



**AGENDA FOR THE REGULAR MEETING OF COUNCIL TO  
COMMENCE AT 6:00 PM, IN THE COUNCIL CHAMBER, CITY  
HALL, 141 WEST 14<sup>TH</sup> STREET, NORTH VANCOUVER, BC, ON  
MONDAY, JUNE 17, 2019**

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**MONDAY, JUNE 17, 2019  
COUNCIL MEETING – 6:00 PM**

“Live” Broadcast via City Website [www.cnv.org/LiveStreaming](http://www.cnv.org/LiveStreaming)  
Complete Agenda Package available at [www.cnv.org/CouncilMeetings](http://www.cnv.org/CouncilMeetings)

**CALL TO ORDER**

**APPROVAL OF AGENDA**

1. Regular Council Meeting Agenda, June 17, 2019

**ADOPTION OF MINUTES**

2. Regular Council Meeting Minutes, June 10, 2019
3. Special Regular Council Meeting Minutes, June 11, 2019

**PUBLIC INPUT PERIOD**

**PRESENTATIONS**

2019 Cool It! Challenge Winners – Mayor Buchanan and Manon Gartside,  
Environmental Educator, British Columbia Sustainable Energy Association

*Information Report, June 5, 2019 – “2019 Cool It! Challenge Winners”*

2019 Living City Award Recipients – Mayor Buchanan and Steve Tornes,  
Vice Chair, Advisory Planning Commission

*Information Report, June 5, 2019 – “2019 Living City Award Recipients”*

**CONSENT AGENDA**

Item \*4 is listed in the Consent Agenda for consideration.

**BYLAW – ADOPTION**

- \*4. “Local Area Service Parcel Tax Bylaw, 2019, No. 8711” (Paved Lane  
Construction East of the 1700 Block of Fell Avenue)





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**DELEGATION**

U11 North Vancouver Football Club Euro Travel Team 2019 – Ephraim Claro, Finn Montgomery, Szilard Puddiford, and Righ Walker – Euro Travel Team 2019 – Report on Italy Travels

*Item 5 refers.*

**CORRESPONDENCE**

5. Darla Johnston, Parent Volunteer, North Vancouver Football Club Euro 2019 Parent Committee – Euro Travel Team 2019 – Report on Italy Travels, February 9, 2019

**DELEGATION**

Ian Rose-Innes, Director, Cycling Without Age North Shore, and Annwen Loverin, Executive Director, Silver Harbour Centre – Trishaw as a Mobility Aid

*Item 6 refers.*

**CORRESPONDENCE**

6. Ian Rose-Innes, Director, Cycling Without Age North Shore – Trishaw as a Mobility Aid, April 11, 2019
7. Rail-Volution – Building Livable Communities with Transit, September 8-11, 2019

**REPORT**

8. Mosquito Creek Bridge Replacement – Funding Reallocation

**BYLAWS – FIRST, SECOND AND THIRD READINGS**

9. “Development Cost Charge (Transportation) Reserve Fund Bylaw, 2018, No. 8623, Amendment Bylaw, 2019, No. 8722” (Funding Reallocation)
10. “Development Cost Charge (Transportation) Reserve Fund Bylaw, 2018, No. 8625, Amendment Bylaw, 2019, No. 8723” (Funding Reallocation)
11. “Development Cost Charge (Transportation) Reserve Fund Bylaw, 2018, No. 8690, Amendment Bylaw, 2019, No. 8724” (Funding Reallocation)

**REPORT**

12. Bike Valet at Community Events – Policy Updates



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**PUBLIC HEARING – 151 East Keith Road – 7:00 PM**

**BYLAWS – THIRD READING**

13. “Zoning Bylaw, 1995, No. 6700, Amendment Bylaw, 2018, No. 8618” (Urban Systems / IMH 151 E Keith Apartments Ltd., 151 East Keith Road, CD-702)
14. “Housing Agreement Bylaw, 2018, No. 8622” (Urban Systems / IMH 151 E Keith Apartments Ltd., 151 East Keith Road, CD-702, Rental Housing Commitments)

**NOTICE OF MOTION**

15. Strategies to Steward Land and Housing Resources – Mayor Buchanan

**COUNCIL REPORT**

Each Council member is permitted 5 minutes to report on their activities.

