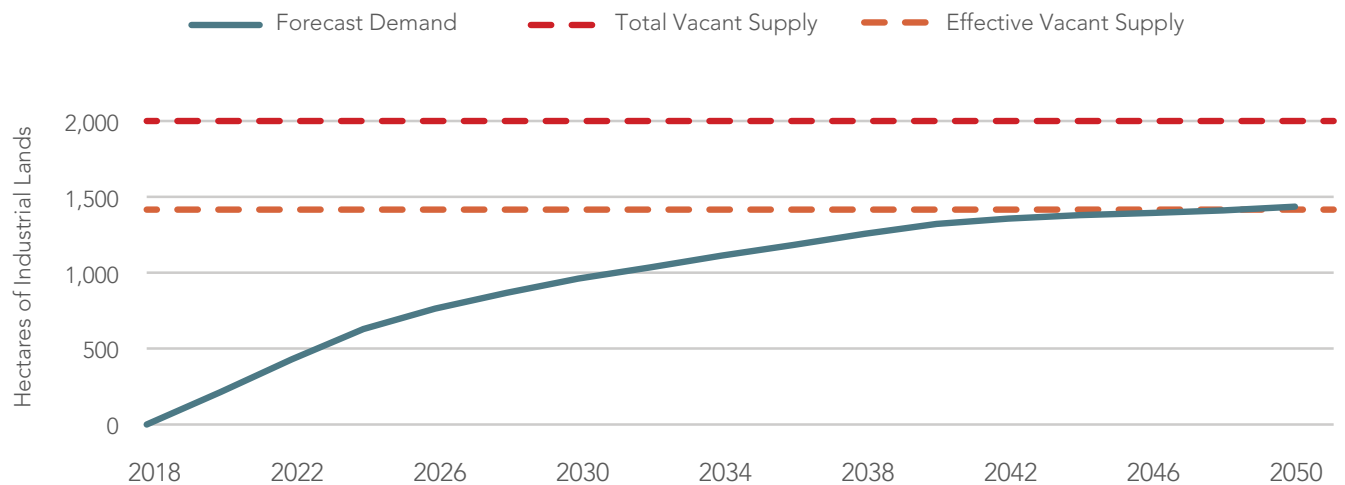


FIGURE 6: EMPLOYMENT GROWTH BASED INDUSTRIAL LAND DEMAND FORECAST, 2019 TO 2050



SOURCE: HEMSON CONSULTING LTD.

FIGURE 7: CONSTRAINT BASED FORECAST OF INDUSTRIAL LAND DEMAND, 2019 TO 2050



SOURCE: HEMSON CONSULTING LTD.

Implications for the Regional Economy

Industrial lands serve a crucial role in accommodating a diverse and resilient economy. Regions that are more economically diverse tend to experience less instability and lower unemployment rates when compared to regions with more homogeneous economies. Complex economies, that is those that feature a web of interconnected industries and business sectors, also tend to be far more resilient, experiencing less economic decline during periods of recession, and faster recoveries.

In the same regard, failing to ensure a suitable amount of industrial land could limit growth and investment, and even weaken existing sectors that rely on these lands. Even with the changing nature of work and technological innovations resulting in evolving land and space needs, industrial lands will still be necessary to ensure regional economic diversity and resiliency for the future.

The consequences of a constrained land supply are already being witnessed, with a number of large industrial businesses and activities being unable to find suitable space to locate or expand their business. For example, the Port of Vancouver has noted that, despite significant improvements to site intensification and optimization in recent years, it may be unable to accommodate future trade demand given its current land supply. As the Port approaches its capacity, it risks losing business to other ports along the western coast.

Similarly, a shortage of space for logistics terminals is seeing demand grow for inland distribution ports as far away as Calgary. These inland ports serve as staging areas for containers that are unloaded in Vancouver, transported by rail to the distribution hub to be unpacked and sorted, and then (for some of the cargo) transported back to Vancouver to be delivered to end users and points of sale. Ashcroft offers an inland terminal focused on processing exports, such as lumber in containers. In other cases, larger manufacturers are choosing to locate, or relocate, to areas with more available and affordable land south of the border, in communities such as Blaine, Washington.

The relocation of these business and trade activity could have a negative impact on the region, province and even country. For example:

- Should a business relocate to another port in the province, such as Prince Rupert, the impact might be neutral for British Columbia and Canada, but negative for the Metro Vancouver region;
- Should a business relocate to Alberta, the net impact might be neutral for Canada, but negative to the Metro Vancouver region and British Columbia; and
- Should a business relocate south of the border (such as Washington, Oregon or California), the net impact would be negative to the Metro Vancouver region, British Columbia, and Canada.

The potential impacts of losing industrial activity due to lack of available land are threefold:

- The loss of local businesses could negatively affect the economy due to the loss of potential or existing jobs and tax revenues for governments.
- The further abroad these businesses locate, the greater the distance goods must be transported, resulting in increased fuel consumption, GHG emissions, traffic congestion and cost to consumers.
- The loss of industrial activity could weaken the region's economic diversity and resiliency, potentially exposing the economy to greater fluctuations in market cycles and broader economic shifts.

It is worth noting that adding new industrial uses in Metro Vancouver could result in its own set of costs and challenges, including increased competition for land and the potential to increase traffic, business related emissions, and possible conflicts with other uses. However, on the whole, these costs and challenges are unlikely to be greater than the net loss and environmental impact that is likely to occur if these businesses relocate elsewhere.



RECOMMENDATIONS

To achieve the Strategy's Vision, and to address the Challenges facing industrial lands in the region, 4 'Big Moves' have been identified:

1. Protect Remaining Industrial Lands
2. Intensify and Optimize Industrial Lands
3. Bring the Existing Land Supply to Market & Address Site Issues
4. Ensure a Coordinated Approach

The 4 'Big Moves' are used to frame and organize the Strategy's 34 recommendations and 10 priority actions.

In considering the 4 'Big Moves' and the recommendations that follow, the Strategy affirms that conversion or use of agricultural lands is not a solution to the shortage of industrial lands in the region. This principle was endorsed by the Industrial Lands Strategy Task Force and Metro Vancouver Board.

Protect Remaining Industrial Lands

Given the ongoing and projected demand for industrial land in the region, it is imperative to protect the region's remaining industrial lands and curb the threats that undermine their use for industrial activities. This means lands for trade-oriented purposes as well as for small, local serving businesses; businesses that are responding to the changing nature of industry as well as more traditional industrial activities.

RECOMMENDATION 1:

That Metro Vancouver, in collaboration with member jurisdictions, First Nations, regional agencies, and other relevant stakeholders, conduct a comprehensive regional land use assessment.

In the context of Metro Vancouver's finite and constrained land base, there is significant pressure for competing, legitimate land uses across the region. There is an opportunity to undertake either a targeted or region-wide assessment of land use, seeking not to reflect what is already in existing policy frameworks, but rather to proactively identify the 'best' locations for different land uses depending on a developed set of criteria (e.g. location, adjacent land uses, lot size, proximity to the goods movement network). Such an assessment could potentially identify areas where a swap of existing land uses may be reasonable, including opportunities to optimize the remaining supply of industrial land.

This assessment will identify, based on a defined set of criteria and cross-jurisdictional considerations, opportunities for more optimized locations and uses of land in order to support regional and local policy objectives and to inform policy changes.

The regional land use assessment will include:

- Metro Vancouver and member jurisdictions work with the Provincial and Federal governments, the Port of Vancouver, and Vancouver Airport Authority, to identify if industrial functions can be integrated or co-located on public industrial lands. This includes examining the potential for joint use for large non-intensive land uses such as extractive activities, public rights-of-way, and others;
- where existing designated land uses are determined to be under-utilized, Metro Vancouver, in consultation with the owners of the subject lands, the member jurisdiction in which they are located, and other relevant stakeholders, may consider the potential for swaps with other land uses. The proposed swap should be considered between lands that are identified as being under-utilized through the land use assessment, located within the same jurisdiction or elsewhere within the region, and improve the potential for the intended uses per the land use assessment criteria; and
- Metro Vancouver assess whether or not there are sufficient supplies of suitable land to reasonably accommodate retail, service or recreational uses that may be restricted from otherwise locating within Industrial areas.

RECOMMENDATION 2:

That Metro Vancouver endeavour to strengthen regional policy to protect industrial lands as part of the update to the regional growth strategy, *Metro 2040*.

Examples of ways to strengthen the regional growth strategy to be considered are:

- increase the voting threshold required for a minor amendment of the Industrial and / or Mixed Employment regional land use designation to General Urban;
- explore 'no net loss of land' as part of the amendment criteria for Industrial lands, recognizing the regional benefit of conversions that are offset by land-swaps in suitable locations within the municipality or elsewhere in the region;
- clarify the definitions and permitted uses on the Industrial and Mixed Employment regional land use designations, including appropriate principal and accessory uses by type and scale; and
- explore permission of mixed-use including residential on Mixed Employment lands immediately proximate to Rail Rapid Transit Stations (distance to be determined, suggest 200 metres) as long as existing industrial space is maintained or expanded and other *Metro 2040* objectives are met (e.g. affordable, rental housing).

RECOMMENDATION 3:

That Metro Vancouver, in collaboration with member jurisdictions and other regional agencies, develop a consistent definition of 'industrial' and guidelines for which primary and secondary (or ancillary) land uses should be permitted in Industrial and Mixed Employment designated areas, respectively.

Member municipalities, as part of their regional context statement updates, would commit to review and update their zoning bylaws using the guidelines as a resource to amend the permitted uses in their local industrial and employment lands.

The definition of 'industrial' in zoning bylaws and associated permitted uses (and scale of accessory uses) varies significantly across the region. Different zoning bylaws permit accessory and non-industrial uses, such as recreation, big-box retail, and places of worship, on industrially zoned lands, creating additional competition for industrial land from uses that should be located elsewhere. The bylaw review and update should remove non-industrial uses from the industrial zoning while adding new types of industrial uses, and limit the scale or size of accessory uses. Definition consistency would limit non-industrial, commercial and institutional uses in certain industrial locations across the region, and would create a clearer understanding and more consistent permissible uses while aligning with the regional growth strategy.

Protecting Trade-Oriented Lands

The following three recommendations have to do with protecting trade-oriented lands in the region. It is important to recognize that trade-oriented industrial uses (e.g. logistics, warehouses, distribution centres, transportation terminals) are a crucial part of the region's role as an economic gateway between the

Asia-Pacific and the rest of the country. These uses typically have a very specific set of location and site needs, such as large, flat sites that allow for movement and storage of goods, with proximate access to highway, port, or rail infrastructure. Amongst the region's industrial land supply, sites that offer these characteristics are limited.

Because these sites also tend to offer attractive characteristics for other types of industrial (and other) uses, trade-oriented uses often compete with other industrial uses when looking to purchase and develop these lands. The low-density nature of many trade-oriented industrial uses means that they often struggle to compete, particularly when increased development densities are permitted under more broad industrial zoning. This is of particular concern when stratification of industrial is permitted, which tends to both significantly increase the price competing users can offer for the land, while also fragmenting ownership of the property, which can be an issue for operators that require large sites. To ensure these strategically located sites are protected, a coordinated approach is required between the region and member jurisdictions.

That is not to say that smaller lots and stratification do not play a critical role in the region. On the contrary, the importance of stratification is recognized in enabling small businesses to own their space, have security of tenure, and opportunity for capital appreciation. Other recommendations are focused on different types of industrial businesses.

Based on feedback from stakeholders, there is a desire when looking for solutions to address conversion of industrial lands to other uses, to first consider local government and regional policy options, rather than a provincial industrial land reserve.

RECOMMENDATION 4:

That Metro Vancouver conduct a collaborative process to develop a clear definition of Trade-Oriented Lands, and subsequently, as part of the *Metro 2040* update, develop a Trade-Oriented land use overlay.

A clear, consistent and collaboratively developed definition and understanding of the extent and location of these lands will support their protection as well as the protection of other types of important industrial lands. A regional overlay identified in the regional growth strategy would distinguish trade-oriented lands within the Industrial land use designation based on confirmed criteria (i.e. site size, proximity to transportation infrastructure linkages) and provide additional guidance to member jurisdictions regarding permitted land uses to protect trade-oriented industrial activities.

RECOMMENDATION 5:

That municipalities identify appropriate areas through Trade-Oriented zoning.

Subsequent to participating in the development of a consistent and collaboratively developed definition for trade-oriented lands, municipalities, through the regional context statement process, would be asked to identify trade-oriented lands within their jurisdiction, and to consider zoning that would limit or restrict non-industrial uses and the fragmentation of parcels with an aim to protecting these strategically located lands for trade-oriented activity. This would be a judiciously applied approach, recognizing the potential limits to flexibility and future use that this zoning would place on these lands.

RECOMMENDATION 6:

That the Province grant municipalities the legislative powers to define permitted forms of tenure (i.e. leasehold versus stratified freehold) on industrial land through local zoning bylaws.

Given the limited control municipalities have over tenure of non-residential land uses through zoning, the province would be requested to grant additional powers under the Local Government Act to ensure these sites are reserved for their intended use. Member jurisdictions may seek to limit stratification of ownership to a minimum size to prevent the fragmentation of large trade-oriented parcels.

Intensify and Optimize Industrial Lands

Given the region's constrained land base, it is critical to encourage the most efficient use of the remaining industrial land supply for all types of industrial users, to remove barriers to the intensification of industrial land, and to investigate opportunities to optimize the location of certain land uses over time. Currently multi-storey industrial buildings are rare in most North American markets because, for most uses, multi-storey space is much more expensive than single storey buildings. However, as the remaining industrial land supply is diminished, it may become feasible for more of this type of development to occur in the future. To encourage more efficient use, it would be beneficial to industrial users to be able to locate in more intensive / dense built forms not restricted by policy or regulation. In addition to removing height and density restrictions in areas that have the opportune market and physical characteristics necessary to support intensive development, municipalities may also seek to offer incentives to attract and realize this form of development in strategic locations.

RECOMMENDATION 7:

That municipalities facilitate the intensification / densification of industrial forms where possible.

Actions to support industrial intensification include:

- removing any unnecessary restrictions to density or height limits, where contextually appropriate;
- planning the space to accommodate new, smaller industrial uses when older, centrally located industrial areas densify;
- allowing mixing of industrial with other employment uses so long as the industrial component is secured as a condition of redevelopment; and
- exploring opportunities to encourage intensification in target areas (i.e. proper geotechnical conditions, access to infrastructure and transit) via incentives. These could include pre-zoning, density bonuses, financial incentives, and/or others.

RECOMMENDATION 8:

That Metro Vancouver conduct a study of the financial factors and other issues that prevent the development of multi-storey industrial spaces in various regional market areas.

The study should identify the gap between development costs and market rents or sales prices in different municipalities to assess if there are ways that Metro Vancouver and member jurisdictions could improve the feasibility of delivering higher-density industrial spaces.

RECOMMENDATION 9:

That the Province review the current approach to property tax assessment and tax rates based on the highest and best use of a property with regard to its impact on industrial businesses.

Growth in assessment values on the region's industrial lands have risen rapidly in recent years. The associated rise in property taxes is putting increasing pressure on industrial businesses, particularly in high growth and high value urban areas where assessing property is based on the highest and best use. These costs can displace industrial users and undermine the intended use for the land. This review should identify if there are reasonable approaches to mitigate the destabilizing effect of rapidly rising assessments and taxes on industrial businesses.

Provide Greater Clarity on Agri-Industrial Uses in the Agricultural Land Reserve

While both industrial and agricultural lands are under pressure to convert to other land uses, these two types of land uses are often posited against one another, particularly as it relates to agri-industrial businesses. While there are some permissible agri-industrial uses within the Agricultural Land Reserve, there is some uncertainty over the scope and scale of activity that can be permitted before these uses would have to relocate to industrial areas.

RECOMMENDATION 10:

That the Ministry of Agriculture, in consultation with the Agricultural Land Commission, amend legislation to define under what conditions, if any, large-scale organic waste processing facilities are permitted in the Agricultural Land Reserve.

RECOMMENDATION 11:

That the Ministry of Agriculture produce guidelines or amend legislation to: define the metrics used to measure the 50/50 rule (i.e. volume, weight, value, etc.) to facilitate consistent application; and clarify the permitted value-added infrastructure of an approved use on an agricultural parcel and define a cap on building infrastructure footprint.

Ensure Consideration of Industrial Lands in a Regional Flood Management Strategy

Climate change and the impacts of rising sea levels are likely to be one of the greatest challenges of our time, and their potential impact on the industrial land supply cannot be ignored. A significant portion of the region's industrial land supply, including most major port and airport facilities, are located within low-lying areas that are anticipated to be at risk in the face of rising sea levels and more frequent flooding events. As the Fraser Basin Council develops a flood management strategy for the region, it is critical that that industrial lands be given proper consideration to ensure the continued economic resiliency of the region, while also recognizing the cost that adaptation measures may have on industrial development in the future.

RECOMMENDATION 12:

That in developing a Regional Flood Management Strategy, the Fraser Basin Council recognize industrial lands and their economic contributions to the broader region when identifying and prioritizing measures to adapt to rising sea levels and flood events.

Bring the Existing Supply to Market & Address Site Issues

To ensure the region's remaining vacant industrial land supply gets to market, identify the local issues facing these lands and the linkages between them, and map out the necessary policy updates and strategic investments in response.

Manage Land Use Conflicts through Buffering Policies

Managing the interface between industrial uses and other sensitive land uses, particularly residential, is a significant issue when it comes to preserving the functionality of existing industrial uses, while also protecting for future economic activity. A broad range of planning tools exist that may contribute to the mitigation of potential, existing or perceived nuisances, offering varying degrees of flexibility or conversely, prescriptiveness. Member jurisdictions across Metro Vancouver incorporate a range of approaches to managing this interface. However, while there are common elements amongst these approaches, there is no single approach that meets the context and needs of each community. While there is little appetite for a "one-size-fits-all" approach to interface management, there are certain leading practices that could be formally recognized through regional guidance to encourage more effective management of industrial edges and conflict prevention.

RECOMMENDATION 13:

That Metro Vancouver, in consultation with member jurisdictions, develop guidelines for land use policies along the edge of planned or developing Industrial areas where no natural or other physical buffer already exists.

These guidelines will serve as reference for member jurisdictions on how to mitigate the potential for conflict with other sensitive uses depending on the context:

- Where the industrial area is already established, direct employment uses, including light industrial, commercial, and office, or other non-sensitive uses to the area abutting the Industrial lands to serve as a transitional land use.
- Where the industrial use is encroaching upon an established sensitive use, direct light industrial and ancillary components of the primary industrial use to the edge of the industrial area where the sensitive use is located to serve as a transitional / buffer land use.
- In cases where the abuttal of industrial and sensitive uses cannot be separated by other land uses, the guidelines should also provide direction towards:
 - Site design best practices including recommended minimum setbacks for different industrial uses, on-site migration measures such as visual screening of mechanical equipment, requirements for enclosing industrial activities, lighting fixtures, and recommended performance standards, where appropriate.
 - Mitigation requirements for residential and other sensitive uses that are encroaching upon areas of existing or planned industrial activity and associated vehicle traffic, including consideration for the potential for future intensification of industrial uses.

RECOMMENDATION 14:

That the Province enable municipalities the discretion to place a warning of anticipated nuisance effects on the title of the lands with sensitive uses that are being developed within a defined proximity of an established or planned industrial use or goods movement corridor.

The notification warning could be included in any offers of purchase and sale, notifying current and prospective owners and tenants that they are within the potential area of influence of the industrial use, and may experience adverse effects as a result.

RECOMMENDATION 15:

That municipalities consider adopting the Railway Association of Canada and Federation of Canadian Municipalities Guidelines for New Development in Proximity to Railway Operations when considering development permissions and changes of use in proximity to active railway corridors and other major goods movement corridors.

Develop Local Bring-To-Market Strategies

Certain parts of the vacant industrial land supply suffer from site-specific challenges, such as limited infrastructure support, environmental concerns, and under-sized parcels. Where vacant lands have not come to the market, municipalities may benefit from the preparation of a bring-to-market strategy. Such a strategy would identify the issues that prevent the development of undeveloped or under-developed industrial land, while providing a roadmap to achieve the municipality's economic goals for its remaining industrial lands.

RECOMMENDATION 16:

That municipalities with vacant or under-developed industrial lands prepare a bring-to-market strategy for their industrial land supply.

The strategy should address:

- assessing the strengths, weaknesses, opportunities and challenges facing the development of vacant industrial land;
- identifying opportunities to encourage reinvestment and more intensive use of existing industrial lands;
- whether municipal assembly and consolidation of fragmented parcels may be required, or is feasible, in order to bring the lands to market;
- if environmental remediation is required and if current programs are sufficient to facilitate the required degree of site remediation; and
- if local servicing and infrastructure improvements are required and can be accommodated through front-ending infrastructure investments or cost-sharing agreements.