**PUBLIC CLARIFICATION PERIOD**

**COUNCIL INQUIRIES**

**NEW ITEMS OF BUSINESS**

**NOTICES OF MOTION**

**CITY CLERK’S RECOMMENDATION**

THAT Council recess to the Committee of the Whole, Closed session, pursuant to the *Community Charter*, Section 90(1)(e) [land matter].

**REPORT OF THE COMMITTEE OF THE WHOLE (CLOSED SESSION)**

**ADJOURN**



## **THE CORPORATION OF THE CITY OF NORTH VANCOUVER**

### **Regular Council Meeting – June 17, 2019**

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#### **CALL TO ORDER**

#### **APPROVAL OF AGENDA**

1. Regular Council Meeting Agenda, June 17, 2019

#### **ADOPTION OF MINUTES**

2. Regular Council Meeting Minutes, June 10, 2019
3. Special Regular Council Meeting Minutes, June 11, 2019

#### **PUBLIC INPUT PERIOD**

The Public Input Period is addressed in sections 12.20 to 12.28 of "Council Procedure Bylaw, 2015, No. 8500."

The time allotted for each speaker appearing before Council during the Public Input Period is two minutes, with the number of speakers set at five persons. Speakers' presentations will be audio and video recorded, as well as live-streamed on the Internet, and will form part of the public record.

To make a submission to Council during the Public Input Period, a person must complete the Public Input Period sign-up sheet at City Hall prior to the Regular Council Meeting. A person who fails to complete, or only partially completes, the Public Input Period sign-up sheet will not be permitted to make a submission to Council during the Public Input Period. The sign-up sheet will be available on the table in the lobby outside the Council Chamber from 5:30 pm until 5:55 pm before a Council meeting.

When appearing before Council, speakers are requested to state their name and address for the record. Speakers may display materials on the document camera at the podium in the Council Chamber and provide written materials to the City Clerk for distribution to Council, only if these materials have been provided to the City Clerk by 4:00 pm on the date of the meeting.

The Public Input Period provides an opportunity for input only, without the expectation of a response from Council, and places the speaker's concern on record.

Speakers must comply with the General Rules of Conduct set out in section 5.1 of "Council Procedure Bylaw, 2015, No. 8500" and may not speak with respect to items as listed in section 12.25(2).

Speakers are requested not to address matters that refer to items from a concluded Public Hearing/Public Meeting and to Public Hearings, Public Meetings and Committee meetings when those matters are scheduled on the same evening's agenda and an opportunity for public input is provided when the particular item comes forward for discussion.

Please address the Mayor as "Mayor, followed by his/her surname" or "Your Worship". Councillors should be addressed as "Councillor, followed by their surname".



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**PRESENTATIONS**

2019 Cool It! Challenge Winners – Mayor Buchanan and Manon Gartside,  
Environmental Educator, British Columbia Sustainable Energy Association

*Information Report, June 5, 2019 – “2019 Cool It! Challenge Winners”*

2019 Living City Award Recipients – Mayor Buchanan and Steve Tornes,  
Vice Chair, Advisory Planning Commission

*Information Report, June 5, 2019 – “2019 Living City Award Recipients”*

**CONSENT AGENDA**

Item \*4 is listed in the Consent Agenda for consideration.

**RECOMMENDATION:**

THAT the recommendation listed within the “Consent Agenda” be approved.

**START OF CONSENT AGENDA**

**BYLAW – ADOPTION**

- \*4. “Local Area Service Parcel Tax Bylaw, 2019, No. 8711” (Paved Lane  
Construction East of the 1700 Block of Fell Avenue)

**RECOMMENDATION:**

THAT “Local Area Service Parcel Tax Bylaw, 2019, No. 8711” (Paved Lane  
Construction East of the 1700 Block of Fell Avenue) be adopted, signed by the  
Mayor and City Clerk and affixed with the corporate seal.