Ensure Transportation Connectivity

The efficient movement of goods and people is critical for many industrial businesses, both local serving and trade-oriented, in terms of where they choose to locate. The maintenance and growth of a resilient and reliable transportation network, including various modes of transit, is an integral component in planning for the effective management of the region's industrial lands. Through the development of the Strategy, transportation issues and linkages were consistently rated one of the highest priorities. As a result, seven recommendations focus on this critical issue.

RECOMMENDATION 17:

That TransLink, Metro Vancouver, the Port of Vancouver, the Vancouver Airport Authority and municipalities continue to work together to proactively designate, manage, and regularly update the Regional Truck Route Network in line with the recommendations of the Regional Goods Movement Strategy to ensure the safety and reliability of the regional goods movement network, while also considering human health implications, such as air quality and noise.

RECOMMENDATION 18:

That TransLink, as part of the update to the Regional Transportation Strategy and implementation through Investment Plans, continue to identify viable new opportunities to create and improve transit linkages between the region's industrial areas and local workers, where such transit can operate efficiently and effectively as part of the region's transit network.

RECOMMENDATION 19:

That the Port of Vancouver, Metro Vancouver, TransLink, the Vancouver Airport Authority, and rail line operators, work together to identify policies and actions that support the optimization and safety of goods movement to and from industrial lands via roads, highways, railways, air, and access points to navigable waterways including short sea shipping.

RECOMMENDATION 20:

That the Province work with municipalities and industry partners to understand, forecast, plan for, and mitigate the impacts of the land demands for truck traffic and truck parking related to goods movement and drayage.

RECOMMENDATION 21:

That the Port of Vancouver continue to work with Metro Vancouver and TransLink to optimize port-related land uses and container drayage.

RECOMMENDATION 22:

That the Greater Vancouver Gateway Council continue its efforts to attract grants and other funding, and leverage their success towards improved infrastructure linkages and capital investments that support regional and local policy goals.

RECOMMENDATION 23:

That the Greater Vancouver Urban Freight Council continue its efforts to coordinate the implementation of the Regional Goods Movement Strategy between its member organizations.

RECOMMENDATION 24:

That regional organizations and stakeholders continue to investigate and implement options designed to reduce the environmental impacts related to the transportation of goods and people in the region, through their respective plans and policies.

This includes:

- the Province's efforts to improve standards such as the Renewable & Low Carbon Fuel Requirements Regulation and improved permitting and tracking to help reduce GHG emissions from the commercial trucking industry;
- Metro Vancouver's efforts towards protecting and improving air quality while ensuring infrastructure, ecosystems and communities are resilient to the impacts of climate change through Climate 2050;
- the Port of Vancouver's efforts to optimize and reduce the carbon impact of its operations;
- TransLink's efforts to optimize goods movement and public transit options through the Regional Goods Movement Strategy and Transport 2050; and
- local municipal efforts to expand access to charging station infrastructure for electric vehicles.

Ensure a Coordinated Approach

Improved cooperation and data tracking between governments and other organizations will be necessary to ensure accurate monitoring and effective land management into the future, while also guiding future coordination and alignment in terms of policy responses to issues as they arise in Metro Vancouver and across southwestern British Columbia.

Improve Data and Monitoring

A challenge for the effective planning and management of industrial lands is timely access to quality data. This is true for both land management and the development of effective economic policy. However, many of the data sources available to the region are prepared infrequently, such as the Census and Industrial Lands Inventory Update, or are not fine-grained enough to provide useful guidance at the local level, such as the Labour Force Survey. Through the development of more effective data collection frameworks and sharing agreements, the region and its partners would be able to greatly improve the quality of data used to inform policy decisions. The following four recommendations are focused on data provision.

RECOMMENDATION 25:

That Metro Vancouver produce an annual report that summarizes changes to the Industrial Lands Inventory, while continuing to publish a comprehensive Regional Industrial Land Inventory every five years.

RECOMMENDATION 26:

That municipalities provide a summary report of local development activity on lands in the Metro Vancouver Industrial Lands Inventory as part of their regional Development Cost Charge reporting, once per year.

RECOMMENDATION 27:

That Metro Vancouver, in collaboration with member jurisdictions, conduct a regional employment survey on a bi-annual basis.

RECOMMENDATION 28:

That major regional industrial land users and organizations, such as the Port of Vancouver, Vancouver Airport Authority, NAIOP Commercial Real Estate Development Association and other relevant stakeholders, consider expanding data sharing partnerships for the purpose of improving economic development and infrastructure investment, guiding land use and goods movement planning, and informing the development of associated policies.

Encourage Growth and Investment through Regional Economic Coordination

Both Metro Vancouver and member jurisdictions have a shared interest in attracting new investment and supporting the retention and expansion of economic activity across the region. In the past, cooperation and collaboration between the economic development initiatives of member jurisdictions has occurred on an ad hoc basis.

Metro Vancouver's new Regional Economic Prosperity Service represents the adoption of a collaborative regional approach to attracting investment to the region. With a focus on regional collaboration to advance shared economic, livability, and sustainability goals, the new service will work closely with member jurisdictions, provincial and federal governments, and other stakeholders to attract new investment in the region - investment that will both increase the number of well-paying, high quality jobs and generate new tax revenues.

RECOMMENDATION 29:

That Metro Vancouver, in consultation with member jurisdictions, First Nations and relevant stakeholder organizations, document and promote the region's value proposition from an industrial economy perspective.

RECOMMENDATION 30:

That Metro Vancouver seek to enhance collaboration across the region to encourage economic growth and diversity, including on industrial lands.

RECOMMENDATION 31:

That Metro Vancouver assist member jurisdictions seeking to develop and update their own local bring-to-market strategies by providing data and research support in line with its mandate.

Coordinate with Neighbouring Regions

Pressures on industrial lands are not limited to Metro Vancouver. Adjacent regional districts are part of the same goods movement network and commuter-shed and they are facing similar land pressures and challenges. However, with these challenges also comes the possibility of shared opportunity. The broader economic region of southwestern British Columbia presents opportunities for collaborative planning that reaches beyond Metro Vancouver's borders, building on shared infrastructure linkages to expand upon the role as an economic gateway, while also creating new opportunities for growth.

RECOMMENDATION 32:

That the Province develop a framework for economic and land use planning coordination between neighbouring regions in the broader southwestern BC economic region to support industrial land use and protection.

The components and benefits of the framework could include the following:

- providing a Lower Mainland-wide view for industrial land management, infrastructure investment and other economic development opportunities between Metro Vancouver and the Fraser Valley Regional District;
- identifying options to expand the Lower Mainland's economic linkages with trade-oriented and industrial uses along major highway and rail corridors in the Squamish-Lillooet Regional District, Thompson-Nicola Regional District, and Regional District of Okanagan-Similkameen; and
- identifying opportunities for short sea shipping and maritime linkages with other port facilities located on Vancouver Island and along the Georgia Strait.

RECOMMENDATION 33:

That the Metro Vancouver Regional District and the Fraser Valley Regional District develop and sign a memorandum of understanding that outlines their shared priorities regarding and commitment to the effective and efficient management, protection, and development of industrial lands within the Lower Mainland.

RECOMMENDATION 34:

That the federal government, in implementing the Port's Modernization Review, take a broader provincial perspective for ports in British Columbia, in part to alleviate land pressure in the Lower Mainland.

THE 4 'BIG MOVES' AND 10 PRIORITY ACTIONS

The challenges facing Metro Vancouver's industrial lands are complex and interconnected, and no single action or stakeholder will resolve them alone. Market forces and types of industrial activity vary significantly by sub-region, such that for example, forms of densification that may be possible in certain urban locations may not be appropriate in other locations. There are a wide range of national, regional and local serving industrial activities in the region, which have different site and location needs, and which, as a result require different policy responses. As a result, recommended actions need to balance regional objectives, while addressing local contexts – 'one size' does not fit all.

Successfully achieving the vision of the Regional Industrial Lands Strategy will require a collaborative approach and sustained effort on the part of various governing bodies and stakeholders with overlapping, yet distinct areas of business and jurisdictions in the region. Each of the identified actions will take time and resources to realize their full potential, and while some recommendations can be implemented in the near term, others may take additional time to build capacity before implementation. The key to success will be to act in partnership, setting early priorities with clearly defined roles.

Success is defined as a prosperous regional economy, full workforce employment, and an efficient transportation system, achieved through an adequate supply and location, as well as use and utilization, of industrial lands that can house a wide variety of industrial functions that support both the regional as well as national economies.

From the list of 34 recommendations, a short-list of 10 priority actions is identified organized within the 4 Big Moves. These priority actions are formed, in part, by packaging together related actions in the longer list into combined shorter-term actions, to be implemented over the first few years of endorsement of the Regional Industrial Lands Strategy.

This priority list reflects the immediacy of specific pressures facing the region's industrial land supply, along with stakeholder feedback gathered through the development of the Strategy. The actions have been identified as having an optimum balance of: short term readiness, a wide level of support, a logical sequence to feed into the subsequent steps of the implementation program, and a direct relevance to inform the update of the regional growth strategy.

Due to the complexity of the issues and areas of overlapping jurisdiction among partner organizations, lead roles are identified for implementation with respect to the core mandates of each partner (see Appendix 1).

Together, these priority actions will work to ensure the region's industrial lands continue to provide an attractive and viable location for industrial businesses to locate, grow and prosper, while supporting the broader regional economy and community. Implementation is expected to be an iterative and ongoing process that will need to be monitored and adapted over time. As recommendations are implemented, new issues will emerge and new priorities may need to be adjusted and the roles of partner organizations may need to be reshuffled. As new challenges arise and new information becomes available, new actions will inevitably need to be identified and prioritized, which may augment or replace other short-listed recommendations.

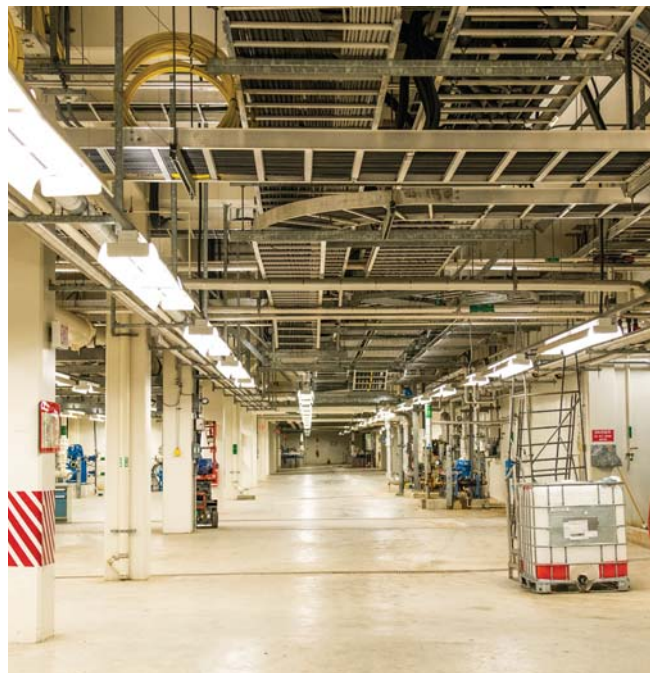
The 4 Big Moves:

- Protect Remaining Industrial Lands
- Intensify and Optimize Industrial Lands
- Bring the Existing Land Supply to Market & Address Site Issues
- Ensure a Coordinated Approach

Big Move 1 - Protect Remaining Industrial Lands

Given the ongoing and projected demand for industrial land in the region, it is imperative to protect the region's remaining industrial lands and curb the threats that undermine their use for industrial activities. This means lands for trade-oriented purposes as well as for small, local serving businesses; businesses that are responding to the changing nature of industry as well as more traditional industrial activities. The priority actions for this Big Move are:

1. **Define Trade-Oriented Lands:** Trade-oriented lands are large sites associated with the transportation of goods to and through the region, such as by rail and the port, which serve a national function and are crucial to the region's economy, warranting additional attention and possible protection. A clear, consistent and collaboratively developed definition and understanding of the extent and location of these important lands will support their protection. Metro Vancouver will work with member jurisdictions to establish a definition for trade-oriented lands. (Subsequent actions identified in the 34 recommendations include consideration of a regional trade-oriented land use overlay in the regional growth strategy and municipal consideration of trade-oriented zoning.)
2. **Undertake a Regional Land Use Assessment:** Given Metro Vancouver's constrained land base, there is significant pressure for competing, legitimate land uses across the region. Metro Vancouver will undertake a targeted or region-wide assessment of land use, looking at and beyond the existing policy framework to proactively identify the 'best' locations for different land uses based on a collaboratively developed set of criteria.
3. **Strengthen Regional Policy:** Through the update of the regional growth strategy, Metro Vancouver will explore implementation changes, such as a consistent definition for Industrial, higher voting thresholds to amend the regional Industrial land use designation, and exploration of 'no net loss' as part of the amendment criteria.
4. **Seek Greater Consistency in Local Government Zoning Definitions and Permitted Uses:** Metro Vancouver will, in collaboration with member jurisdictions and other regional agencies, develop a consistent definition of 'industrial' and guidelines for permitted uses. Member jurisdictions, through regional context statement updates, will review and update their zoning bylaws using the established guidelines.



Big Move 2 - Intensify and Optimize Industrial Lands

Given the region's constrained land base, it is critical to encourage the most efficient use of the remaining land supply for all types of industrial users, to remove barriers to the intensification of industrial land, and investigate opportunities to optimize the location of certain land uses over time. The priority action for this Big Move is:

5. **Facilitate the Intensification / Densification of Industrial Forms Where Possible:** Municipalities will review and remove unnecessary restrictions to density or height limits where appropriate; plan space to accommodate new, smaller industrial uses when older, centrally located

industrial areas densify; allow mixing of industrial with other employment uses so long as the industrial component is secured as a condition of redevelopment; and explore opportunities to encourage intensification / densification in target areas. Metro Vancouver will explore allowing mixed-use with residential on Mixed Employment lands immediately proximate to Rail Rapid Transit Stations (distance to be determined, but suggest 200 metres) as long as existing industrial space is maintained or expanded and other *Metro 2040* objectives are met (e.g. affordable, rental housing).



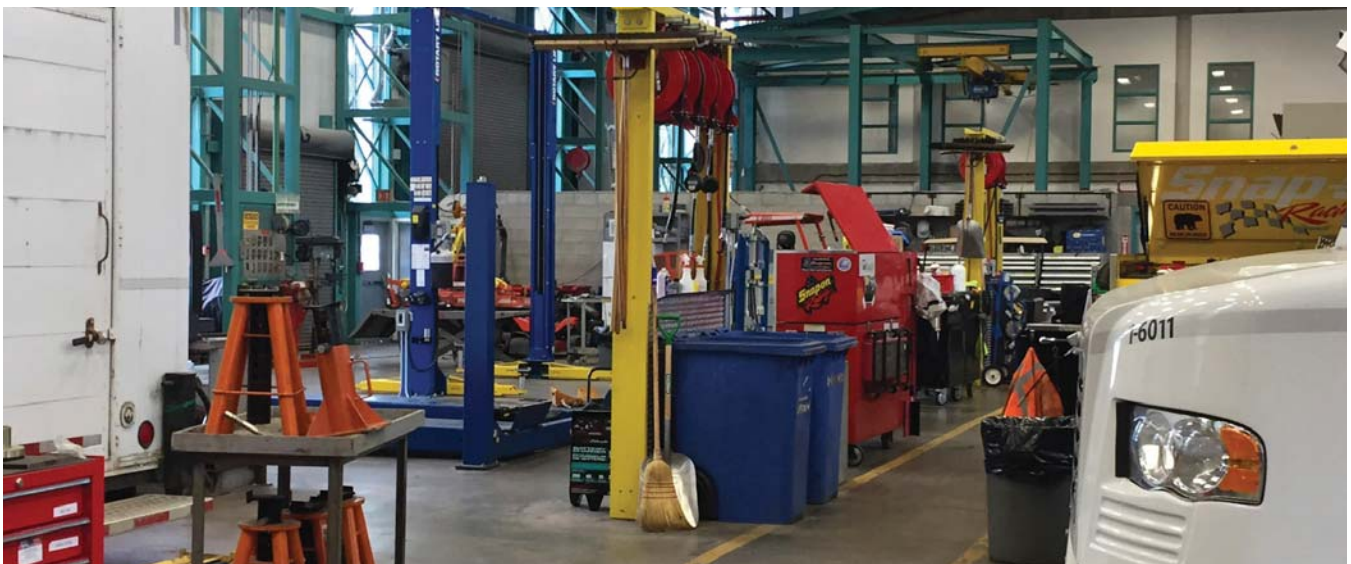
Big Move 3 - Bring the Existing Land Supply to Market & Address Site Issues

Certain parts of the vacant industrial land supply suffer from site-specific challenges, such as limited infrastructure support, environmental concerns, and under-sized parcels. Where vacant lands have not come to the market, local municipalities may benefit from the preparation of a bring-to-market strategy. Such a strategy would identify the issues that have prevented the development of the undeveloped or under-developed industrial land, while providing a roadmap to achieve the municipality's economic goals for its remaining industrial lands. The priority actions for this Big Move are:

6. **Prepare Bring-to-Market Strategies for Vacant or Under-developed Industrial Lands:** Municipalities with vacant or under-developed industrial lands will prepare a bring-to-market strategy for their industrial land that addresses strengths, weaknesses, opportunities and challenges, to encourage reinvestment and more intensive use, considers municipal assembly and consolidation

of fragmented parcels, whether environmental remediation is required, and if local servicing and infrastructure improvements are required and can be accommodated through front-ending infrastructure investment agreements.

7. **Ensure Transportation Connectivity:** The efficient movement of goods and people is critical for many industrial businesses, both local serving and trade-oriented, in terms of where they choose to locate. The maintenance and growth of a resilient and reliable transportation network, including various modes of transit, is an integral component in planning for the effective management of the region's industrial lands. TransLink, Metro Vancouver, the Port, the Airport and municipalities will continue to work together to coordinate investments in the transportation network, implement the Regional Goods Movement Strategy, enhance the regional truck route network, support efficient container drayage and provide transit for industrial workers.



Big Move 4 - Ensure a Coordinated Approach

Improved cooperation and data tracking among governments and other agencies and organizations is necessary to ensure effective land management and accurate monitoring into the future. Coordination also guides future alignment of policy responses to issues as they arise across Metro Vancouver and southwestern British Columbia. The priority actions for this Big Move are:

8. **Coordinate Strategies for Economic Growth and Investment:** Metro Vancouver and member jurisdictions have a shared interest in attracting new investment and supporting economic and employment activities across the region. Many municipalities have an economic development office or department that work to retain and support the expansion of local businesses. Metro Vancouver's new Regional Economic Prosperity Service provides the opportunity for a regional approach to economic development that will amplify and complement the work of member jurisdictions.
9. **Improve Data and Monitoring:** Timely access to quality data is critical for land management and the development of effective economic policy. Metro Vancouver will update the Industrial Lands Inventory to have a better understanding of the current land uses and supply, and complete a Regional Employment Survey.

10. **Develop a Framework for Coordination:** Pressures on industrial lands are not limited to Metro Vancouver. Adjacent regional districts are part of the same goods movement network and commuter-shed and they are facing similar land pressures and challenges. The Province will work collaboratively with Metro Vancouver, TransLink, and municipalities on cross-boundary economic and land use planning matters and will develop a framework for economic and land use planning coordination between neighbouring regions in the broader southwestern BC economic region to support industrial land use and protection. For example, Metro Vancouver will seek to sign a memorandum of understanding with the Fraser Valley Regional District outlining shared priorities regarding the effective and efficient management, protection, and development of industrial lands within the Lower Mainland.



APPENDIX 1 – REGIONAL INDUSTRIAL LANDS STAKEHOLDERS

Many different agencies and organizations play a key role in the management, development, and use of industrial lands in the Metro Vancouver region. Each stakeholder has a different role, different responsibilities, and different authority over the rules, guidelines and decision making as it relates to industrial land and its use.

	ORGANIZATION	PURPOSE/MANDATE	ROLES AND RESPONSIBILITIES RELATED TO INDUSTRIAL LANDS
LOCAL	Member Jurisdictions (21 municipalities, 1 Treaty First Nation, 1 Electoral Area)	Represent the interests and respond to the different needs and changing circumstances of their communities, residents and businesses	<ul style="list-style-type: none"> • Designates permitted land uses and regulates the density and form of development through local zoning by-laws, neighbourhood plans, and Official Community Plans • Builds and maintains streets, sidewalks, and local infrastructure • Regulates traffic, use of streets (including on-street parking) and the size and weight of vehicles that are permitted to travel on municipal streets • Sets municipal property tax rates for industrial and commercial uses
REGIONAL	Metro Vancouver	Delivers regional services, policy and political leadership on behalf of 23 member jurisdictions	<ul style="list-style-type: none"> • Supports coordinated land use planning around common framework for regional growth management • Communicates value of industrial land uses and improved regional transportation network, including efficient goods movement, as components of broader economic ecosystem
	TransLink: South Coast British Columbia Transportation Authority (SCBCTA)	<p>Transportation authority for Metro Vancouver region</p> <p>Mandate to provide a regional transportation system that moves people and goods and supports the regional growth strategy and regional and provincial environmental and economic objectives</p>	<ul style="list-style-type: none"> • Operates integrated regional transit system • Owns and operates 5 bridges • Together with municipalities, co-funds and co-manages the Major Road Network (MRN) • Leads and facilitates regional goods movement research and planning

	ORGANIZATION	PURPOSE/MANDATE	ROLES AND RESPONSIBILITIES RELATED TO INDUSTRIAL LANDS
PROVINCIAL	Government of British Columbia	<p>Authority to create local governments and their governing legislation</p> <p>Authority over Provincial Highways, roads and other infrastructure</p> <p>Coordinates high level economic development issues of provincial interest</p> <p>Authority to identify and legislate use for specific lands</p>	<ul style="list-style-type: none"> • Provincial legislation, such as the Local Government Act • Provincial transportation planning and policy • Administers a number of acts related to transportation and goods movement, including the Motor Vehicle Act and the Commercial Transport Act • Highway construction and maintenance • Commercial vehicle safety and enforcement • Port and airport development • Infrastructure grants • Major capital project management • Provincial emergency management
	Agricultural Land Commission	<p>Preservation of agricultural land</p> <p>Encourage local governments, First Nations, and others to enable and accommodate farm use of agricultural land and uses compatible with agriculture in their plans, by-laws and policies</p>	<ul style="list-style-type: none"> • Sets regulations for determining which business activities are permitted within the Agricultural Land Reserve • Determines the scope and scale at which agri-industrial businesses are allowed to locate within the ALR before having to relocate to industrial lands in the urban containment boundary
FEDERAL	Government of Canada	<p>Promotes safe, secure, efficient and environmentally-responsible transportation and trade-oriented land uses in Canada</p>	<ul style="list-style-type: none"> • Administers a number of Acts related to transportation, including vehicle requirements on new equipment pursuant to the Canada Motor Vehicle Safety Standards (CMVSS), engine emission standards, and rail safety standards • Provides funding to help improve major infrastructure systems, including Asia Pacific Gateway initiatives • Works with its portfolio partners, other government departments and jurisdictions and industry to ensure that trade-related networks and transportation systems work well • 18 Port authorities fall under the federal portfolio across Canada

	ORGANIZATION	PURPOSE/MANDATE	ROLES AND RESPONSIBILITIES RELATED TO INDUSTRIAL LANDS
FEDERAL	Port of Vancouver	Responsible for the stewardship of federal port lands in and around the region	<ul style="list-style-type: none"> Oversees transportation operation in collaboration with terminal operators, railroads and shippers to ensure efficient goods movement on port lands and waters Acquires and develops lands to support current and anticipated port-related operations
	Vancouver Airport Authority	Oversees Vancouver International Airport's operations	<ul style="list-style-type: none"> Develops and maintains airport infrastructure and oversees day-to-day operations at Vancouver International Airport Plays active role in the development and growth of air cargo and the Asia Pacific Gateway
NON-PROFIT INDUSTRY AND BUSINESS ASSOCIATIONS	Greater Vancouver Gateway Council	Collaborate to establish a globally competitive Pacific Gateway in trade and travel between North America and the Asia Pacific economies	<ul style="list-style-type: none"> Advocates for members' interests Conducts and publish research studies Provides economic impact data Supports and coordinate application for infrastructure grants
	Greater Vancouver Urban Freight Council	Champion the implementation of the priority actions identified in the Regional Goods Movement Strategy (RGMS)	<ul style="list-style-type: none"> Advocates for members' interests Coordinates the implementation of policy recommendations related to goods movement in the region Monitors and assesses the progress of RGMS implementation Periodically reviews current priorities against the region's evolving goods movement needs
	BC Trucking Association (BCTA)	Province-wide, non-partisan, non-profit motor carrier association formed to advance the interests of British Columbia motor carriers and associated industrial land owners	<ul style="list-style-type: none"> Promotes prosperous, safe, efficient and responsible commercial road transportation industry Conducts research Advocates for member interests

	ORGANIZATION	PURPOSE/MANDATE	ROLES AND RESPONSIBILITIES RELATED TO INDUSTRIAL LANDS
NON-PROFIT INDUSTRY AND BUSINESS ASSOCIATIONS	NAIOP Commercial Real Estate Development Association	Non-partisan, non-profit business association formed to advance the interest of commercial land users, developers and real estate industry	<ul style="list-style-type: none"> • Conducts research • Provides education on commercial and industrial development and real estate related issues • Advocates for member interests
	Boards of Trade and Chambers of Commerce	Non-partisan, non-profit business associations formed to advance the interest of their members	<ul style="list-style-type: none"> • Conducts research • Advocates for member interests
	Fraser Basin Council	A charitable non-profit society committed to advancing sustainability in the Fraser Basin and across British Columbia	<ul style="list-style-type: none"> • Conducts and publishes research related to environmental and economic sustainability • Preparation of BC Regional Adaptation Collaborative to strengthen regional capacity and increase action to advance adaption planning and implementation related to climate change impacts, including coastal flooding
PRIVATE SECTOR	Railways	Delivers returns on investments by providing fast and reliable transportation for rail and intermodal customers	<ul style="list-style-type: none"> • Provides rail and intermodal services to customers consistent with federal Rail Transportation Acts • Coordinates with local governments on adjacent land use and road networks
	Goods Movers	Deliver returns on investments by providing fast and reliable transportation for their customers	<ul style="list-style-type: none"> • Provides road delivery services to customers consistent with federal, provincial and municipal regulations
	Business	Deliver a return on investment for shareholders or owners	<ul style="list-style-type: none"> • Consumes and produces goods and services to support customers and other businesses • Makes locational, transportation, investment, real estate, and scheduling decisions to advance business operations

APPENDIX 2 – INDUSTRIAL MARKET CONDITIONS

Industrial land values and lease rates across Metro Vancouver have increased significantly between 2014 and 2019. These changes reflect the scarcity of large, vacant, developable industrial land parcels, speculation that densities will increase on industrial lands, increased competition from other uses, or outright conversion to non-industrial uses.

METRO VANCOUVER INDUSTRIAL LAND VALUES AND LEASE RATES, 2014, 2017, 2019

	LATE 2014	LATE 2017	EARLY 2019
Average Vacant Industrial Land Value (\$million/acre)	\$1.1 million to \$1.6 million	\$1.9 million to \$3.0 million	\$2.0 million to \$4.0 million
Average Rental Rate for Industrial Space (\$/net square foot per year)	\$8	\$10	\$12

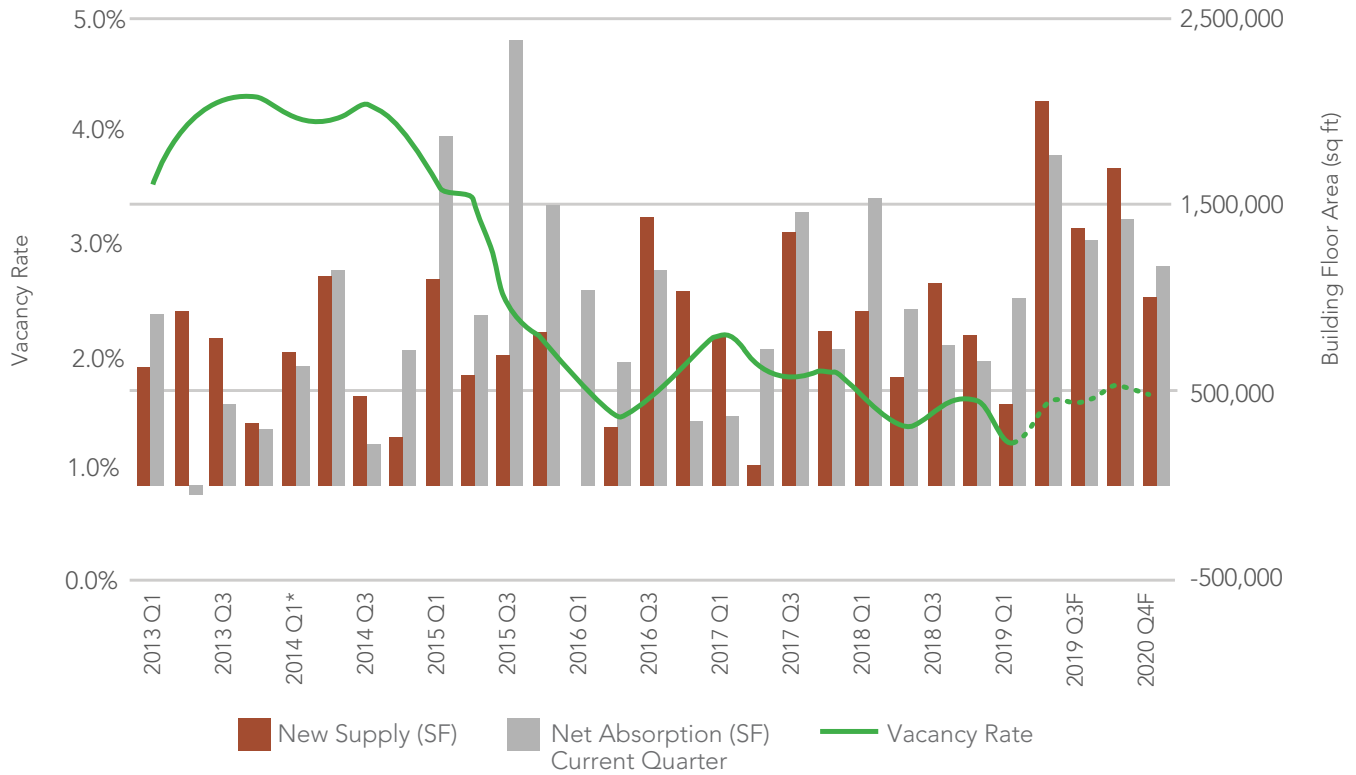
SOURCE: ADAPTED FROM THE CHANGING NATURE OF INDUSTRY AND INDUSTRIAL DEMAND IN METRO VANCOUVER: DISCUSSION PAPER, SECTION 4.7, 2019

NOTE: FIGURES REPRESENT AVERAGE VALUES FOR THE REGION AS COMPILED BY MAJOR BROKERAGES. INDUSTRIAL TRANSACTIONS IN SPECIFIC SUB-MARKETS MAY BE GREATER OR LESS THAN RANGE SHOWN.

These land price and lease rate increases are the result of changing dynamics between the supply and demand of industrial land and space availability. In the case of Metro Vancouver, demand and competition for these lands has been steadily increasing, while the supply of available industrial land has declined. Demand for industrial space has far outpaced how quickly developers have been able to bring new buildings to the market, resulting in a regional vacancy rate that has averaged below 2% for the past 4 years.

The industrial market has remained extremely tight over this period, with the lack of supply and strong demand putting upward pressure on lease rates and land values.

METRO VANCOUVER INDUSTRIAL MARKET ABSORPTION TRENDS AND FORECAST, 2013-2020



SOURCE: COLLIERS INTERNATIONAL, METRO VANCOUVER INDUSTRIAL MARKET: Q1 2019

APPENDIX 3 – INDUSTRIAL LANDS INVENTORY AND ABSORPTION

The preliminary update to Metro Vancouver's 2015 Industrial Lands Inventory indicates significant absorption of lands since 2015, including many of the last remaining larger well-configured sites. While the remaining supply of about 1,050 hectares (2,600 acres) of truly vacant land and another 750 ha (1,860 acres) of non-industrial land uses that may eventually be developed for industrial, may seem significant, much of it is lands that are difficult to develop, fragmented and require assembly to bring to market, otherwise encumbered or whose owners are disinterested in development. As more of the unencumbered available sites are developed, an increasing proportion of that supply will be the more "difficult" or unavailable sites.

Typically, full development of an industrial land supply is about a remaining 5% long-term land vacancy or 95% development; in Metro Vancouver's case the long-term vacancy would be about 570 ha of an 11,330 ha developed and vacant supply. Assuming there is no future conversion of industrial land to non-industrial uses, the effective available supply for future development would be about 1,430 ha.

The constrained land supply in Metro Vancouver means that there are two perspectives that can be taken to the demand forecast:

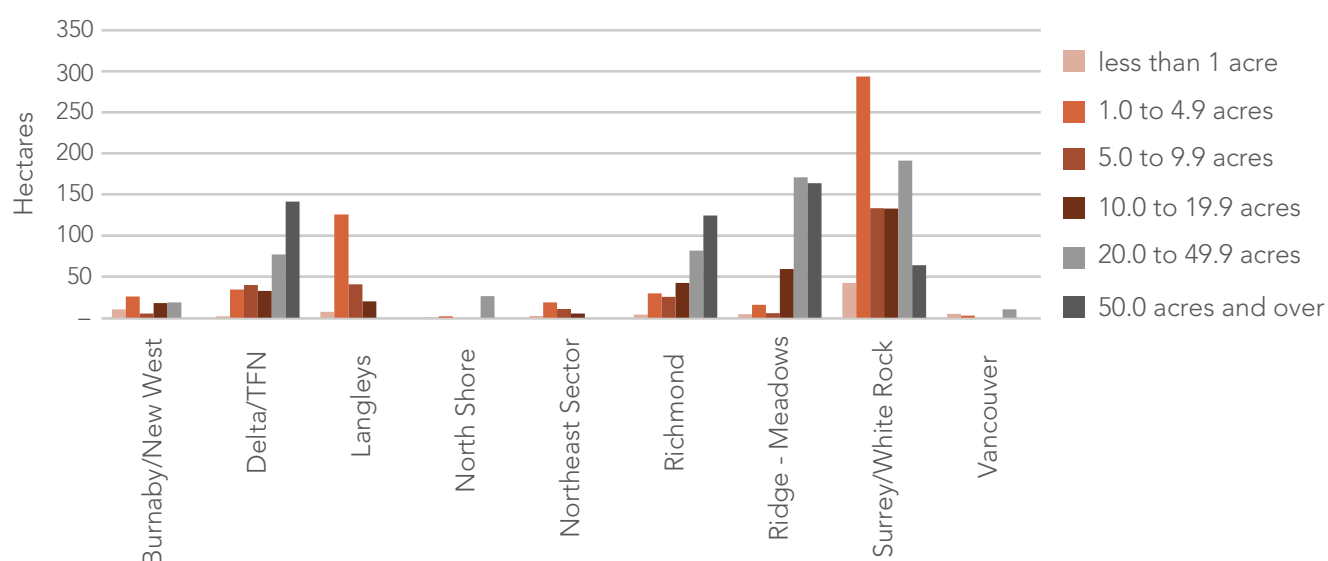
- What would the land demand be if there were no significant supply constraints in the region?
- Given the supply constraints, what is the demand for land that can be satisfied in the market and when will the available long-term supply near 95% full development?

There are two primary ways of forecasting land demand, both of which are appropriate ways to forecast and each can be used as a check on the other approach:

- The absorption method looks at the recent history of land absorption and projects the absorption forward for the forecast period. Adjustments to the forecast rate of absorption, up or down, are made to reflect expectations for a changing proportion of economic activity occurring on industrial land and higher or lower levels of overall growth expected, compared to the past.
- The employment-based method starts with a forecast of total employment and the relative growth by economic sector. The proportion of growth within sectors that may be accommodated on industrial lands provides an employment forecast for industrial areas. Applying an anticipated floor space per worker (in m² per employee) or an employment density (in employees per net ha) provides building space and land areas.

For larger sites (20+ acres / 8+ hectares) that point has already been reached in Metro Vancouver. To meet the demand forecast to 2050, an additional 800 to 1,600 ha (2,000 to 4,000 acres) would need to be added to current vacant land inventory.

VACANT LANDS BY SITE SIZE AND SUB-REGION, METRO VANCOUVER, 2015



SOURCE: METRO VANCOUVER 2015 INDUSTRIAL LANDS INVENTORY: TECHNICAL REPORT

As part of the Regional Industrial Lands Strategy work, Hemson Consulting looked at industrial land demand from an employment growth perspective. The forecast is based on a low, medium and high range of Metro Vancouver employment growth from the current base. Following a long-standing pattern of economic change, a continued shift in the employment base of Metro Vancouver away from those sectors occupying industrial land towards service sector uses is assumed to continue. Under this assumption, 20% of the region's job growth would be accommodated on industrial lands through forecast period to 2050 (from 27% of

total regional employment in the 2016 base year), with approximately 5% occurring within already developed industrial lands via intensification. Assuming an employment density of 35 jobs per net ha, the resulting industrial land demand to accommodate this growth would be 70 to 100 ha per year (175 to 250 acres per year). Similarly, an additional 650 to 1,550 ha (1,600 to 3,800 acres) of land on top of existing supply would be a reasonable estimate of the lands needed to satisfy this demand.



EXECUTIVE SUMMARY

Regional Industrial Lands Strategy

KEY STATS

- Industrial lands comprise **4%** of the region's land base, and accommodate **27%** of the region's jobs.
- Industrial land prices in Metro Vancouver are amongst the **highest** in North America.
- Industrial jobs pay **10% higher** than the regional average.
- Business activity on industrial lands generate **30%** of the region's total Gross Domestic Product (GDP).

THE CHALLENGE

The Metro Vancouver region is experiencing a critical shortage of industrial land. Continued population and employment growth in a constrained geography have contributed to the challenges facing Metro Vancouver's industrial land supply. With strong demand for industrial space, many industrial businesses are finding it increasingly difficult to find suitable space to operate in this region.

The main challenges facing Metro Vancouver's industrial lands are:

1. A Constrained Land Supply
2. Pressures on Industrial Lands
3. Site and Adjacency Issues
4. A Complex Jurisdictional Environment

VISION

The Regional Industrial Lands Strategy, approved by the Metro Vancouver Board on July 3, 2020, seeks to:

Ensure sufficient industrial lands to meet the needs of a growing and evolving regional economy to the year 2050.



THE RESPONSE

In response to the challenges facing industrial lands and the interests of stakeholders in the region, Metro Vancouver, with the support of the Industrial Lands Strategy Task Force, prepared the Regional Industrial Lands Strategy. The Strategy includes **34 recommendations** with **10 priority actions**, organized around **4 Big Moves**:

Big Move 1 - Protect Remaining Industrial Lands

Given the ongoing and projected demand for industrial land in the region, it is imperative to protect the region's remaining industrial lands and curb the threats that undermine their use for industrial activities. This means lands for trade-oriented purposes as well as for small, local serving businesses; businesses that are responding to the changing nature of industry as well as more traditional industrial activities.

Big Move 3 - Bring the Existing Land Supply to Market & Address Site Issues

Certain parts of the vacant industrial land supply suffer from site-specific challenges, such as limited infrastructure support, environmental concerns, and under-sized parcels. Where vacant lands have not come to the market, local municipalities may benefit from the preparation of a bring-to-market strategy. Such a strategy would identify the issues that have prevented the development of the undeveloped or under-developed industrial land, while providing a roadmap to achieve the municipality's economic goals for its remaining industrial lands.

Big Move 2 - Intensify and Optimize Industrial Lands

Given the region's constrained land base, it is critical to encourage the most efficient use of the remaining land supply for all types of industrial users, to remove barriers to the intensification of industrial land, and investigate opportunities to optimize the location of certain land uses over time.

Big Move 4 - Ensure a Coordinated Approach

Improved cooperation and data tracking among governments and other agencies and organizations is necessary to ensure effective land management and accurate monitoring into the future. Coordination also guides future alignment of policy responses to issues as they arise across Metro Vancouver and southwestern British Columbia.