**END OF CONSENT AGENDA**



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**DELEGATION**

U11 North Vancouver Football Club Euro Travel Team 2019 – Ephraim Claro, Finn Montgomery, Szilard Puddiford, and Righ Walker

Re: Euro Travel Team 2019 – Report on Italy Travels

*Item 5 refers.*

**CORRESPONDENCE**

5. Darla Johnston, Parent Volunteer, North Vancouver Football Club Euro 2019 Parent Committee, February 9, 2019 – File: 01-0220-07-0001/2019

Re: Euro Travel Team 2019 – Report on Italy Travels

**RECOMMENDATION:**

THAT the correspondence from Darla Johnston, Parent Volunteer, North Vancouver Football Club Euro 2019 Parent Committee, dated February 9, 2019, regarding the “Euro Travel Team 2019 – Report on Italy Travels”, be received with thanks.

**DELEGATION**

Ian Rose-Innes, Director, Cycling Without Age North Shore, and Annwen Loverin, Executive Director, Silver Harbour Centre

Re: Trishaw as a Mobility Aid

*Item 6 refers.*

**CORRESPONDENCE**

6. Ian Rose-Innes, Director, Cycling Without Age North Shore, April 11, 2019 – File: 01-0230-0054/2019

Re: Trishaw as a Mobility Aid

**RECOMMENDATION:**

THAT the correspondence from Ian Rose-Innes, Director, Cycling Without Age North Shore, dated April 11, 2019, regarding “Trishaw as a Mobility Aid”, be received with thanks.



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**CORRESPONDENCE – Continued**

7. Rail-Volution – Building Livable Communities with Transit, September 8-11, 2019  
– File: 01-0390-01-0001/2019

Re: Rail-Volution – Building Livable Communities with Transit,  
September 8-11, 2019, Vancouver, BC

**RECOMMENDATION:**

PURSUANT to the correspondence regarding “Rail-Volution – Building Livable Communities with Transit”:

THAT Council members be authorized to attend the Rail-Volution – Building Livable Communities with Transit Conference, to be held September 8-11, 2019, in Vancouver, BC;

THAT the expenses be paid in accordance with City Policy;

AND THAT the funds be provided from the Conference Education Travel Account.

**REPORT**

8. Mosquito Creek Bridge Replacement – Funding Reallocation  
– File: 11-5400-02-0001/1

Report: Manager, Engineering Planning and Design, June 5, 2019

**RECOMMENDATION:**

PURSUANT to the report of the Manager, Engineering Planning and Design, dated June 5, 2019, entitled “Mosquito Creek Bridge Replacement – Funding Reallocation”:

THAT \$1,480,000 of existing funding from Project 51162 B-Line: Queensbury to Heywood be reallocated to fund the Mosquito Creek Bridge Replacement;

THAT “Development Cost Charge (Transportation) Reserve Fund Bylaw, 2018, No. 8690, Amendment Bylaw, 2019, No. 8724” (Funding Reallocation) be considered;

*Continued...*



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**REPORT – Continued**

8. Mosquito Creek Bridge Replacement – Funding Reallocation  
– File: 11-5400-02-0001/1 – Continued

THAT \$1,000,000 of existing funding from Project 53226: Moodyville Neighbourhood Transportation Improvements be reallocated to fund the Mosquito Creek Bridge Replacement;

THAT “Development Cost Charge (Transportation) Reserve Fund Bylaw, 2018, No. 8625, Amendment Bylaw, 2019, No. 8723” (Funding Reallocation) be considered;

THAT \$200,000 of existing funding from Project 51168: 2019 Pavement Management be reallocated to fund the Mosquito Creek Bridge Replacement;

THAT \$190,000 of existing funding from Project 53240: East 3<sup>rd</sup>: Queensbury to Kennard be reallocated to fund the Mosquito Creek Bridge Replacement;

THAT “Development Cost Charge (Transportation) Reserve Fund Bylaw, 2018, No. 8623, Amendment Bylaw, 2019, No. 8722” (Funding Reallocation) be considered;

THAT \$350,000 of existing funding from Project 56084: Watermain Replacement East 21<sup>st</sup>: Lonsdale to St. Georges be reallocated to fund watermain replacement associated with the Mosquito Creek Bridge Replacement;

THAT \$180,000 of existing funding from Project 56085: Watermain Replacement St. Georges: East 21<sup>st</sup> to East 22<sup>nd</sup> be reallocated to fund watermain replacement associated with the Mosquito Creek Bridge Replacement;

AND THAT \$200,000 of existing funding from Project 51089: Bridge Rehabilitation be reallocated to fund the Mosquito Creek Bridge Replacement.