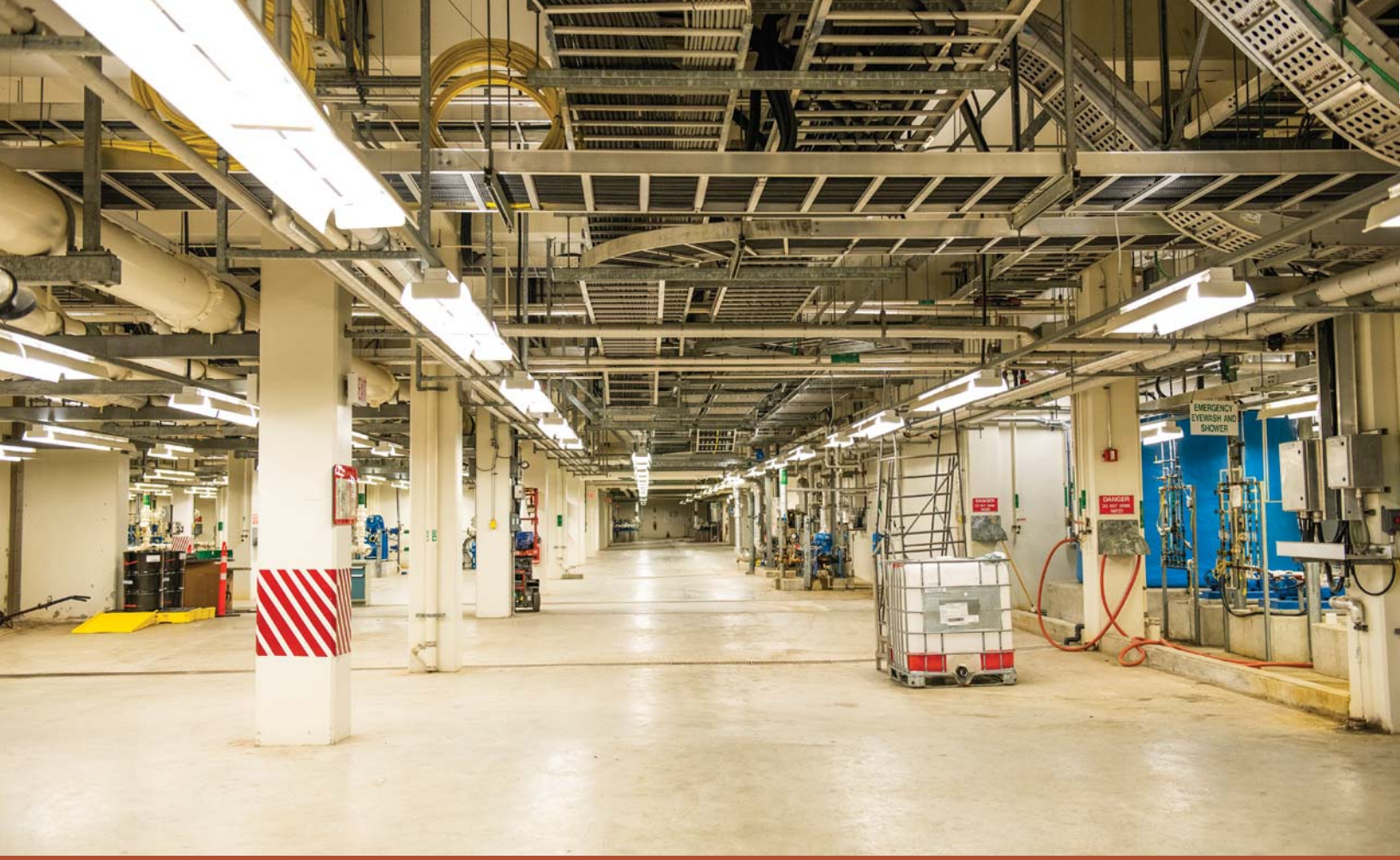
PRIORITY ACTIONS

The Strategy's 10 priority actions for early implementation are:

- 1. Define 'Trade-Oriented' Lands** – These large sites associated with the transportation of goods to and through the region serve a national function and are crucial to the economy. A clear, consistent and collaboratively-developed definition will support their protection.
- 2. Undertake a Regional Land Use Assessment** – Proactively, in collaboration with municipalities, identify the 'best' locations for different types of land uses based on a set of criteria.
- 3. Strengthen Regional Policy** – In *Metro 2040*, the regional growth strategy, explore stronger policy measures such as higher voting thresholds to amend the regional Industrial land use designation.
- 4. Seek Greater Consistency in Local Government Zoning Definitions and Permitted Uses** – Collectively develop consistent definitions for permitted industrial uses on Industrial lands and seek implementation through municipal plans and bylaws.
- 5. Facilitate the Intensification / Densification of Industrial Forms Where Possible** – Promote multi-level industrial buildings or other development forms, by removing regulatory barriers like zoning height and density limits to encourage a more efficient use of the limited land supply; also consider mixed-use on Mixed Employment lands near rapid transit stations.
- 6. Prepare Bring-to-Market Strategies for Vacant or Under-Developed Industrial Lands** – Proactively address issues preventing the development of vacant or under-utilized industrial lands, which may have unique site challenges, such as servicing limitations, soil qualities, and ownership assembly.
- 7. Ensure Transportation Connectivity** – Critical for industrial businesses, work together to coordinate investment in the transportation network, implement the Regional Goods Movement Strategy, enhance the regional truck route network, and promote efficient container drayage and transit for industrial workers.
- 8. Coordinate Strategies for Economic Growth and Investment** – Profile the importance of industrial lands for the economy, and link with municipal economic development objectives and the Metro Vancouver Regional Economic Prosperity Service, to attract investment to the region.
- 9. Improve Data and Monitoring** – Update the Metro Vancouver Regional Industrial Lands Inventory to have a better shared understanding of the current land uses and supply, and conduct a Regional Employment Survey.
- 10. Develop a Framework for Coordination** – For cross-boundary economic and land use planning matters, work with the adjacent regional districts and the Province to advance coordinated infrastructure investments, land use planning, and economic development.

Taken together, the actions in the Regional Industrial Lands Strategy will help to ensure Metro Vancouver's industrial lands continue to provide an attractive and viable location for industrial businesses to locate, grow and prosper, while supporting the broader economy and community.





INDUSTRIAL LANDS - FUNCTION AND DEFINITION

Industrial lands are crucial to supporting a prosperous and sustainable economy, and have important linkages with employment, transportation, and taxation matters in this region. Many activities on industrial lands provide for the day-to-day needs of Metro Vancouver's residents and businesses, and contribute to a diversified regional economy.

'Industrial' represents a wide spectrum of uses and intensities, ranging from large distribution and transportation lands, warehouses, manufacturing and processing facilities, to small local-serving production and suppliers, as well as new, emerging technology-driven businesses with integrated work spaces, which all need different types of accommodations and locations to support their operations. Many regional jobs and businesses are also connected to national / international trade through the port and airport, which require industrial lands for the handling of goods.

ACKNOWLEDGEMENTS

Metro Vancouver would like to thank all participants who contributed to the development of the Regional Industrial Lands Strategy.

The Industrial Lands Strategy Task Force provided core input, direction and guidance. Representatives included:

Appointed elected officials from Metro Vancouver

BC Ministry of Jobs, Trade and Technology

Port of Vancouver

TransLink

BC Chamber of Commerce

Agricultural Land Commission

Urban Development Institute

Beedie Group

Value Property Group

FOR MORE INFORMATION

metrovancover.org

(search 'Regional Industrial Lands Strategy')



metrovancover
SERVICES AND SOLUTIONS FOR A LIVABLE REGION

Summary of Regional Industrial Lands Strategy Recommendations and their Application to the City of North Vancouver

Recommendations (Actions)	Intent	Application to CNV
1. Undertake a Regional Land Use Assessment	Identify, based on a defined set of criteria and cross-jurisdictional considerations, opportunities for more optimized locations and uses of land in order to support regional and local policy objectives and to inform policy changes.	CNV would participate as required
2. Strengthen Regional Policy (RGS)	<ul style="list-style-type: none"> • Increase the voting threshold required for a minor amendments; • Explore 'no net loss of land' as part of the amendment; • Clarify the definitions and permitted uses on the Industrial and Mixed Employment regional land use designations; and, • Explore permission of mixed-use on Mixed Employment lands immediately proximate to Rail Rapid Transit Stations as long as existing industrial space is maintained or expanded and other RGS objectives are met. 	<p>Given CNV's dense, mixed-use environment, this could have ramifications in the future should there be a desire to transition from industrial and employment designations to other designations.</p> <p>Providing there is flexibility within the RGS policies related to proximity with Frequent Transit Networks and the ability to meet other RGS objectives, these changes should be manageable.</p>
3. Define 'Industrial' Use and Create Guidelines for Primary and secondary uses	<ul style="list-style-type: none"> • Create a consistent definition of 'industrial' uses • Create guidelines for which primary and secondary (or ancillary) land uses should be permitted in Industrial and Mixed Employment designated areas 	As part of the Regional Context Statement update, CNV would commit to review and potentially update the Zoning Bylaw using the guidelines as a resource to amend the permitted uses in the local industrial and employment lands zones.
4. Define Trade-Oriented Lands and Develop an Overlay	<ul style="list-style-type: none"> • Create a clear definition of Trade-Oriented Lands. • Develop a Trade-oriented land use Overlay in the RGS. 	<p>CNV would participate as required.</p> <p>CNV would seek to ensure that the overlay does not eliminate the opportunity to intensify adjacent lands, where appropriate. In addition, the overlay should not negatively impact the desired street character designations near or within the overlay, particularly as it relates to trucking routes.</p>
5. Consider Trade-Oriented Zoning	Consider zoning that would limit or restrict non-industrial uses and the fragmentation of parcels with an aim to protect these lands for trade-oriented activity.	CNV would be judicious in applying any new zoning so as not to limit flexibility and future use that this zoning could place on these lands.

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6. Define Permitted Tenures	The province grant Municipalities the ability to define permitted forms of tenure.	CNV would participate as required.
7. Facilitate Intensification / Densification	Municipalities to facilitate intensification / densification through: <ul style="list-style-type: none"> removing restrictions to density or height limits, where appropriate; planning space to accommodate new, smaller industrial uses; allowing mixing of industrial with other employment; and exploring opportunities to encourage intensification in target areas via incentives. 	CNV would review the Zoning Bylaw and other policies to determine if any opportunities to make such adjustments exist and prepare policy options for consideration.
8. Undertake Metro Vancouver Financial Study	Metro Vancouver to conduct a study of the financial factors and other issues that prevent the development of multi-storey industrial spaces.	CNV would participate as required.
9. Review Taxation Approach	The Province review the current approach to property tax assessment and tax rates.	CNV would support this review.
10. Clarity on Agri-Industrial Uses in the ALR	Define under what conditions, if any, large-scale organic waste processing facilities are permitted in the ALR.	N/A
11. Clarity on Agri-Industrial Uses in the ALR	Ministry of Agriculture produce guidelines or amend legislation to: <ul style="list-style-type: none"> define the metrics used to measure the 50/50 rule; and, clarify the permitted value-added infrastructure and define a cap on building infrastructure footprint. 	NA
12. Undertake Regional Flood Management Strategy	The Fraser Basin Council recognize industrial lands and their economic contributions to the broader region when identifying and prioritizing measures to adapt to rising sea levels & flood events.	CNV would participate as required
13. Manage Land Use Conflicts through Buffering Policies and Guidelines	<ul style="list-style-type: none"> Develop guidelines for land use policies along the edge of planned or developing Industrial areas. The guidelines would serve as reference on how to mitigate the potential for conflict with other sensitive uses. 	<p>CNV would participate as required.</p> <p>CNV would seek to ensure flexibility in the guidelines' application so that lands adjacent to light industrial and employment lands are able to reasonably redevelop.</p>

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14. Enable Warning of Anticipated Nuisance Effects on the Title	The Province enable municipalities the discretion to place a warning of anticipated nuisance effects on the title of the lands with sensitive uses that are being developed within defined proximity of an established or planned industrial use or goods movement corridor.	CNV would support this
15. Adopt Railway Proximity Guidelines	Municipalities consider adopting the Railway Association of Canada and FCM Guidelines for New Development in Proximity to Railway Operations when considering development permissions and changes of use in proximity to active railway corridors and other major goods movement corridors.	CNV typically considers proximity issues through its development process CNV will review the guidelines to consider their adoption
16. Develop Local Bring-to-Market Strategies	Municipalities with vacant or under-developed industrial lands prepare a bring-to-market strategy.	N/A
17. Ensure Transportation Connectivity – Regional Truck Route Network	TransLink, Metro Vancouver, the Port of Vancouver, the Vancouver Airport Authority and municipalities continue to work together to proactively designate, manage, and regularly update the Regional Truck Route Network in line with the recommendations of the Regional Goods Movement Strategy.	CNV will continue to monitor and bring forward changes as required. <ul style="list-style-type: none"> • This could impact CNV's most important commercial high streets (i.e. Lonsdale Avenue, East and West Esplanade). • CNV needs to form part of the conversation concerning the route planning for commercial and heavy vehicles. • CNV and regional bodies need to explore all options to develop appropriate solutions to protect our residents and create vibrant, liveable, and healthy urban environments, while still accommodating the required heavy vehicles.
18. Ensure Transportation Connectivity – Transit Linages	TransLink, as part of the update to the Regional Transportation Strategy and implementation through Investment Plans, continue to identify viable new opportunities to create and improve transit linkages between the region's industrial areas and local workers.	CNV would participate as required CNV will review this as a part of the Mobility Strategy
19. Support the Optimization and Safety of Goods Movement	Port of Vancouver, Metro Vancouver, TransLink, Vancouver Airport Authority, and rail line operators, work together to identify policies & actions that support the optimization and safety of goods movement to and from industrial lands.	CNV would participate as required

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20. Understand Land Demands for Truck Traffic and Truck Parking	The Province work with municipalities and industry partners to understand, forecast, plan for, and mitigate the impacts of the land demands for truck traffic and truck parking.	CNV would participate as required
21. Optimize Port-Related Land Uses	Port of Vancouver continue to work with Metro Vancouver and TransLink to optimize port-related land uses and container drayage.	CNV would participate as required
22. Greater Vancouver Gateway Council	Greater Vancouver Gateway Council continue its efforts to attract grants and other funding, and leverage their success towards improved infrastructure linkages and capital investments that support regional and local policy goals.	CNV would participate as required
23. Greater Vancouver Urban Freight Council	Greater Vancouver Urban Freight Council continue its efforts to coordinate the implementation of the Regional Goods Movement Strategy between its member organizations.	CNV would participate as required
24. Reduce Environmental Impacts	Regional organizations and stakeholders continue to investigate and implement options designed to reduce the environmental impacts related to the transportation of goods and people in the region, through their respective plans and policies.	CNV continues to work on this through its policy efforts including the upcoming Mobility Strategy, Environment Strategy, Economic Investment Strategy, and more.
25. Improve Data and Monitoring	Metro Vancouver produce an annual report that summarizes changes to the Industrial Lands Inventory, while continuing to publish a comprehensive Regional Industrial Land Inventory every five years.	CNV would participate as required
26. Provide a Summary Report of Local Development Activity	Municipalities provide a summary report of local development activity on lands in the Metro Vancouver Industrial Lands Inventory as part of their regional Development Cost Charge reporting, once per year.	CNV would produce this report on an annual basis
27. Conduct an Employment Survey	Metro Vancouver, in collaboration with member jurisdictions, conduct a regional employment survey on a bi-annual basis.	CNV would participate as required
28. Expand Data Sharing Partnerships	Major regional industrial land users and organizations consider expanding data sharing partnerships for the purpose of improving economic development and infrastructure investment, guiding land	CNV would participate as required

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	use and goods movement planning, and informing the development of associated policies.	
29. Regional Economic Coordination	Metro Vancouver, in consultation with member jurisdictions, First Nations and relevant stakeholder organizations, document and promote the region's value proposition from an industrial economy perspective.	CNV would participate as required
30. Enhance Regional Collaboration	Metro Vancouver seek to enhance collaboration across the region to encourage economic growth and diversity, including on industrial lands.	CNV would participate as required
31. Bring-to-Market Strategies Support	Metro Vancouver assist member jurisdictions seeking to develop and update their own local bring-to-market strategies by providing data and research support in line with its mandate.	N/A
32. Coordinate with Neighbouring Regions	The Province develop a framework for economic and land use planning coordination between neighbouring regions in the broader southwestern BC economic region to support industrial land use and protection.	CNV would participate as required
33. Memorandum of Understanding with the FVRD	Metro Vancouver Regional District and the Fraser Valley Regional District develop and sign a memorandum of understanding that outlines their shared priorities regarding and commitment to the effective and efficient management, protection, and development of industrial lands within the Lower Mainland.	CNV would participate as required
34. Port's Modernization Review	The Federal government, in implementing the Port's Modernization Review, take a broader provincial perspective for ports in British Columbia, in part to alleviate land pressure in the Lower Mainland.	N/A



To: Regional Planning Committee

From: Eric Aderneck, Senior Planner, Regional Planning and Housing Services

Date: September 18, 2020 Meeting Date: October 9, 2020

Subject: ***Metro 2040 Industrial and Mixed Employment Policy Review Recommendations***

RECOMMENDATION

That the MVRD Board endorse the *Metro 2040* Industrial and Mixed Employment Policy Review recommendations as presented in the report dated September 18, 2020, titled “*Metro 2040* Industrial and Mixed Employment Policy Review Recommendations” as the basis for *Metro 2050* industrial and employment policy updates.

EXECUTIVE SUMMARY

To inform the update to *Metro 2040*, Metro Vancouver has undertaken an Industrial and Mixed Employment Policy Review. To improve the regional growth strategy policies for Industrial and Mixed Employment lands and support the vision for the region’s industrial lands as outlined in the recently approved Metro Vancouver Regional Industrial Lands Strategy, staff are recommending the following directions to guide the development of new and amended policy language:

1. Update and clarify the definition of industrial uses;
2. Strengthen regional policies to protect Industrial lands for industrial uses, through:
 - a. Establishing a higher voting threshold to amend the regional land use designation for Industrial lands;
 - b. Developing a new regional overlay for trade-oriented lands, which may include restricting strata tenure and unit sizes;
 - c. Clarifying and strengthening the definition of industrial uses;
3. Introduce some flexibility for Mixed Employment lands by rail rapid transit stations to accommodate higher job density and other regional growth strategy objectives;
4. Encourage industrial lands intensification / densification, where contextually appropriate to the activity and location, and while also considering interface with other uses;
5. Undertake a regional land use assessment; and
6. Improve climate action.

PURPOSE

To seek endorsement by the Regional Planning Committee and MVRD Board of the *Metro 2040* Industrial and Mixed Employment Policy Review recommendations.

BACKGROUND

As part of the update to the regional growth strategy, Metro Vancouver is completing a series of policy reviews by topic area in 2020, which will inform the development of refined and new policy language as part of the second phase of work planned for 2021. The Policy Reviews are intended to explore: what’s working, what’s not working, and what’s missing, in terms of regional policies.

The purpose of the *Metro 2040* Industrial and Mixed Employment Policy Review is to identify ways to enhance the approach in the regional growth strategy by: taking into account research, best practices, industrial and economic trends, development patterns, implementation challenges, and input from stakeholders and member jurisdictions, including those received through the preparation of the new Regional Industrial Lands Strategy. The Policy Review recommendations are now ready for Committee and Board review.

INDUSTRIAL AND MIXED EMPLOYMENT POLICY REVIEW

On March 6, 2020, the Regional Planning Committee received a report outlining the scope of work for the Policy Review (Reference 1).

On July 3, 2020, the MVRD Board approved the Regional Industrial Lands Strategy, which contains as priority actions several recommendations for updating the regional growth strategy. The Policy Review work, along with recommendations from the recently completed Regional Industrial Lands Strategy, shaped the recommendations outlined in this report.

Critical Policy Issues and Stakeholder Engagement

The intended benefits of the proposed policy changes are to enhance the protection and utilization of the region's industrial land capacity and job supporting lands to meet the needs of a growing and evolving regional economy to the year 2050. These recommendations should be considered as a package; the responses should be holistically reviewed as a balancing set of trade-offs, rather than as individual or stand-alone items or options.

During the Policy Review, preliminary issues / challenges were identified, with each having policy implications:

- Commercial uses on Industrial lands
- The changing nature of industry
- Displacement of existing industrial uses on Mixed Employment lands
- Pressure for mixed use development by rail rapid transit stations
- Lack of industrial lands supply
- The role of Mixed Employment lands
- Emerging climate change issues

These issues were explored with multiple stakeholders in July 2020, through meetings with structured discussion questions. A meeting was held with the Regional Planning Advisory Committee (and other relevant member jurisdictions staff) on July 9, 2020, and separate meetings with other stakeholder groups were also held - Port of Vancouver (July 22), NAIOP (July 22), and UDI (July 23). The proposed *Metro 2040* Industrial and Mixed Employment Policy Review Recommendations were presented to the Regional Planning Advisory Committee (RPAC) on September 18, 2020. The purpose of the meetings was to seek municipal / agency / stakeholder feedback on possible directions for the Policy Review.

The stakeholder comments, provided in detail in the Attachment, include:

- A wide recognition of the limited supply of industrial lands in the region;

- Concerns about a possible single definition of 'industrial' for the entire region; noting the complexities and issues associated with higher density industrial and mixed use developments;
- Interest in a certain amount of flexibility to accommodate minor changes, but a desire for clarity of purpose to prevent encroachments of non-industrial uses onto industrial lands;
- Necessity for stronger measures to protect the region's remaining industrial land supply;
- The Port expressed a need for an immediate freeze on the rezoning of any industrial lands; concern over encroachments of non-industrial uses on industrial lands including possibly residential on Mixed Employment lands by rail rapid transit stations; and
- The development community sought greater flexibility in land uses and densities to better respond to market trends, particularly proximate to rail rapid transit stations.

The RPAC and other stakeholder comments have been considered and incorporated as appropriate into the proposed Policy Review's recommendations for consideration by the Regional Planning Committee and MVRD Board.

POLICY REVIEW RECOMMENDATIONS

Based on the research and engagement, the Policy Review recommendations are summarized in Table 1.

Table 1. Industrial and Mixed Employment Policy Review Recommendations

Directions	Benefit
1. Update and clarify the definition of industrial uses Update the definition of industrial land uses / activities, using current terms to reflect new and emerging technologies / methods.	<ul style="list-style-type: none"> • Clearer definition of 'Industrial' uses for a more consistent implementation across the region. • Recognition that different types of industrial activities require different types of land / access.
2. Strengthen regional policies to protect Industrial lands for industrial uses <ol style="list-style-type: none"> a. Establish a higher voting threshold to amend the regional land use designation for Industrial lands; b. Develop a new regional overlay for trade-oriented lands; c. Clarify and strengthen the definition of industrial uses 	<ul style="list-style-type: none"> • Recognition of role of trade-oriented lands in the region and greater protection. • Greater protection of industrial uses on industrial lands, possibly through new policy tier or overlay (e.g. change from Type 3 to Type 2 minor amendment). • Restrict strata tenure and unit sizes on trade-oriented lands.
3. Introduce some flexibility for Mixed Employment lands by rail rapid transit stations to accommodate higher job density and other regional growth strategy objectives Some lands in locations by rail rapid transit stations are facing pressures to develop to higher value and density forms.	<ul style="list-style-type: none"> • Introduce more flexible policies for Mixed Employment lands by rail rapid transit stations, for example within 200 metres. • Allow these sites to accommodate higher employment densities and possibly some residential uses, which help advance other regional and local objectives, such as transit-oriented development, and affordable rental housing.
4. Encourage industrial intensification / densification, where contextually appropriate to the activity and location	<ul style="list-style-type: none"> • Increase industrial intensification / densification to increase the industrial capacity of lands. • Further consider interface with other uses.

Recognizing local contexts, encourage appropriate new forms of industrial.	<ul style="list-style-type: none"> • Need to delineate urban form and use intensity.
5. Undertake a regional land use assessment In a region with constrained land base, there is significant pressure for competing, legitimate land uses.	<ul style="list-style-type: none"> • Proactively identify the 'best' locations for different land uses depending on a developed set of criteria (e.g. location, adjacent land uses, lot size, proximity to the goods movement network). • Identify opportunities for more optimized locations and uses of land in order to support regional and local policy objectives and to inform policy changes.
6. Improve climate action Climate change lens that explores opportunities for how climate change mitigation and adaptation issues can be better considered.	<ul style="list-style-type: none"> • Adapt to climate change, such as rising sea levels and flood events, and enhance resiliency, including through ecosystems services and green infrastructure. • Advance greenhouse gas reduction, climate change mitigation, and natural hazard resilience initiatives.

NEXT STEPS

If Policy Review recommendations are endorsed by the Regional Planning Committee and MVRD Board, staff will begin drafting specific policy language for *Metro 2050* early in 2021. The next phase of work will be informed by input from the other *Metro 2040* policy reviews underway, including work being undertaken on social equity, climate change, and resiliency. There will be further opportunities for the Regional Planning Committee, the Regional Planning Advisory Committee, and the Intergovernmental Advisory Committee members to consider and shape policy wording in this and the other policy areas of the regional growth strategy.

ALTERNATIVES

1. That the MVRD Board endorse the *Metro 2040* Industrial and Mixed Employment Policy Review recommendations as presented in the report dated September 18, 2020, titled "*Metro 2040* Industrial and Mixed Employment Policy Review Recommendations" as the basis for *Metro 2050* industrial and employment policy updates.
2. That the Regional Planning Committee receive for information the report dated September 18, 2020, titled "*Metro 2040* Industrial and Mixed Employment Policy Review Recommendations".

FINANCIAL IMPLICATIONS

There are no financial implications to this report. The *Metro 2040* Policy Reviews are all being undertaken as part of the regular work plan in the Board approved 2020 Regional Planning budget.

CONCLUSION

Industrial and Mixed Employment lands and the associated policies are a key component of the regional growth strategy. The purpose of the *Metro 2040* Industrial and Mixed Employment Policy Review is to explore and identify the challenges and opportunities to enhance the associated regional policies taking into account policy research, current practices, emerging trends, and stakeholder engagement. In focusing on the noted areas it will help to ensure the policy framework for *Metro 2050* better supports industrial and other employment activities in the region.

Attachment

Summary of Stakeholder Engagement Comments

Reference

[Metro 2040 Industrial and Mixed Employment Policy Review Scope of Work, Regional Planning Committee Meeting](#), March 6, 2020

41281269

SUMMARY OF STAKEHOLDER ENGAGEMENT COMMENTS

Regional Planning Advisory Committee (RPAC) Members

- Concern over additional regional land use restrictions; desire to retain municipal ability to make local land use decisions.
- Concern over strengthening Industrial lands protection policies in the regional growth strategy through higher Metro Vancouver Board voting thresholds for land use designation amendments, such as through creation of a second tier of industrial or a trade-oriented land use designation / overlay.
- Question the need of creating another higher protection level or tier for some industrial lands, as Mixed Employment lands already have some 'local industrial' uses that serve the community and are intended to be retained, although municipalities do not want those lands protected / encumbered by further regional growth strategy policies.
- Need for adequate time to review the proposed policy changes, specifically when the draft new / amended policy language is available, and for municipalities to consider how proposed policies relate to map designation changes that will be required in local OCPs.

Municipalities

- Concerned about the notion of a shared or single definition of 'industrial' for the entire region.
- Acknowledge that industrial is changing, and need for the definition and designation to also change over time. Suggest that the definition could be written to protect the first floor of a building as industrial, with other uses above.
- Desire flexibility, at least for the 'small' things. Balance of sometimes wanting a hard line to stop encroachment of non-industrial uses onto industrial lands, and in other cases flexibility to accommodate new mixes of uses.
- Recognize the conflict of objectives between residential development pressures by rail rapid transit locations and the protection of industrial lands and employment uses.
- Recognize that mixed use projects on high cost lands require a residential component to make them financially viable for developers. However, making it easier to introduce residential uses at rail rapid transit locations makes it harder to protect employment lands. Thus need for some level of flexibility of uses in such unique situations and recognizing local context, which is a difficult balance.

Port of Vancouver

- Immediate action is required - need to stop conversions of industrial lands now. Given the urgency of the issue, recommend an immediate freeze on rezoning and other actions.
- Support exploration of a possible Industrial Land Reserve. Furthermore, beyond only including existing lands, consider also including recently 'lost' industrial lands (such as redesignated/rezoned to other uses, but not yet built) into the Reserve.
- Supports a higher voting threshold for regional industrial land use designation changes.

- Mixed Employment designated lands used for industrial purposes should be re-designated to Industrial to protect them for industrial use regulations.
- Preserving the land base should be top priority, and possibly supplemented by a no-net-loss principle to add lands elsewhere as compensation.
- If industrial lands get displaced it only increases pressure on remaining lands elsewhere in the region.
- Concerned about possibly allowing more flexible and mixed uses by rail rapid transit stations, as lands would be subject to speculation and increased values, to the detriment of industrial uses.
- Metro Vancouver should press to protect industrial lands beyond its limited policy powers. For example, if stronger measures or additional tools are needed to protect industrial lands, seek legislation changes from senior levels of government.

NAIOP and UDI Development Industry Associations

- Need flexibility, rather than being too prescriptive. Restrictions are a disincentive to development. If the zoning is too restrictive, it will prevent change over time as is occurring in the market.
- Developers can be creative if they understand the problem/issue and opportunity.
- Mixed use at rail rapid transit stations makes sense, and a mix of light industrial in these locations could be accommodated. Commercial and industrial uses in buildings are possible on the right building/site configuration for the use. The uses could be a mixed use combination, with the addition rather than loss of industrial space.
- Distance from rail rapid transit stations of only 200 metres is too short – need a minimum of 400 metres or 800 metres, which is an acceptable walking distance from stations. Otherwise, not capturing the full opportunity presented by rapid transit stations if the area is too small.
- Also need to consider workforce housing by employment areas and rapid transit.
- There is a need for commercial uses on industrial lands – industry is changing (e.g. digital/high tech) faster than land uses.
- However, intensified / densified industrial space is a substitute product – appropriate uses for upper floors are limited and hard to market.
- Consider an incentive for industrial uses by adding additional density or uses (like is done for residential development when affordable housing objectives are being considered).
- Need to be careful with the ‘trade-oriented’ designation, as it will be perceived as down-zoning of some privately owned lands.
- Blanket policies are not appropriate across the region – there are different areas/contexts, which require clarity yet flexibility in emerging markets.

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The Corporation of **THE CITY OF NORTH VANCOUVER**
STRATEGIC & CORPORATE SERVICES DEPARTMENT
ENGINEERING, PARKS AND ENVIRONMENT DEPARTMENT

REPORT

To: Mayor Linda Buchanan and Members of Council

From: Ian Steward, Property Services Coordinator
Michael Hunter, Manager, Parks and Environment

Subject: PARKLAND ACQUISITION – PORTION OF 250 EAST 15TH STREET –
FUNDING APPROPRIATION

Date: November 25, 2020 File No: 02-2260-01-0001/2020

The following is a suggested recommendation only. Refer to Council Minutes for adopted resolution.

RECOMMENDATION:

PURSUANT to the report of the Manager, Parks & Environment and the Property Services Coordinator, dated November 25, 2020, entitled “Parkland Acquisition – Portion of 250 East 15th Street – Funding Appropriation”:

THAT “Development Cost Charge (Parks) Reserve Fund Bylaw, 2020, No. 8809” (Purchase of Land for Park Purposes at 16th Street and St. Andrews Avenue), a Bylaw to appropriate an amount of \$1,386,000 from the DCC (Parks) Reserve Fund, be considered;

THAT (Funding Appropriation #2076) an amount of \$14,000 be appropriated from the General Reserve Fund for the purpose of funding the Project;

THAT should any of the appropriated amount remain unexpended as at December 31, 2023, the unexpended balance shall be returned to the credit of the said Reserve Fund;

THAT staff be authorized to purchase a portion of 250 East 15th Street for park purposes not to exceed \$1,400,000 including applicable taxes, expenses and fees, substantially as outlined in said report;

THAT the Mayor and City Clerk be authorized to sign and seal any such documents as may be required in order to give effect to the above motions;

AND THAT the Mayor and City Clerk be authorized to sign a License Agreement allowing the owners of 250 East 15th Street to use the City's newly purchased property as a staging area during the construction of the proposed adjacent development.

ATTACHMENTS:

1. Proposed "Development Cost Charge (Parks) Reserve Fund Bylaw, 2020, No. 8809" (Purchase of Land for Park Purposes at 16th Street and St. Andrews Avenue) [#1987687](#)
2. Sketch of proposed park location
3. Report of the Planner 2, dated May 13, 2020, "Rezoning Application: 250 East 15th Street (Nacel Properties Ltd, CD-726)" [#1917249](#)

PURPOSE:

This report seeks appropriation of funding from the Development Cost Charge (Parks) Reserve Fund (Attachment #1) for the purchase of a property for park purposes from Nacel Properties Ltd. (Cressey Development) at 250 East 15th Street (Attachment #2).

BACKGROUND:

The City of North Vancouver Parks Master Plan has identified the need to acquire a new "neighbourhood scale" park in the Central Lonsdale East area in order to meet the Official Community Plan goal of ensuring that all residents are within 400m (or a 5 minute walk) of parkland. The importance of easy access to green spaces in urban neighbourhoods has been highlighted by the World Health Organization, which recommends that all people should be within a 5 minute walk of at least 0.5 ha of park or public open space.

At the June 1, 2020 Regular Meeting of Council, a rezoning application from Nacel Properties Ltd. (Cressey) seeking to redevelop their ageing rental property at 250 East 15th Street was presented for consideration. As detailed in the staff report (link provided as Attachment #3), the proposal included construction of three new residential rental buildings and the provision of a new 24,600 sq.ft. public park on the site's north-eastern quadrant at East 16th Street and St. Andrew's Avenue (attachment #2) to be purchased by the City for \$1,322,988. Cressey will develop the new park at their expense, in conjunction with the adjacent development.

Council directed staff to secure the Community Benefits which includes the parkland at the north east corner of the site (see attachment #2).

Council approved first and second reading of Zoning Bylaw, 1995, No. 6700, Amendment Bylaw, 2020, No. 8769” (Nacel Properties Ltd., 250 East 15th Street, CD-726) on June 1st, and referred the project to a Public Hearing. The Public Hearing was completed on June 22nd, 2020, at which point Council unanimously approved third reading of the bylaw.

DISCUSSION:

The Zoning Bylaw amendment for the Cressey development is expected to be presented for final adoption in late 2020 or early 2021. Assuming the development receives final approval, the City is required to provide Cressey with an initial \$200,000 deposit to secure the parkland, with the balance of the \$1,322,988 purchase price to be provided at closing. In order to have those funds available, staff is now requesting Council's approval to appropriate \$1,400,000 from the Development Cost Charge (Parks) Reserve Fund and General Reserve Fund for the purchase of the new parkland and the associated taxes, charges and fees. A bylaw to complete the requested appropriation is attached for Council's consideration.

Details on the real estate process and the process for the planning, design and delivery of the new park are outlined below.

Park Development Process

A preliminary conceptual plan for the new park was presented in the May 13th Report to Council and at the June 22nd Public Hearing. City staff will work with Cressey's consultant team to refine the concept and then will present the draft to the public for feedback in early 2021. Similar to the consultation process underway for the new park on Eastern Avenue, City staff will provide information on-site and in the neighbourhood, supported by online materials and a survey through the Let's Talk engagement tool. The conceptual plan will then be refined, and the final design prepared to the City's satisfaction.

Real Estate Process

The anticipated process to deliver the park is as follows:

- The proposed sale of the land and subsequent park development will be secured with a Sale and Build to Suit agreement that will be finalized once the overall site receives final adoption of the associated Zoning Bylaw amendment.
- Cressey will subdivide their property creating a 24,600 sq.ft. site for sale to the City.
- The City will then secure the property with a deposit, take a security deposit for park construction by Cressey, pay the remainder of the purchase price and provide a license agreement to Cressey for use of the land for a construction staging for residential component.
- It is expected that Cressey would commence development of the park when they are nearing completion of their project so park and development would be completed at the same time based on City approved design. Cressey is responsible for the cost of constructing the park.

The final transactional agreements have not been finalized but are expected to include a deposit of \$200,000 to Cressey upon final adoption of the rezoning Bylaw and City's approval of the subdivision plan. The balance of the purchase price is to be paid after the satisfaction of various conditions including the subdivision of the parcel of property from the remainder of the property, demolition of the existing structures on the site and payment by Cressey of a deposit for the construction of the parkland when the main development is constructed. The initial payment of \$200,000 payable upon successful rezoning requires approval by Council of the attached appropriation bylaw.

FINANCIAL IMPLICATIONS:

Development Cost Charges (DCC's) are collected by the City through the development approval process where a change of use or an expansion of floor area is proposed. The City DCC's have been established as means of funding the growth related expansion of City park and transportation infrastructure, including the acquisition of new parkland. As of December 31, 2019, the Development Cost Charge (Parks) Reserve Fund had a balance of \$17.2 million. The 2020-2029 Project Plan approved by Council includes a \$10 million provision, inclusive of a 1% municipal assist factor, for park acquisitions. The proposed \$1.4 million appropriation for this property falls within that budgeted amount, with \$1,386,000 from the DCC (Parks) Reserve Fund and \$14,000 (1%) from the General Reserve Fund.

Note: The Bylaw is for a slightly higher amount than the purchase price in the event there are some unforeseen expenses. All unused funds will be returned to the Reserve Fund.

The estimated cost for design and construction of the new park, which will be borne by Cressey, is estimated at approximately \$1.3 million. Once constructed, annual City maintenance costs are anticipated to be \$20,000 per year, depending on the final design of the park.

INTER-DEPARTMENTAL IMPLICATIONS:

The acquisition of land for park purposes has been coordinated with Planning and Development, Engineering, Parks and Environment, Strategic and Corporate Services and Finance.

Once the park land has been acquired, Engineering, Parks & Environment will coordinate the public process and park development.

CORPORATE PLAN AND/OR POLICY IMPLICATIONS:

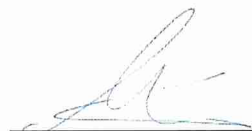
The creation of a new park supports Council's Strategic Plan, and vision for creating the "healthiest small city in the world."

This initiative also aligns with the Parks Master Plan goal of having all residents of the City of North Vancouver living within 400m (five minute walk) from a park. The acquisition of new parkland is supported by Chapter 5 of the Parks, Recreation and Open Space, in the Official Community Plan. The acquisition of parkland in East Central Lonsdale was identified as a priority in the 2010 Parks Master Plan.

This initiative also supports a broad number of goals and objectives expressed in the City's Official Community Plan including:

- 5.17 Acquire new parkland in areas identified as park deficient and in growth neighbourhoods.

RESPECTFULLY SUBMITTED:



Michael Hunter,
Manager, Parks and Environment



Ian Steward,
Property Services Coordinator

THE CORPORATION OF THE CITY OF NORTH VANCOUVER

BYLAW NO. 8809

**A Bylaw to authorize the expenditure of monies from the
Development Cost Charge (Parks) Reserve Fund
for the 2020 Project Plan Appropriations.**

WHEREAS the entire City is listed in “Development Cost Charges Bylaw, 2016, No. 8471” as an area where development cost charges for parks will be levied;

AND WHEREAS the acquisition of parkland is a capital cost permitted to be paid using Development Cost Charge funds under Section 566 (2) (b) of the *Local Government Act*;

NOW THEREFORE the Council of The Corporation of the City of North Vancouver, in open meeting assembled, enacts as follows:

1. This Bylaw shall be known and cited for all purposes as “**Development Cost Charge (Parks) Reserve Fund Bylaw, 2020, No. 8809**” (**Purchase of Land for Park Purposes at 16th Street and St. Andrews Avenue**).
2. The following amount is hereby appropriated from the Development Cost Charge (Parks) Reserve Fund for the purpose of funding:
 - A. \$1,386,000 for the “16th Street and St. Andrews Avenue Park” project.