*Items 9, 10 and 11 refer.*

**BYLAWS – FIRST, SECOND AND THIRD READINGS**

9. “Development Cost Charge (Transportation) Reserve Fund Bylaw, 2018, No. 8623, Amendment Bylaw, 2019, No. 8722” (Funding Reallocation)

**RECOMMENDATION:**

THAT “Development Cost Charge (Transportation) Reserve Fund Bylaw, 2018, No. 8623, Amendment Bylaw, 2019, No. 8722” (Funding Reallocation) be given first, second and third readings.



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**BYLAWS – FIRST, SECOND AND THIRD READINGS – Continued**

10. “Development Cost Charge (Transportation) Reserve Fund Bylaw, 2018, No. 8625, Amendment Bylaw, 2019, No. 8723” (Funding Reallocation)

**RECOMMENDATION:**

THAT “Development Cost Charge (Transportation) Reserve Fund Bylaw, 2018, No. 8625, Amendment Bylaw, 2019, No. 8723” (Funding Reallocation) be given first, second and third readings.

11. “Development Cost Charge (Transportation) Reserve Fund Bylaw, 2018, No. 8690, Amendment Bylaw, 2019, No. 8724” (Funding Reallocation)

**RECOMMENDATION:**

THAT “Development Cost Charge (Transportation) Reserve Fund Bylaw, 2018, No. 8690, Amendment Bylaw, 2019, No. 8724” (Funding Reallocation) be given first, second and third readings.

**REPORT**

12. Bike Valet at Community Events – Policy Updates – File: 16-8330-05-0001/1

Report: Deputy Director, Planning and Development, and Planning Assistant, Transportation, June 5, 2019

**RECOMMENDATION:**

PURSUANT to the report of the Deputy Director, Planning and Development, and Planning Assistant, Transportation, dated June 5, 2019, entitled “Bike Valet at Community Events – Policy Updates”:

THAT the updated Shipyards and Foot of Lonsdale Events Policy (C59B) and Policy for Events in Parks and Public Open Spaces (C57C) be approved.





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**PUBLIC HEARING – 151 East Keith Road**

“Zoning Bylaw, 1995, No. 6700, Amendment Bylaw, 2018, No. 8618” (Urban Systems / IMH 151 E Keith Apartments Ltd., 151 East Keith Road, CD-702) and “Housing Agreement Bylaw, 2018, No. 8622” (Urban Systems / IMH 151 E Keith Apartments Ltd., 151 East Keith Road, CD-702, Rental Housing Commitments) would rezone the subject property from a High Density Apartment Residential (RH-1) Zone to a Comprehensive Development 702 (CD-702) Zone to permit the development of two infill buildings that would add 40 residential rental units to the existing tower. The infill buildings would add 0.88 FSR to the property, with the tallest building being approximately 18.1 metres (59.3 feet) tall. A total of 77 parking stalls would be located within the existing parking structure.

*Bylaw Nos. 8618 and 8622 to be considered under Items 13 and 14.*

**AGENDA**

Staff presentation  
Applicant presentation  
Representations from the public  
Questions from Council  
Motion to conclude the Public Hearing

**BYLAWS – THIRD READING**

13. “Zoning Bylaw, 1995, No. 6700, Amendment Bylaw, 2018, No. 8618” (Urban Systems / IMH 151 E Keith Apartments Ltd., 151 East Keith Road, CD-702)

**RECOMMENDATION:**

THAT “Zoning Bylaw, 1995, No. 6700, Amendment Bylaw, 2018, No. 8618” (Urban Systems / IMH 151 E Keith Apartments Ltd., 151 East Keith Road, CD-702) be given third reading.

14. “Housing Agreement Bylaw, 2018, No. 8622” (Urban Systems / IMH 151 E Keith Apartments Ltd., 151 East Keith Road, CD-702, Rental Housing Commitments)

**RECOMMENDATION:**

THAT “Housing Agreement Bylaw, 2018, No. 8622” (Urban Systems / IMH 151 E Keith Apartments Ltd., 151 East Keith Road, CD-702, Rental Housing Commitments) be given third reading.