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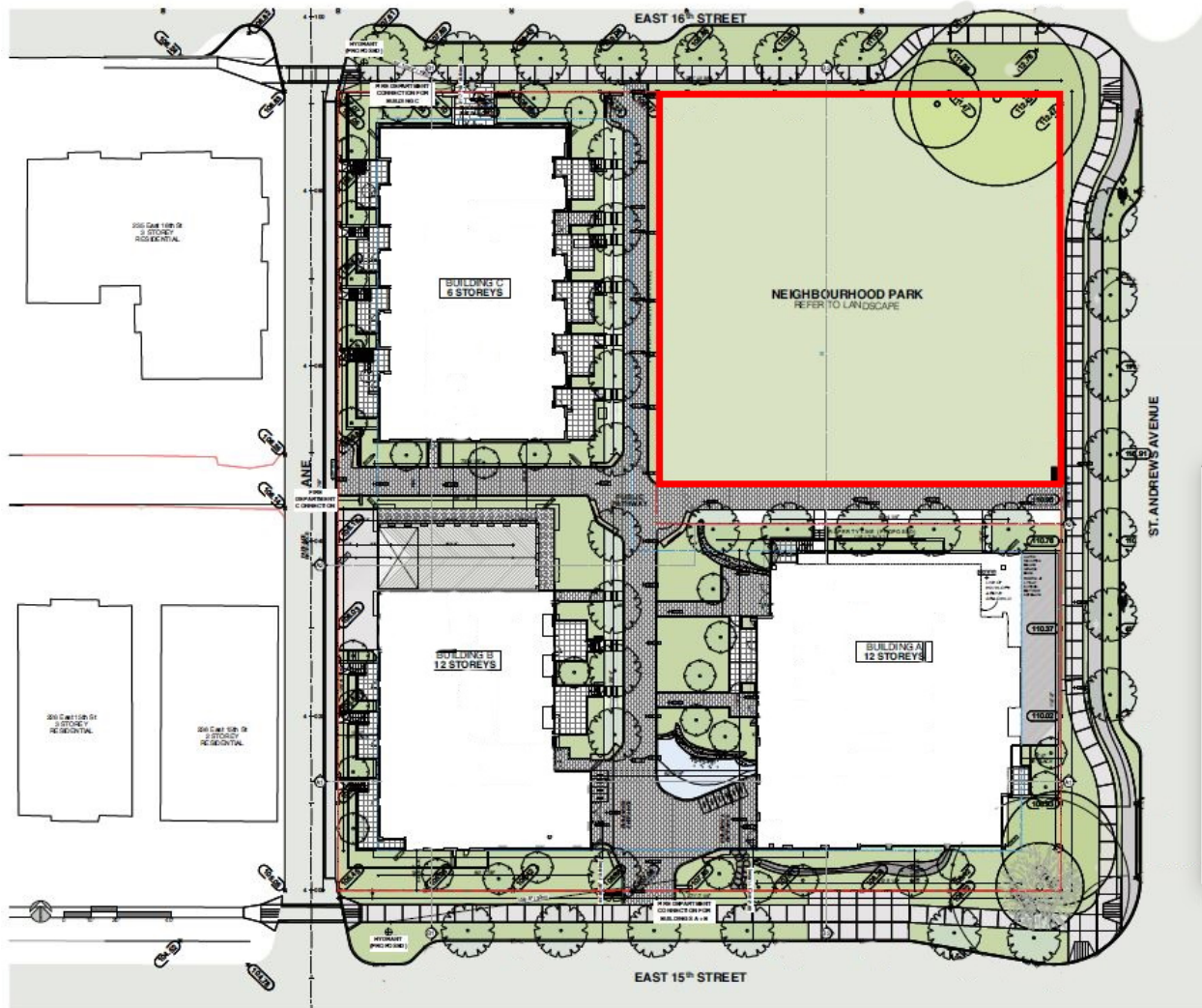
ADOPTED on the <> day of <>, 2020.

MAYOR

CORPORATE OFFICER

Attachment #2

Sketch of proposed neighbourhood park at 16th Street and St. Andrews Avenue





 Division Manager	 Director	 CAO
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The Corporation of **THE CITY OF NORTH VANCOUVER**
PLANNING & DEVELOPMENT DEPARTMENT

REPORT

To: Mayor Linda Buchanan and Members of Council

From: Mike Friesen, Planner 2

Subject: REZONING APPLICATION: 250 EAST 15th STREET (NACEL
PROPERTIES LTD, CD-726)

Date: May 13, 2020 File No: 08-3360-20-0472/1

The following is a suggested recommendation only. Refer to Council Minutes for adopted resolution.

RECOMMENDATION

PURSUANT to the report of the Planner 2, dated May 13, 2020, entitled
"Rezoning Application: 250 East 15th Street (Nacel Properties Ltd, CD-726)":

THAT "Zoning Bylaw, 1995, No. 6700, Amendment Bylaw, 2019, No. 8769"
(Nacel Properties Ltd, 250 East 15th Street, CD-726) be considered and be
referred to a Public Hearing;

THAT "Housing Agreement Bylaw, 2020, No. 8770" (Nacel Properties Ltd, 250
East 15th Street, CD-726, Rental Housing Commitments) be considered and
referred to a Public Hearing;

THAT notification be circulated in accordance with the *Local Government Act*;

THAT the community benefits listed in the report section "Density Bonus and
Community Benefits" be secured through agreements at the applicant's expense
and to the satisfaction of the Director of Planning and Development;

AND THAT the Mayor and City Clerk be authorized to sign all necessary
documentation to give effect to the motion.

ATTACHMENTS

1. Context Map (Doc# [1912389](#))
2. Consolidated Drawing Package (February) (Doc# [1912385](#))
3. Preliminary Park Design (Doc# [1885281](#))
4. Off-Site Servicing Requirements (Doc# [1885276](#))
5. Developer Information Session Summary (Doc# [1912390](#))
6. Zoning Amendment Bylaw No. 8769 (Doc# [1912422](#))
7. Housing Agreement Bylaw No. 8770 (Doc# [1912425](#))

PROJECT DESCRIPTION

The project, located at 250 East 15th Street, consists of three rental residential buildings – two 12-storey buildings adjacent to East 15th Street and one 6-storey building adjacent to East 16th Street – and the provision of a public park on the site's north-eastern quadrant. The project proposes 281 secured rental residential units, including 28 mid-market units (as well as two guest suites for the use of tenants' friends and family), and includes two-storeys of underground parking, bicycle parking, and storage that provides 266 vehicle parking spaces and 474 bicycle parking spaces. The subject site currently hosts two rental residential buildings composed of 101 units that were built in 1964. The requested changes to the Zoning Bylaw to permit this development are identified in Table 1 below, and Attachment 6 – Zoning Amendment Bylaw No. 8769 is the amending bylaw with the proposed changes.

Table 1. Requested Changes to the Zoning By-law

Consideration	Official Community Plan	Current Zoning	Proposed Designation/Regulation
Zone/Designation	Residential Level 5 (north) and Level 6 (south)	RM-1	CD-726
Density	R5 = 2.6 FSR R6 = 3.3 FSR Site = 2.95 FSR	Maximum density of 1.6 FSR	Maximum density of 2.95 FSR
Height	R5 = 6 storeys R6 = 37 metres	Maximum height of 13.0 metres and 3 storeys	R5 = Maximum building height of 16 metres and 6 storeys R6 = Maximum building height of 33.5 metres and 12 storeys
Siting (Setbacks)	N/A	Front, Rear or Exterior Lot Line Setback: 6 metres (20 feet) Interior Lot Line Setback: 4.5 metres (15 feet)	Front, Rear or Exterior Lot Line Setback: between 3-4.5 metres (10-15 feet) Interior Lot Line Setback: between 3-9 metres (10-30 feet)
Unit Separation	N/A	Minimum of between 3 and 12 metres (10 and 40 feet) of separation between living areas and 24 metres (80 feet) above the 4 th storey	Minimum of 7.6 metres (25 feet) between building faces, increasing to 22.5 metres (75 feet) above the 6 th storey.
Minimum unit size	N/A	37.16 square metres (400 square feet)	32 square metres (345 square feet)

Building Width and Length	N/A	Shall not exceed a horizontal width above 30.48 metres (100 feet) above the 3 rd storey.	Requirement waived.
Driveway Slope	N/A	No greater than 10% in first 6 metres (20 feet) and 12.5% thereafter	Requirement waived.
Exterior finishes	N/A	No requirement.	All exterior finishes design, and landscaping shall be approved by the Advisory Design Panel.

POLICY FRAMEWORK

The subject site contains two designations – one designation each for the south and north portions of the site –within the Official Community Plan: Residential Level 5 (R5) to the north and Residential Level 6 (R6) to the south. These designations permit a range of multi-family housing types, with a focus on mid-rise in the R5 designation and high-rise in the R6 designation. These two designations focus on providing quality housing with a mix of unit sizes that support attractive and active streets in the City's central core.

Metro 2040	
Goal 1 Create a Compact Urban Area	Intensifying this site with new development that is consistent with and scaled to the planned vision for the surrounding neighbourhood will support the highest and best use of the land promoting a compact urban area.
Goal 2 Support a Sustainable Economy	Redevelopment that includes a range of rental units to address a variety of household sizes on this site promotes housing forms that can support a diversity of income levels and ensure people live close to where they work.
Goal 4 Develop Complete Communities	The proposed development provides housing for a range of household types as well as a new park to support an active lifestyle and social opportunities in the City's core.
Goal 5 Support Sustainable Transportation Choices	Intensification of this site will support future transit investments along Lonsdale Avenue and will develop a separated AAA Bike Facility along St Andrews Avenue. The site is proximate to community and commercial amenities and is well situated to provide the occupants with a variety of transportation choices across the North Shore and the greater region.

Official Community Plan	
<p><u>Lonsdale Regional City Centre</u></p> <p><i>Policy 1.1.2</i> Align growth with the development community amenities and infrastructure</p> <p><i>Policy 1.1.4</i> Support the role of the Lonsdale Regional City Centre as the urban core of the City, in part, by locating high trip-generating uses such as high density residential and commercial uses there</p> <p><i>Policy 1.3.1</i> Ensure that new development is compatible with the established urban form of the City, reflecting the primacy of the Lonsdale Regional City Centre and the transition through mid- and low-rise buildings to lower-density residential neighbourhoods</p> <p><i>Policy 1.4.5</i> Focus amenity and infrastructure investments in the Lonsdale Regional City Centre and Frequent Transit Development Areas</p>	<p>Intensification of the site supports the Central Lonsdale Area and future transit infrastructure and community amenity investments.</p> <p>Development is located to support local economy and make use of existing infrastructure and amenities.</p> <p>Density and height are consistent with the OCP guidelines and are greatest adjacent to an institutional use (Lions Gate Hospital), and is reduced as it crosses the site.</p> <p>The public park will provide an amenity for what is currently an area underserved for parks. The cycling infrastructure will provide an important link in the City's north-south cycling network.</p>
<p><u>Building and Urban Design</u></p> <p><i>Policy 1.3.5</i> Encourage design excellence in developments through carefully considered, high quality architecture and landscaping, with varied designs which are interesting, sensitive and reflective of their surroundings</p> <p><i>Policy 1.3.6</i> Encourage architecture that responds to the unique context of the City in a sensitive, sustainable, and aesthetically compatible manner</p> <p><i>Policy 1.3.9</i> Explore ways to activate laneways in the City including opportunities for varied uses, pedestrian and cycling activity as</p>	<p>The East 15th Street frontage presents an amenity space to activate the street and rights-of-way through the site provide pedestrian linkages to and from the park. Each building presents a varied but consistent architectural style.</p> <p>The proposed architecture and building materials of the new dwelling have been chosen with sustainability, maintenance, and aesthetics as key determinants. The buildings have been designed to present entrances and glazing at a human scale to provide an attractive urban realm.</p> <p>The development activates the laneway through the inclusion of townhouse units and vegetation through the lane as well as design features integrate pedestrians.</p>

<p>well as storm water management and urban agriculture.</p> <p><i>Policy 1.3.10</i> Encourage active, healthy lifestyles and the opportunity for more social connections through planning and active design principles that encourage physical activity and contribute to enhanced walkability and active streets, sidewalks and public spaces</p> <p><i>Policy 1.4.3</i> Consider recreational, cultural and other community spaces as aspects of informal community living rooms, and essential 'social infrastructure', particularly in high-density neighbourhoods like Central/Lower Lonsdale</p> <p><i>Policy 1.4.4</i> Incorporate active-design principles in new development that encourage physical movement and social interaction thereby contributing to a healthier community</p> <p><i>Policy 2.1.1</i> Invest in cycling and pedestrian networks and facilities to make these more attractive, safer, and convenient transportation choices for all ages and abilities with an aim to increase these ways of travelling over single-occupant vehicle use</p> <p><i>Policy 5.3.3</i> Provide appropriate spaces for and encourage the installation of public art in parks, greenways and open spaces</p>	<p>The project will provide multiple upgrades to the City's active transportation network, and provide a public park to support outdoor activity.</p> <p>The project includes multiple elements from the City's active design guidelines such as including garden plots for residents, prominent stairwells, and a significant social space for the future residents.</p> <p>The proposed development includes a separated bike facility along St. Andrews Avenue, signal and intersection improvements at East 15th and St Andrews and intersection improvements at East 16th and St Andrews to facilitate active transportation in the neighbourhood. The development also includes more than minimum required bicycle parking and a cycling repair/maintenance area.</p> <p>The proposed development will include a public art component.</p>
<p><u>Housing</u></p> <p><i>Policy 1.5.1</i> Provide opportunities for a range of housing densities, diversified in type, size and location</p>	<p>Provides a range of unit types and sizes to accommodate various family sizes and lifestyles. The unit mix includes townhomes – with direct access to the public realm – and apartments ranging in size from studio to 3 bedroom. 281 units will be developed, 28 of which will be a part of the City's mid-market rental</p>

<p><i>Policy 1.5.4</i> Prioritize the development and revitalization of affordable rental housing and use density bonusing and density transfers to incentivize the retention, renewal and/or replacement of rental units as a public benefit</p> <p><i>Policy 1.5.6</i> Ensure a sufficient number of new apartment building provide accessible units and that these accessible units are represented across various unit sizes</p>	<p>programme. Additionally, there will be two guest suites for use by the tenants.</p> <p>The City will secure 25% of the proposed units – or 76 units – will be secured as Level 2 accessible, with the remaining 75% being secured as Level 1 accessible.</p>
<p><u>Park and Greenspace</u></p> <p><i>Policy 3.4.2</i> Consider the potential for food production on public and private land including rooftops, community gardens, micro gardens, composting, urban farming, orchards, beehives and edible landscaping on residential boulevards, park land and rights-of-way</p> <p><i>Policy 4.2.6</i> Increase the ratio of productive, permeable green space to hard impermeable surface areas as redevelopment occurs</p> <p><i>Policy 5.1.2</i> Ensure access to a range of outdoor recreational pursuits and facilities for community members of all ages and abilities</p> <p><i>Policy 5.1.7</i> Acquire new parkland in areas identified as park deficient and in future growth areas</p> <p><i>Policy 5.1.9</i> Acquire parkland through dedication associated with development projects in the City, including consideration of trade-offs between varying building heights and acquiring desired park spaces</p>	<p>The proposed development includes a community garden and outdoor amenity spaces accessible to future residents. Depending on community feedback, the inclusion of community gardens may be considered for the public park as well.</p> <p>The park provides a significant ratio of greenspace for the site as a whole. Non-park area is largely impermeable at ground level, though portions of the buildings' roofs are landscaped.</p> <p>The development includes several opportunities for outdoor pursuits including social (shared rooftop amenity spaces) and recreational (proposed park).</p> <p>The acquisition of the proposed park area will provide greenspace to the eastern side of Central Lonsdale. This area is part of the City's core – a medium to high density residential area – and is currently underserved in terms of accessible park area.</p> <p>The proposed site design conforms to the City's official community plan – in terms of height and density – while permitting the acquisition of area for a neighbourhood park.</p>

Housing Action Plan	
<p><i>Action #3 Mid-Market Rental Units</i> To enable affordability for low and moderate income renters.</p>	<p>The proposed development includes 28 mid-market rental units. The distribution of unit sizes for these units will be: 3 studio units; 14 1-bedroom units; 5 2-bedroom units; 6 3-bedroom units.</p>
<p><i>Action #4 Family Friendly Housing</i> To increase the number of three or more bedroom units appropriate for larger and/or extended families within new multi-unit residential developments.</p>	<p>Twenty percent (57 total) of the proposed development's units contain 3-bedrooms. This is double the City policy's recommended minimum.</p>
Sustainable Development Guidelines	
<p><i>Natural Systems</i> The ability of natural systems, both global and local, to support life. Parks and green spaces help regulate the climate, clean and filter water and air, and provide recreational and aesthetic benefits. Maintaining healthy natural systems will reduce strain on municipal infrastructure, support local wildlife and enhance quality of life for community members.</p>	<p>The park secures a significant green node in perpetuity.</p> <p>Stormwater is addressed on site to reduce impact of major storm events.</p> <p>Preservation of several prominent mature trees.</p> <p>The inclusion of trees and plants that support bird and insect populations.</p>
<p><i>Physical Structures/Infrastructure</i> The ability to effectively deliver basic services, shelter and physical amenities required to sustain the health and well-being of the community. This includes water supply, sanitary sewer, storm water drainage, solid waste management, roads, telecommunications, and energy efficiency and conservation including district energy. As well, this category includes attractive streetscapes, durable buildings, provision of a range of housing types and adequate community amenities.</p>	<p>The project will achieve Step 3 of the BC Building Code Step Code, one step greater than the requirements at the time of application.</p> <p>The project provides a significant number of rental units for the City, including 28 below market units, for a variety of household sizes, from studio units to 3-bedrooms.</p> <p>The project will connect to the Lonsdale Energy Corporation district energy system.</p> <p>The project will upgrade required infrastructure (water, sewer, stormwater systems) as well as improve active transportation infrastructure in and around the site.</p> <p>Additional secured and covered bicycle parking has been integrated into the landscape design in order to support active transportation of residents.</p>

<p><i>Human Potential</i></p> <p>The ability of our local community to support our residents in their pursuit of individual livelihood objectives including access to education, healthy food transportation and affordable housing. Meeting these basic needs is essential for the maintenance and growth of human capacity.</p>	<p>The project includes a range of units to serve a broad spectrum of lifestyles, incomes, and abilities.</p> <p>Significant area has been dedicated to recreational pursuits to support residents in living a healthy lifestyle.</p> <p>The project includes numerous active transportation elements including infrastructure improvements and cycling maintenance facilities.</p>
<p><i>Social Connections</i></p> <p>The ability of our community to foster communication, interaction and networks to respond effectively to community issues. These may include supporting community members with low incomes, lone-parent families, and matters specific to children, youth, seniors and people with disabilities.</p>	<p>The project proposes social space as a central element to the project. By centralising and highlighting the areas for social interaction, the project intends to foster greater community interaction between residents. Additionally the proposed park can be a hub for the broader community.</p>
<p><i>Cultural Diversity</i></p> <p>The ability of our community to support and celebrate a diversity of cultural backgrounds. This includes recognition of the traditions of the Squamish Nation and the many cultures of residents who make the City their home. With both tangible and intangible elements, cultural capacity has economic implications and is strongly connected to social traditions. Manifestations of cultural practices can range from spiritual practices to heritage buildings</p>	<p>The project will include a public art piece, to be located in a prominent location. The North Vancouver Public Art Advisory Committee will support the project in determining the final location and what an appropriate piece may be.</p>

PLANNING ANALYSIS

Site Context

The surrounding land uses are identified in Table 2 below.

Table 2. Surrounding Uses

Direction	Address	Description	Zoning
North – Across East 16 th Street	260 East 16 th St	50-unit, 3 storey, rental residential building	Medium Density Apartment Residential 1 (RM -1)
East – across St Andrews Ave	1504-1552 St Andrews Ave	2-2.5 storey duplex	Two-Unit Residential 1 (RT-1)
South – Across East 15 th	230-240 E 13 th St, 231 E 15 th St	Multi-storey health services complex	Public Use and Assembly 1 (P-1)
West – Across Lane	235 East 16 th St	19 unit, three storey rental residential building	CD-425
West – Across Lane	236 East 15 th St	9 unit, 2 storey, rental residential building	Medium Density Apartment Residential 1 (RM -1)

The subject site is located on the eastern edge of the City's high-density residential core. It is north of the Lions Gate Hospital's Evergreen House, east and south of existing apartment buildings, and west of existing duplexes (the duplexes are a part of the duplex special study area and may be re-designated). The site currently contains two residential apartment buildings that contain a combined 101 units.

The site is proximate to numerous urban amenities, including two major bus routes (Lonsdale and 15th Street), health services, civic amenities (City Hall, City Library, Harry Jerome Community Recreation Centre), bike routes (St. Andrews, 13th Street), and commercial services. Please see Attachment 1 for several maps illustrating the context.

Use

Residential Level 6 has the purpose of providing well-designed high-density development in the Lonsdale Regional City Centre, while Residential Level 5 should provide quality multi-family housing with a mix of unit sizes, and a focus on creating attractive and active streets. R6 permits a maximum density of 3.3 FSR and a maximum height of 37 metres while R5 permits a maximum density of 2.6 FSR and 6 storeys; the combined site density is therefore 2.95 FSR with a height of 37 metres on the southern portion of the site and 6 storeys on the northern portion.

The policy framework applicable to the subject site supports the proposed high-density residential apartment use. The site is located in the City's high-density core and is in close proximity to frequent transit infrastructure (Lonsdale Avenue and East 15th Street) as well as Central Lonsdale's commercial and social amenities. The proposed development will secure 28 mid-market rental units at reduced rates (10% below CMHC average rates) while increasing the number of rental units from 101 to 281 (an increase of 181 units).

The proposed 24,600 square foot park is consistent with the permitted land uses and will provide an area that is currently underserved in park space with a green hub to support physical and social activity.

Intensity

The proposed intensification on the site is appropriate for the neighbourhood given the applicable OCP and City policy framework for the subject site and the surrounding land use designations. The increase in intensity is supportable due to the subject site's proximity to the Lonsdale Core, the project's response to the City's policy, and the public benefits achieved by the City through the acquisition of a new park. The Residential Level 5 and 6 designations permits the development of mid-rise and high-rise multi-family apartment buildings.

With the proposed development the site will accommodate a total of 281 units split between three buildings, as well as two guest suites. The south portion of the site, designated R6 with a maximum density of 3.3 FSR, hosts two 12 storey buildings, while the north portion of the site, designated R5 with a maximum density of 2.6 FSR, hosts a six storey building and the proposed public park. The project achieves the site's maximum blended density 2.95 FSR through the provision of market rental housing – including 10% of units secured for ten years at 10% below CMHC market rents – secured through a housing agreement (see Attachment 7 – Housing Agreement Bylaw No. 8770). The application was received prior to the enactment of policy to secure 10% of the units in perpetuity.

The breakdown of the unit types is as follows:

- Studio – 34 (12%) total, 3 mid-market and 2 guest suites
- 1-Bedroom – 148 (52%) total, 14 mid-market
- 2-Bedroom – 44 (16%) total, 5 mid-market
- 3-Bedroom – 57 (20%) total, 6 mid-market

The proposed project includes 266 parking spaces, including the appropriate ratio of visitor and accessible stalls, which surpasses the 169 spaces that would be required under current Zoning Bylaw requirements. The project also exceeds the minimum required bicycle parking stipulated by the Zoning Bylaw, providing 474 spaces rather than the 422 required by bylaw; to further support cycling in the development the proposal includes a cycling repair/maintenance workshop as a part of its amenity space. The project also provides for public realm improvements such as a separated cycling facility, a pedestrian and cyclist-controlled signal, and curb realignment to support active transportation in and around the site.

Regarding the environmental standards, the proposed development will achieve Step 3 of the BC Building Code Step Code. Outlets capable of supporting Level 2 electric vehicle charging capacity will be provided for all required parking spaces, with the exception of visitor stalls. Storm water will be addressed through on-site mitigation, including retention tanks to support stormwater management during more significant storm events. Covered bicycle parking has also been integrated into the landscape design in order to promote active transportation modes.

Form

The urban form proposed is consistent with the planned character of the surrounding neighbourhood as illustrated by the City's Official Community Plan. The form of the development will conform generally to the attached drawings (Attachment 2: Consolidated Drawing Package, February). The design proposes three buildings on the

site, two 12-storey buildings located adjacent to East 15th Street, and one 6-storey building on the western portion of the site's East 16th Street frontage.

The three buildings frame a proposed public park that has been placed in the north-east quadrant of the site. To secure access to the proposed public park, several rights-of-way – both running through the middle of the site, one north-south oriented and the other east-west – will be secured to ensure public access through the site. The three buildings are designed with a common architectural character, but the details of each building – such as the materials, windows, and balcony design – differentiate the buildings so as to not have them appear repetitive.

The principle design element of the project, aside from the provision of the public park, is the span between the two 12-storey buildings along 15th Street. The proposed span will host the majority of the shared amenity spaces for the complex, including a games room, a reception area, a large lounge, washrooms, and a bookable event space that could be used for hosting a party or a cooking class for the building. The spaces will be available to the tenants of all three buildings and the applicant has stated the importance of forming a community for tenants that will provide renters with a greater social experience than a traditional rental building. Significant glazing of these amenity spaces will support the activation and attractiveness of the streetscape on East 15th Street and will provide passive surveillance of the bus stop located directly in front of the proposed project. Other street frontages are activated through the inclusion of at-grade entrances to units, or where the grade does not support entrances, balconies and landscaping are utilized to ensure an attractive and activated interface between the public and private realms. A rooftop amenity space – only accessible to residents – is included on the 6-storey building and the connecting span that includes: community garden plots, planters for additional vegetation, a lounge area and outdoor dining/cooking area, as well as a greenhouse structure with a washroom. The easternmost 12-storey building includes a rooftop component only accessible to the tenants of the top units.

Density Bonus and Community Benefits

Analysis of the project regarding the City's Density Bonus and Community Benefits Policy indicates that the proposed project, were it not providing secured market rental housing, would include community benefits valued at approximately \$16M as outlined in Table 3 below. The community benefit contribution in question is calculated from the existing permitted density to the proposed maximum bonus density.

Table 3. Estimated Value of Community Benefits through Density Bonusing

Density Value Calculation	Value
Density Bonus to Max Density OCP Density (28,515.59 square feet @ \$25 / sq. ft.)	\$712,889
Density Bonus to Max Bonus OCP Density (81,473 square feet @ \$190 / sq. ft.)	\$15,479,892
Total Value of Community Benefits	\$16,192,781

In lieu of providing a cash contribution the proposed project will provide rental housing – 281 units secured in perpetuity through a housing agreement including 28 mid-market rental units – as is stipulated through the City's Official Community Plan and 2018

Density Bonus and Community Benefits Policy. Bonusing for rental housing is intended to assist the City in achieving its housing goals set out in the Official Community Plan and Housing Action Plan.

In addition to the items above, staff seek Council's direction to secure the following items as a condition of the development:

- Purchase of the public park lands, on which the applicant will construct a City approved park;
- Public art incorporated into the project with a value of \$175,000;
- Public access rights-of-way through the project to provide pedestrian connection;
- Improved offsite works, including expanded cycling and pedestrian facilities and a new intersection signal (see Attachment 4 – Off-Site Servicing Requirements).

Park Acquisition

The proposed development includes the provision of a public park to support the achievement of goals and objectives outlined in the City's Official Community Plan – *Policy 5.1.7*, acquire new parkland in areas identified as park deficient and in future growth areas – as well as the City's Parks Master Plan – *Goal 2*, Ensure that parks are well distributed throughout the community, particularly in higher density areas. In this case, the acquisition of a park is made possible by the size of the site, which can accommodate the site density in a fashion consistent with the OCP while reserving approximately 30% of the site (24,600 sq.ft.) as a public space.

The City can direct Development Cost Charges collected through redevelopment throughout the City towards the acquisition of parks, but is more limited in how those funds can be used for the improvement (construction, new structures, programming, and maintenance) of the park. By purchasing land, as opposed to receiving land and funding improvements, the City has an opportunity to acquire an improved park. Staff therefore recommend that the City purchase the park from the applicant at a fair value, and that the applicant improve the park in tandem with their residential development. Under the proposed agreement, the City will purchase the park at a value of \$53.78/sq.ft. for a total purchase cost of \$1,322,988. The design of the park is subject to public consultation and the applicant will be required to construct the final design at their cost; staff estimate the value of the park improvements of the preliminary design to be approximately \$1.3 million.

Several major benefits emerge through having the park constructed by the applicant as a part of their larger development, including:

- Timing: a complete park will be delivered in tandem with the larger development with no lag between the completion of the rental housing and the delivery of an amenity for the community.
- Efficiencies: construction efficiencies can be achieved regarding various aspects of the project including earth moving, the installation of infrastructure (electricity, stormwater management), the purchase of surface materials and plant materials, earth moving, and labour costs. These efficiencies provide greater value to residents of the City of North Vancouver than could be delivered if the City were

responsible for the delivery of the park after the completion of the apartment building.

- Reduced impact on adjacent residents: by limiting the project to one construction period, the impact of construction (noise, parking disruption, untidiness) is reduced.

A preliminary design has been included as a part of this report (see: Attachment 3 – Preliminary Park Design), that includes play area, lawn/open space, seating areas, circulation, and the provision of new trees. The design of the park is not finalized, and the agreement will require a public design exercise to allow the neighbourhood to provide valuable feedback and help to identify desired park elements and programming. The Parks Department will review revised plans to ensure that the design incorporates public feedback and that the park achieves required City standards prior to the issuance of a full building permit for the residential portion of the project.

COMMUNITY CONSULTATION

Developer Information Session

A Developer Information Session (DIS) was held May 9th, 2019. Twenty-seven members of the public signed in to the session and six comment forms were received. The applicant has provided a summary of the event (Attachment 5 – Developer Information Session Summary).

The attendees were primarily those who live in the building. The DIS was held early in the process, prior to the applicant being able to contact many of the tenants through their formal tenant relocation process. The tenant relocation strategy is discussed further in the “Tenant Relocation Strategy” section below.

Items identified as concerns by the community feedback include the affordability of future units, the height of the proposed buildings, the removal of existing trees, impact of construction, and the effect of dog waste on the proposed park.

Tenant Relocation Strategy

As an existing rental building, the applicant is required to provide support to existing tenants as stipulated by the City’s Residential Tenant Displacement Policy.

The policy provides several key elements to support existing tenants in transition to new housing should the project be approved, including:

- Provide early notification to tenants to inform them of the rezoning process;
- Find up to three comparable rental units (i.e. same number of bedrooms, in the City of North Vancouver, at not more than 10% above the City’s average rent for a unit of that size);
- Compensate all tenants with three months rent to assist in securing alternate accommodations;
- Compensate tenants for moving expenses;
- Provide all tenants with 4 months notice only after the issuance of a demolition permit;
- Provide first right of refusal to existing tenants to live in the new building.

The applicant has agreed to all of the requirements of the City's Residential Tenant Displacement Policy and has undergone a concerted effort to engage existing residents in order to understand their housing needs. The applicant has been able to engage with the majority of residents, though some tenants have not responded to efforts to engage in relocation discussions.

ADVISORY BODY INPUT

The application was directed to the Advisory Design Panel on May 15th, 2019. The Panel recommended the approval of the project subject to addressing the following issues to the satisfaction of the Development Planner assigned to the file:

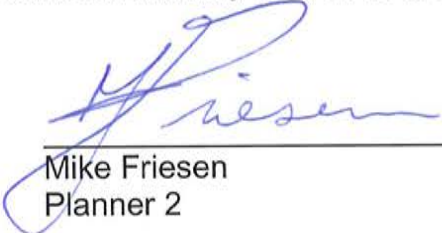
- Encouraged to include additional solar shading on the upper elevations of the south and west facades to mitigate solar heat gain;
- Ensure the park amenity is successfully planned along side the development while also considering the connections from the street, programming of the park, rain protection and effective lighting of the paths;
- Consider achieving Step 4 of the Step Code;
- Include clear and definitive unit identification for first responders;
- Further review of the P1 bike corridors for improved CPTED protection; and
- Consider additional outlets for e-bikes.

In response to the Advisory Design Panel's suggestions the applicant reviewed the aspects identified and amended various aspects of the proposal, these include: improving solar shading; unit identification for units accessible from the street; security for the parkade; and increasing outlets for e-bikes.

CONCLUSION

The proposed application represents an appropriate development for the land use designation and a design that is responsive to the planned vision for the community. Intensification – particularly of market and mid-market rental residential projects – adjacent to the Lonsdale Core will support transportation infrastructure improvements and enhanced amenities in the area. The acquisition of a public park will secure an amenity for future generations of residents at a reasonable cost to the City. The requested zoning change and development proposal are consistent with the Region's and the City's planning policies. Overall, the application looks to implement a development that provides a significant neighbourhood amenity as well as increased density in an appropriate design & location.

RESPECTFULLY SUBMITTED:



Mike Friesen
Planner 2

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THE CORPORATION OF THE CITY OF NORTH VANCOUVER

BYLAW NO. 8809

**A Bylaw to authorize the expenditure of monies from the
Development Cost Charge (Parks) Reserve Fund
for the 2020 Project Plan Appropriations.**

WHEREAS the entire City is listed in “Development Cost Charges Bylaw, 2016, No. 8471” as an area where development cost charges for parks will be levied;

AND WHEREAS the acquisition of parkland is a capital cost permitted to be paid using Development Cost Charge funds under Section 566 (2) (b) of the *Local Government Act*;

NOW THEREFORE the Council of The Corporation of the City of North Vancouver, in open meeting assembled, enacts as follows:

1. This Bylaw shall be known and cited for all purposes as “**Development Cost Charge (Parks) Reserve Fund Bylaw, 2020, No. 8809**” (**Purchase of Land for Park Purposes at 16th Street and St. Andrews Avenue**).
2. The following amount is hereby appropriated from the Development Cost Charge (Parks) Reserve Fund for the purpose of funding:
 - A. \$1,386,000 for the “16th Street and St. Andrews Avenue Park” project.

READ a first time on the <> day of <>, 2020.

READ a second time on the <> day of <>, 2020.

READ a third time on the <> day of <>, 2020.




ADOPTED on the <> day of <>, 2020.

MAYOR

CORPORATE OFFICER

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 Department Manager	 Director	 CAO
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The Corporation of **THE CITY OF NORTH VANCOUVER**
COMMUNITY & PARTNER ENGAGEMENT DEPARTMENT

REPORT

To: Mayor Linda Buchanan and Members of Council

From: Leah Herman, Community Development Coordinator

Subject: 2020 CHILDREN AND YOUTH INITIATIVES FUND – SPECIAL PROJECT
GRANT APPLICATIONS

Date: November 24, 2020 File No: 05-1850-20-003/2020

The following is a suggested recommendation only. Refer to Council Minutes for adopted resolution.

RECOMMENDATION

PURSUANT to the report of the Community Development Coordinator, dated November 24, 2020, entitled “2020 Children and Youth Initiatives Fund – Special Project Grant Applications”:

THAT the following Children and Youth Initiative Fund grant allocations totaling \$20,500 for 2020 be approved:

Girl Guides of Canada	
Cedar Shadows on the Move.....	\$1,000
Mountainside Secondary School	
Bicycle Mechanics.....	\$3,000
North Shore Crisis Services Society	
PEACE Camp.....	\$1,500
North Shore Neighbourhood House / Edible Garden Project	
Garden-Based Learning at Loutet Farm.....	\$3,000
North Shore Restorative Justice Society	
Youth Justice Lab Series.....	\$2,000
North Shore Women’s Centre	
Fearless: Girls’ Empowerment Camp.....	\$2,000
RC Life Skills Society	
Life Skills Boxing Program via Griffins Boxing & Fitness.....	\$1,500
St. Thomas Aquinas Regional High School	
BariSTA Café.....	\$2,000
Sutherland Secondary School	
Girls Group Glam (Bread and Roses).....	\$2,000
Washington Kids Foundation	
WKF Her Time.....	\$1,000

Jack.org	
Talk at the Top Jack Summit.....	\$1,500
TOTAL.....	\$20,500

THAT the following applications be denied:

Queen Mary Community Elementary Developing Self-Advocacy Skills and Awareness for Safety
Sutherland Secondary School Sutherland's Got Talent

THAT a copy of Council's resolution be sent to the District of North Vancouver and the North Vancouver School Board (District #44) for their information;

THAT the Children and Youth Initiatives Grant Committee be thanked for their time and participation;

AND THAT staff be directed to review the Children and Youth Initiatives Fund and report back on opportunities to improve the efficiency and effectiveness of the granting process and adequacy of funding levels.

ATTACHMENTS

1. Children and Youth Initiative Fund Grant Guidelines (Doc # [1939259](#))
2. Summary of Children and Youth Initiatives Grant Applications 2020 (Doc # [1990307](#))

BACKGROUND

Since 1998, the City has disbursed the Children and Youth Initiatives Fund to provide seed money to support or initiate events, programs and services for children and youth in the City of North Vancouver.

The City offers grants of up to \$3,000 for special projects undertaken for children and youth up to the age of 21 years and up to \$500 for projects undertaken for youth between the ages of 12 and 21 years.

Grants approved under this fund are for new programs or services, or to enhance or augment existing programs or services which have proven beneficial to young members of our community. All activities supported by this fund must be based in the City of North Vancouver or be of primary benefit to children and youth residents in the City. Applicants are encouraged to involve children and youth who are not normally involved in community activities. Funds can be used to cover expenses such as staffing costs and/or program expenses such as building rental, equipment rentals, publicity, entertainment and/or refreshments.

The Children and Youth Initiatives Grant intake process in previous years aligned with the Youth Awards and Scholarships intake each spring but was moved to the fall to better accommodate funding cycles and planning timelines for service providers. This will be the fifth year the City has conducted the intake in September.

DISCUSSION

The 2020 Children and Youth Initiatives Grants Review Selection Committee met via Webex on November 5th to review applications and make recommendations for Council's consideration. 13 applications were received this year totalling a request of \$34,300 from an available fund balance of \$20,500. The Review Committee is recommending that 11 out of the 13 applicants receive funding, for a total allocation of \$20,500.

Appointed Grants Committee members included Councillors Tina Hu and Angela Girard and School District Trustee Kulvir Mann. Five youth served as Selection Committee members this year based on their level of community involvement with youth in the City. These youth representatives volunteered their time on the Selection Committee for both the Youth Awards and Scholarships and the Children and Youth Initiatives Fund. Youth representatives included Andrew Warner of North Van Arts, Arley Morris, Maya Tomes, Neil Chowdhury of CityFest and Christopher Zupanec, Youth Outreach Worker at the North Shore Neighbourhood House. Lisa Hubbard, Executive Director of the North Shore Neighbourhood House volunteered her time this year for one youth mentor representative who was unable to attend.

The Committee reviewed applications that met granting criteria and the emerging needs of young people in the current climate of a pandemic. Given the growing need of youth mental health support this year, the Committee considered new and existing initiatives that could safely carry out initiatives through the pandemic and provide opportunities for social connection, health and wellbeing.

City staff provided the Committee with a secured information package to review in advance of the selection process. During the selection meeting, City staff provided background on the granting process, outlined guidelines and criteria, detailed summaries of each application and informed on any associated past funding. The Committee discussed and addressed any conflicts of interest in the selection process to ensure all grants were reviewed and selected in an equitable and fair manner.

Criteria included:

- Program or project that supports connection and wellbeing during COVID-19
- Program or project that supports youth mental health
- Program or project that is preventative in nature, youth driven, or involves youth in program development and implementation
- Supports new events, programs or services
- Provides opportunities for skill development, builds self-esteem and self confidence
- Enhances existing events, programs or services which have proven beneficial to young members of our community
- Involves children and youth who are not normally involved in community activities
- Integrates children and youth with disabilities, low-income families and minority groups
- Encourages broad community involvement or interagency cooperation and collaboration
- Program or project is carried out in the City or primarily benefits City youth

Although 12 of the 13 applications met grant criteria, the limited amount of funds meant that not all applications received the full funding requested. In these cases, the Committee took

into consideration new initiatives, the number of City youth served, history of past funding through the granting program and alternative funding sources.

Funds were allocated by deciding majority vote. Three applications received majority vote for full funding and eight applications for partial funding. One application was rejected due to not meeting grant criteria and one application secured alternative funding from another source for their initiative.

The partial funding formula was based on a flat funding amount of \$1000 with new and existing programs not previously funded prioritized for top-up amounts. Remaining funds were then allocated to existing programs proven beneficial to City youth, who received grants last year. Attachment #2 provides a summary of the applications received and recommended allocations from the Committee.

FINANCIAL IMPLICATIONS

If Council approves the recommendations of the Selection Committee, the total available fund balance of \$20,500 will be allocated from the Children and Youth Initiatives Fund.

INTER-DEPARTMENTAL IMPLICATIONS

City Communications promoted the 2020 Children and Youth Initiatives Grants Intake through social media channels.

STRATEGIC PLAN, OCP OR POLICY IMPLICATIONS

The City of North Vancouver's Strategic Plan supports the health and well-being of all which includes providing opportunities for children and youth to engage and connect with their community. Through supporting creative initiatives that foster a sense of place, invite play and improve happiness, we enhance social connection and build a strong sense of belonging for youth throughout the City.

CNV4ME, the City's Child, Youth and Family Friendly Strategy, supports children and youth in the community through supporting access to a wide variety of community based opportunities that help enhance skills, develop leadership abilities and contribute to community empowerment.

RESPECTFULLY SUBMITTED:



Leah Herman
Community Development Coordinator



**COMMUNITY & PARTNER ENGAGEMENT DEPARTMENT
BUSINESS AND COMMUNITY PARTNERSHIPS**
CITY OF NORTH VANCOUVER T 604 985 7761
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BC / CANADA / V7M 1H9 CNV.ORG

CHILD AND YOUTH INITIATIVES FUND GRANTS GUIDELINES

(Agency Initiated Project Grants and Youth Initiated Project Grants)

2020 DEADLINE: September 30, 2020 @ 5 p.m.

1.0 PURPOSE OF THE FUND

The purpose of this fund is to support or initiate events, programs and services for children and youth in the City of North Vancouver. All activities supported by this fund must be based in the City of North Vancouver, or be of primary benefit to children and youth resident in the City. This fund is not available for sports programs, or to supplement core funding for Outreach Youth Services. Efforts should be made to involve children and youth who are not normally involved in community activities. Grants approved under this fund shall be for new events, programs or services, or to enhance existing events, programs or services which have proven successful.

2.0 OBJECTIVES

Events, programs and services which are funded under this initiative will meet fully, or in part, the following objectives.