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**NOTICE OF MOTION**

15. Strategies to Steward Land and Housing Resources – Mayor Buchanan

WHEREAS the City of North Vancouver has the goal to be the healthiest small city in the world;

WHEREAS the City is examining ways to build affordable and mid-market housing to help create a livable community for all our residents;

WHEREAS the City values its limited land resources and remains committed to protecting these resources for generations to come;

WHEREAS the City requires fiscal responsibility with respect to the management of its resources;

AND WHEREAS there exists, in practice, numerous vehicles, approaches and scenarios for achieving Council's healthy community, affordable housing and responsible fiscal management goals and objectives;

NOW THEREFORE BE IT RESOLVED THAT Council direct staff to initiate an examination of the appropriate vehicles and strategies to steward land and housing resources, including but not limited to, a community land trust, non-profit, and community foundation with clearly stated goals, objectives and outcomes.

**COUNCIL REPORT**

Each Council member is permitted 5 minutes to report on their activities.

**PUBLIC CLARIFICATION PERIOD**

The Public Clarification Period is limited to 10 minutes in total and is an opportunity for the public to ask a question regarding process or clarification on an item on the Regular Council Agenda. The Public Clarification Period concludes after 10 minutes and the Regular Council Meeting reconvenes.

**COUNCIL INQUIRIES**

**NEW ITEMS OF BUSINESS**

**NOTICES OF MOTION**

**CITY CLERK'S RECOMMENDATION**

THAT Council recess to the Committee of the Whole, Closed session, pursuant to the *Community Charter*, Section 90(1)(e) [land matter].

**REPORT OF THE COMMITTEE OF THE WHOLE (CLOSED SESSION)**

**ADJOURN**

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**MINUTES OF THE REGULAR MEETING OF COUNCIL HELD IN THE  
COUNCIL CHAMBER, CITY HALL, 141 WEST 14<sup>th</sup> STREET, NORTH  
VANCOUVER, BC, ON **MONDAY, JUNE 10, 2019****

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**PRESENT**

**COUNCIL MEMBERS**

Mayor L. Buchanan  
Councillor H. Back  
Councillor D. Bell  
Councillor A. Girard  
Councillor T. Hu  
Councillor J. McIlroy  
Councillor T. Valente

**STAFF MEMBERS**

L. McCarthy, CAO  
K. Graham, City Clerk  
C. Baird, Deputy City Clerk  
J. Peters, Assistant City Clerk  
B. Pearce, Director, Strategic and Corporate Services  
H. Reinhold, Deputy Director, Strategic and Corporate Services  
C. Nichols, Manager, Human Resources  
D. Pope, Director, Engineering, Parks and Environment  
B. Willock, Manager, Engineering Planning and Design  
B. Themens, Director, Finance  
M. Epp, Director, Planning and Development  
J. Draper, Deputy Director, Planning and Development  
S. Galloway, Manager, Planning and Development  
H. Evans, Community Planner  
L. Vargas, Traffic Engineer  
B. Green, Traffic Engineering Technician  
C. French, Planning Assistant  
L. Orr, Manager, Business and Community Partnerships  
P. Duffy, Manager, Bylaw Services

The meeting was called to order at 6:00 pm.

**APPROVAL OF AGENDA**

Moved by Councillor Bell, seconded by Councillor Girard

1. THAT the Regular Agenda of June 10, 2019 be amended by removing Item #13 – “Integrated Planning Strategy” and the related presentation;

AND THAT the Agenda, as amended, be approved.