- 2.1 Be preventative in nature.
- 2.2 Create opportunities for youth to initiate projects.
- 2.3 Create opportunities to contribute to community well-being and to participate in community activities.
- 2.4 Provide opportunities for skill development.
- 2.5 Develop self-esteem and self-confidence.
- 2.6 Be responsive to the diverse and changing needs of children and youth and, as such, give special consideration to children and youth with disabilities, from low-income families, and of newcomers.
- 2.7 Encourage broad community involvement (financial and human resources) from children and youth, business, organizations and individuals.
- 2.8 Maximize the use of existing community resources through creative partnership arrangements.
- 2.9 Involve children and youth in program development and implementation.
- 2.10 Encourage inter-agency / organization, cooperation and collaboration.

3.0 CATEGORIES OF GRANTS

3.1 Youth Initiated Project Grants

The intention of the Youth Initiated Project Grant is to involve children and youth in the development and implementation of projects and events, which can be supported by youth workers, community service agencies and businesses. The intent of youth involvement is to provide opportunities for youth to take the lead in organizing and delivering a program or an event.

Grants of up to \$500 will be available for Projects and Events. The grant may be used to cover such things as building / facility rental costs, equipment rental costs, publicity, entertainment, refreshments, etc. Applications are available on the City of North Vancouver's website at www.cnv.org/YouthGrants.

3.2 Agency Initiated Project Grants

Agency Initiated Projects shall be preventative in nature, providing children and youth with information, skills and activities which promote healthy living and positive activities. Grants of up to \$3,000 will be available for new projects, or to augment existing programs. Grant recipients will be required to provide one-third (1/3) of the amount of the grant received as in-kind services and / or cash.

Core funded agencies, non-profit societies, community groups, Parent Advisory Councils, and School District No. 44 are eligible to apply for these grants. Programs should be offered in the City of North Vancouver, or be of primary benefit to City children and youth. Preference will be given to projects which exhibit inter-agency collaboration. The grant may be used to cover staffing costs and / or program expenses. Applications are available on the City of North Vancouver's website at www.cnv.org/YouthGrants.

4.0 ELIGIBILITY

Youth, non-profit societies, community groups, Parent Advisory Councils and School District No. 44 are eligible to apply for grants under this initiative. Private consultants and businesses are not eligible to apply directly to this fund, but may be partners in projects managed by the above-mentioned groups. Outreach Youth Service agencies are not eligible to apply for a grant under this fund if it is to be applied to core operating costs of Outreach Youth Worker positions. These agencies may, however, apply for a grant that is for a Youth Initiated Project and Event, or Agency Initiated Project that compliments the services provided by Outreach Youth Workers.

All projects, events, programs and services supported by this fund must be located in the City of North Vancouver, or be of primary benefit to children and youth (up to the age of 24 years) residents in the City of North Vancouver.

5.0 APPLICATION AND APPROVAL PROCESS

Individuals and groups interested in applying for a grant under this fund shall complete the application form provided and submit to *Leah Herman, Community Development Coordinator, City of North Vancouver, 141 West 14th Street, North Vancouver, BC V7M 1H9*. Deadlines for applications may apply.

Applications for Agency Initiated Grants shall be reviewed by a committee comprised of City staff responsible for child and youth issues, two City Councillors, a School Trustee (#44), and youth representatives. This committee will make recommendations to Council regarding allocations.

Applications for Youth Initiated Projects and Events shall be reviewed by City staff responsible for child and youth issues. Staff will recommend grant allocations to Council under this category. **The deadline for applications is 5:00 pm on September 30, 2020.**

All applications will be reviewed using the criteria outlined herein. If you have any questions about the application form, or the grants program, please call Leah Herman, Community Development Coordinator at 604-983-7385 or email: youth@cnv.org.

6.0 ACCOUNTABILITY

Grant recipients will be required to submit an evaluation of the project at its completion, using a form to be provided by the City or available on the City's website at www.cnv.org/YouthGrants.

7.0 ADMINISTRATION OF FUND

This fund will be administered by staff responsible for child and youth issues.

2020 CHILD & YOUTH INITIATIVES FUND GRANT APPLICATIONS

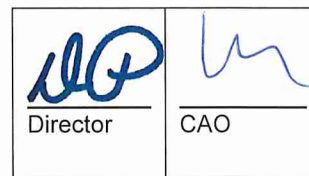
APPLICANT AND SUMMARY DESCRIPTION OF SERVICE/PROJECT	2020 AMOUNT REQUESTED	2020 AMOUNT RECOMMENDED
New Applicant - Existing Program <u>Girl Guides of Canada (Cedar Shadows)</u> Cedar Shadows on the Move - Due to COVID-19, Girl Guides are unable to conduct their usual meetings as they normally would. A major stumbling block has been the cost of meeting spaces and the cost of food. This amendment to continue running the program for youth would involve making "pop-up" kits that would allow two groups per weekday night to meet outdoors under cover. Grant funds would be used to purchase the items needed to make the kits, including lights, tables, hand sanitizer, sound systems, enhanced First Aid supplies, rain ponchos and glow-sticks.	\$3,000	\$1,000
Previous Applicant – New Program <u>Mountainside Secondary School</u> Bicycle Mechanics - This new elective allows Mountainside students to obtain 4 credits towards graduation. Learning bike mechanics will give students life long skills and a sense of confidence as well as increase their employability with skills transferable to other trades. Grant funds would be used for the tools and materials required for this class.	\$3,000	\$3,000
Previous Applicant – Existing Program <u>North Shore Crisis Services Society</u> PEACE Camp - The PEACE (Prevention, Education, Advocacy, Counselling and Empowerment) counselling program offers a series of summer day camps for at risk North Shore children and youth. This is an existing program that will consist of two four-day camps which are organized and run by PEACE counsellors and volunteers. Camp is free for participants with food and transportation to and from activities provided. The primary focus is on building relationships, giving campers and volunteers alike the opportunity to socialize and build self-esteem. Grant funds would be used for the cost of a counsellor and supplies.	\$1,500	\$1,500

APPLICANT AND SUMMARY DESCRIPTION OF SERVICE/PROJECT	2020 AMOUNT REQUESTED	2020 AMOUNT RECOMMENDED
<p>Previous Applicant – Existing Program</p> <p><u>North Shore Neighbourhood House / Edible Garden Project</u></p> <p>Garden-Based Learning at Loutet Farm – This existing program provides field trips for students from grades K - 7 focusing on garden-based education. The goal is to provide experiential, interactive programming that dives into the cycles of food production and creates connections to biology, ecology, meteorology, and more. The aim is to provide programming accessible to all students and inspire teachers as well. Grant funds would be used for the wage of the Education Coordinator as well as bus transportation for the students.</p>	\$3,000	\$3,000
<p>Previous Applicant – New Program</p> <p><u>North Shore Restorative Justice Society</u></p> <p>Youth Justice Lab Series – This new program will design and launch a youth-focused speaker series which would consist of virtual monthly speaker sessions. Speakers will deliver an hour's worth of content on such topics as decolonization, equity, and restorative justice followed by a restorative circle guided by staff wherein participants can share their reflections and experiences with the topic. Goals include creating a safe and supportive safe for youth to connect, utilizing restorative justice practices to engage youth in building community, targeting and building relationships with at-risk and marginalized youth and supporting development of communication skills and self awareness. Grant funds would be used for community outreach, honorariums for speakers and staff wages.</p>	\$3,000	\$2,000
<p>Previous Applicant – Existing Program</p> <p><u>North Shore Women's Centre</u></p> <p>Fearless: Girls' Empowerment Camp - The annual Girls' Empowerment Camp is an existing program that provides a forum for teen girls from across the North Shore to come together to explore current issues affecting them in a respectful, supportive and fun environment through the</p>	\$3,000	\$2,000

APPLICANT AND SUMMARY DESCRIPTION OF SERVICE/PROJECT	2020 AMOUNT REQUESTED	2020 AMOUNT RECOMMENDED
<p>guidance of an experienced youth facilitator. Over the course of a week, the group engages in a variety of interesting activities including discussion, visual art & drama activities, outdoor excursions, a self-defence workshop and media critique. The goal is to support teen girls (inclusive of gender variant youth) to access their individual and collective strengths in order to empower themselves and to help create social change. Grant funds would be used to cover facility costs, presenters, equipment and day trips.</p>		
<p>Previous Applicant – New Program</p> <p><u>Queen Mary Community Elementary</u></p> <p>Developing Self-Advocacy Skills and Awareness for Safety - All students in Grades 1 - 7 will take part in new workshops facilitated by Saleema Noon, Sexual Health Educator. These workshops are designed to teach students about their bodies and how to protect themselves from sexual abuse. The workshops provide tools to make smart decisions and to successfully navigate the changes ahead. Topics involve puberty, safety on the internet, healthy body image and assertiveness. An evening workshop for parents is also included. Grant funds would be used to cover the cost of the program facilitator.</p>	\$700	\$0
<p>Previous Applicant – Existing Program</p> <p><u>RC Life Skills Society</u></p> <p>Life Skills Boxing Program via Griffins Boxing & Fitness – This existing program is in its fourth year and helps expand youths' knowledge on boxing skills and working on a healthy mind set. The program to teach boxing is a tiered Glove System similar to karate for boxing. Students build confidence, become more fit and learn a time honoured sport. The founder of Life Skills drives students from different high schools on the North Shore to their workout appointment at Griffins, followed by a smoothie and conversation to encourage peer social connection. Grant funds would be used to cover boxing classes, smoothies and mileage.</p>	\$3,000	\$1,500

APPLICANT AND SUMMARY DESCRIPTION OF SERVICE/PROJECT	2020 AMOUNT REQUESTED	2020 AMOUNT RECOMMENDED
New Applicant – Existing Program <u>St. Thomas Aquinas Regional High School</u> BariSTA Café - This existing program has been active at STA for a few years within the Special Education Department. It is designed as a youth-directed/staff-supported approach to foster practical skills and personal growth for students with special needs to transition into the workforce upon graduation. Grant funds would be used to purchase equipment to expand the program and continue running the Café.	\$2,100	\$2,000
New Applicant – Existing Program <u>Sutherland Secondary School</u> Girls Group Glam (Bread and Roses) - This peer support group has been meeting regularly at lunch hour and after school for several years at Sutherland. GGG is a fluid group of youth who provide a safe place to connect and run youth inspired activities. Grant funds would be used to fund activities that are focused on self care, cooking, photography and nutrition.	\$3,000	\$2,000
Incomplete Application <u>Sutherland Secondary School</u> Sutherland's Got Talent – A new program providing a creative showcase for students by way of talent show for students attending Sutherland Secondary. Grant funds would be used for supplies to run the talent show.	\$3,000	\$0 Incomplete Application
Previous Applicant – New Program <u>Washington Kids Foundation</u> WKF Her Time – This program has been developed to specifically support female youth aged 13 - 18 years as they navigate critical years of their lives. This program is inclusive and eliminates barriers such as financial, transportation and family circumstances. Staff will lead and mentor young women and promote positive mental and physical health. Specific details include partnering with the Vancouver Police Department to provide motivational speakers, integration with	\$3,000	\$1,000

APPLICANT AND SUMMARY DESCRIPTION OF SERVICE/PROJECT	2020 AMOUNT REQUESTED	2020 AMOUNT RECOMMENDED
the Foundry for a 2 hour session each week, coordination with the CMHA to support youth dealing with issues such as depression and anxiety. Grant funds would be used to supply coaches and mentors, nutrition/meal support for attendees and transport for field experiences.		
Previous Applicant – Existing Program <u>Jack.org</u> Talk at the Top Jack Summit - Across Canada, youth mental health crisis lines have seen an increase in demand; young people need easily accessible mental health resources. This existing program is a youth-led, community-based forum for young people in North Vancouver to exchange knowledge, enhance their skills, and build solidarity around common goals. Young people are empowered to advocate and collaborate for change during this challenging time when coming together in person isn't possible. Grant funds would be used to cover staff costs, speakers and outreach material.	\$3,000	\$1,500



The Corporation of **THE CITY OF NORTH VANCOUVER**
ENGINEERING, PARKS AND ENVIRONMENT DEPARTMENT

REPORT

To: Mayor Linda Buchanan and Members of Council

From: Karyn Magnusson, Deputy Director, Engineering Parks and Environment

Subject: SPIRIT TRAIL MOSQUITO CREEK MARINA – FUNDING
REALLOCATION

Date: November 25, 2020 File No: 12-6130-20-0056/1

The following is a suggested recommendation only. Please refer to Council Minutes for adopted resolution.

RECOMMENDATION

PURSUANT to the report of the Deputy Director, dated November 25, 2020, entitled “Spirit Trail Mosquito Creek Marina – Funding Reallocation”:

THAT \$67,371.93 of existing funding from project Green Necklace: West 21st Street-Jones to Lonsdale (50154) be reallocated to fund Mosquito Creek Marina Spirit Trail (50115) project;

AND THAT \$51,531.30 of existing funding from project Green Necklace: Lonsdale to Grand BLVD/E 19th (50155) be reallocated to fund Mosquito Creek Marina Spirit Trail (50115) project.

DISCUSSION

In the fall of 2018, the City and the Squamish Nation collectively opened the section of the Spirit Trail that passes through the Squamish Nation’s Mission I.R. #1 and through the operating Mosquito Creek Marina. This section is a critical link in the realization of the North Shore Spirit Trail concept and has been enjoyed by many, with over 450,000 people making trips through this section of the Spirit Trail so far in 2020, which is a 22% increase over 2019.

Since the opening, the City and the Squamish Nation have continued to find ways to improve the safety and experience of the trail. Additional expenses included increasing fence heights for after-hours security, improving resiliency of the infrastructure and art installations, restoring the functionality of the marina operations that were affected by the adjacent Spirit Trail construction and introducing wayfinding, conflict management

signage and paint markings including the ‘slow your roll’ campaign implementation to improve user comfort and reduce conflicts.

At this time, the project is deemed complete. Ongoing maintenance and repair of the improvements are the responsibility of the Squamish Nation; however, there are areas of work where the City has taken on aspects of maintenance and functional improvements because of our expertise and equipment availability. These areas include ongoing adjustments to signage and wayfinding to provide consistency with the Spirit Trail overall and to reduce user conflicts on the popular multiuse trail, landscape maintenance in areas that are continuous with City property, and snow removal and ice treatment along the Spirit Trail path. City efforts are funded from annual operations budgets.

FINANCIAL IMPLICATIONS

In order to fund the project modifications and improvements for the Spirit Trail Mosquito Creek Marina Project, staff propose to reallocate \$118,903 of surplus capital funds from similar projects, namely:

- \$67,371.93 of capital funds from Project Green Necklace: West 21st Street-Jones to Lonsdale (50154); and
- \$51,531.30 of capital funds from Project Green Necklace: Lonsdale to Grand BLVD/E 19th (50155).

Both of these sections of the Green Necklace are complete and the funds identified are surplus and not from DCC funding sources.

Total project funding and costs are as follows:

TOTAL PROJECT FUNDS	AMOUNT
2013 City Budget	\$ 30,000.00
2014 City Budget	\$ 2,800,000.00
2016 City Budget	\$ 3,100,000.00
2018 City Budget	\$ 443,865.65
Transfer from closed utility projects	\$ 68,831.76
Provincial Grants	\$ 400,000.00
TOTAL FUNDS	\$ 6,842,697.41
PROJECT COSTS	\$ 6,961,600.64
BALANCE	\$ (118,903.23)

INTER-DEPARTMENTAL IMPLICATIONS

Finance and Engineering, Parks and Environment staff coordinated to identify the proposed funding reallocation.

STRATEGIC PLAN, OCP OR POLICY IMPLICATIONS

The 2018-2022 Council Strategic Plan identifies a vision to create the Healthiest Small City in the World, through five key priorities. Provision of safe, accessible and inviting

multimodal paths, such as the Spirit Trail, helps provide both recreational opportunities for our community and supports the following priorities:

- A City for People; and
- A Connected City.

The North Shore Spirit Trail incorporates a broad number of Goals and Objectives expressed in the City's Official Community Plan, including access to safe transportation systems.

RESPECTFULLY SUBMITTED:



Karyn Magnusson
Deputy Director, Engineering Parks and
Environment

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Department Manager	Director	CAO
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The Corporation of **THE CITY OF NORTH VANCOUVER**
STRATEGIC & CORPORATE SERVICES DEPARTMENT

REPORT

To: Mayor Linda Buchanan and Members of Council

From: Heather Reinhold, Deputy Director, Strategic & Corporate Services

Subject: MICKEY McDOUGALL FEASIBILITY STUDY – EXTENSION OF FUNDING REQUEST

Date: November 16, 2020 File No: 02-0800-30-0013/1

The following is a suggested recommendation only. Refer to Council Minutes for adopted resolution.

RECOMMENDATION

PURSUANT to the report of the Deputy Director, Strategic & Corporate Services, dated November 16, 2020, entitled “Mickey McDougall Feasibility Study – Extension of Funding Request”:

THAT Funding Appropriation No.1738 for \$150,000 - Mickey McDougall Feasibility Study with Flicka Gymnastics, be extended by one year;

AND THAT should any of the above appropriated amounts remain unexpended as December 31, 2021, the unexpended balance be returned to the credit of the Civic Amenity Reserve Fund.

BACKGROUND

As part of the Harry Jerome Community Centre process, funding, in the amount of \$150,000, was appropriated in June 2017 for a cost-shared feasibility study with Flicka Gymnastics for the re-purposing of the Mickey McDougall gym for the purposes of Gymnastics.

In 2018, in coordination with Flicka Gymnastics and the North Vancouver Lawn Bowling Club (NVLBC), a feasibility study was undertaken for the Mickey McDougall building & site. The feasibility study contemplated three scenarios:

1. Repurpose the existing Mickey McDougall building for use by Flicka with minimal interference on the remainder of the site and build a new clubhouse for NVLBC in the northeast corner;
2. Renovate the existing Mickey McDougall building to expand the gym for use by Flicka, add underground parking and co-locate NVLBC's clubhouse with a building expansion;
3. Replace the existing Mickey McDougall building (phased) for use by Flicka, construct underground parking and build a new clubhouse for NVLBC on St. Andrews Ave.

In 2018, the first scenario (repurposing the existing Mickey McDougall building for use by Flicka and a new lawn bowling facility on the northeast corner) was confirmed as the preferred direction from a budget, stakeholder need and site opportunity perspective.

As the feasibility scope was broadened to incorporate the NVLBC project, the study was not cost shared with Flicka Gymnastics and 50% of the cost was allocated to the Lawn Bowling project.

The NVLBC project has proceeded based on the preferred scenario for the site, with a future clubhouse to be located at the northeast corner of the site.

DISCUSSION

In the years since the 2018 feasibility study was completed, Flicka Gymnastics has an updated understanding of their program needs and construction costs have changed. The representatives of Flicka Gymnastics has indicated that the previous preferred option of a straight repurpose of the Mickey McDougall building will no longer meet the needs of their organization.

An update to the Feasibility Study is planned to ensure that a preferred option can be detailed to meet Flicka's program needs with an associated cost estimate. Flicka Gymnastics will be responsible for the capital costs of a renovation / new facility. The City would be responsible for any life-safety upgrades associated with re-use of the facility.

Existing funding is available to undertake a feasibility update now, however, as this work is anticipated to extend into early to mid-2021, the existing funding, which expires at the end of this year, needs to be extended.

FINANCIAL IMPLICATIONS

Funding in the amount of \$150,000 was appropriated in 2017 for a feasibility study with Flicka Gymnastics. There is \$78,632 remaining in the project budget. Expenses to date have been the 2018 Feasibility Study cost (\$60,470 - with 50% allocated to NVLBC project) and allocation for TFT staffing. The remaining funding is sufficient to complete the feasibility update.

Included within the 2021-2030 Capital Plan is a request for \$1,358,313 for owner upgrades and an allocation for TFT staffing.

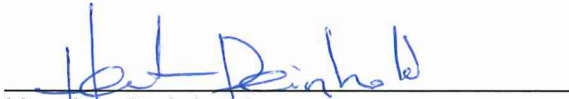
INTER-DEPARTMENTAL IMPLICATIONS

The re-use or renovation of Mickey McDougall facility for use by Flicka Gymnastics will be developed in coordination with Finance, Planning and the North Vancouver Recreation and Culture Commission.

STRATEGIC PLAN, OCP OR POLICY IMPLICATIONS

The re-use or renovation of Mickey McDougall facility for use by Flicka Gymnastics supports Council's vision of the Healthiest Small City in the World by supporting and enhancing the health and wellness of our community.

RESPECTFULLY SUBMITTED:



Heather Reinhold
Deputy Director, Strategic & Corporate Services