**CARRIED UNANIMOUSLY**

**ADOPTION OF MINUTES**

Moved by Councillor Bell, seconded by Councillor Girard

2. Regular Council Meeting Minutes, May 27, 2019

**CARRIED UNANIMOUSLY**

## **PROCLAMATIONS**

Mayor Buchanan declared the following proclamations:

National Health and Fitness Day – June 1, 2019

Built Green Day – June 5, 2019

World Elder Abuse Awareness Day – June 15, 2019

## **PUBLIC INPUT PERIOD**

- Aline Burlone, 818 East 6<sup>th</sup> Street, North Vancouver, spoke regarding the Cloverley Traffic Calming Plan.
- Miles Norman, 739 East 5<sup>th</sup> Street, North Vancouver, spoke regarding the Cloverley Traffic Calming Plan.
- Olav Langelaar, 728 East 5<sup>th</sup> Street, North Vancouver, spoke regarding the Cloverley Traffic Calming Plan.
- Sue Knapp, 605-108 East 14<sup>th</sup> Street, North Vancouver, spoke regarding smoke pollution emissions at 1352 Lonsdale Avenue.

## **CONSENT AGENDA**

Moved by Councillor McIlroy, seconded by Councillor Girard

THAT the recommendations listed within the “Consent Agenda”, be approved.

**CARRIED UNANIMOUSLY**

## **START OF CONSENT AGENDA**

## **REPORTS**

\*3. Statutory Report Required Under Bill 27 – File: 05-1610-01-0001/2019

Report: Director, Finance, May 22, 2019

Moved by Councillor McIlroy, seconded by Councillor Girard

PURSUANT to the report of the Director, Finance, dated May 22, 2019, entitled “Statutory Report Required Under Bill 27”:

THAT the report be received and filed.

**(CARRIED UNANIMOUSLY)**

## **CONSENT AGENDA – CONTINUED**

\*4. 2019 Living City Grants – File: 05-1850-20-0008/2019

Report: Environmental Sustainability Specialist, May 22, 2019

Moved by Councillor McIlroy, seconded by Councillor Girard

PURSUANT to the report of the Environmental Sustainability Specialist, dated May 29, 2019, entitled “2019 Living City Grants”:

THAT 2019 Living City grants totaling \$15,000 be awarded as follows:

<b>Name of Applicant</b>	<b>Amount Recommended</b>
The Cascadia Society for Social Working	\$2,267
North Shore Neighbourhood House, Edible Garden Project	\$3,000
Gerry’s Garden Society	\$1,567
Lower Lonsdale Community Gardens	\$3,000
Lookout Housing & Health Society	\$2,066
SFU, Pacific Water Research Centre	\$1,500
Wildcoast Ecological Society	\$1,600
<b>TOTAL</b>	<b>\$15,000</b>

AND THAT the Coho Society of the North Shore, Fresh Air Learning Society, Ocean Ambassadors Canada and Wild Bird Trust be thanked for their applications and advised that their applications have been denied at this time.

**(CARRIED UNANIMOUSLY)**

## **END OF CONSENT AGENDA**

### **DELEGATION**

Charlotte Ellice, Branch Manager, BC SPCA, West Vancouver Branch

Re: BC SPCA Services and Resources

Charlotte Ellice, Branch Manager, BC SPCA, West Vancouver Branch provided a PowerPoint presentation regarding “BC SPCA Services and Resources” and responded to questions of Council, along with the Manager, Bylaw Services.

## **CORRESPONDENCE**

5. Charlotte Ellice, Branch Manager, BC SPCA, West Vancouver Branch,  
February 28, 2019 – File: 09-3900-01-0001/2019

Re: BC SPCA Services and Resources

Moved by Councillor McIlroy, seconded by Councillor Girard

THAT the correspondence from Charlotte Ellice, Branch Manager, BC SPCA West Vancouver Branch, dated February 28, 2019, regarding “BC SPCA Services and Resources”, be received with thanks;

AND THAT staff be directed to investigate the suggested bylaw amendments raised by the BC SPCA and associated cost implications.

**CARRIED UNANIMOUSLY**

## **DELEGATION**

Murray Mollard, Executive Director, North Shore Community Resources

Re: North Shore Young Citizens’ Forum Engagement

Elizabeth Chick, Hassan Merali and Ali White, Members, North Shore Community Resources, provided a PowerPoint presentation and responded to questions of Council.

## **CORRESPONDENCE**

6. Murray Mollard, Executive Director, North Shore Community Resources,  
May 1, 2019 – File: 10-4710-01-0001/2019

Re: North Shore Young Citizens’ Forum Engagement

Moved by Councillor Bell, seconded by Councillor Hu

THAT the correspondence from Murray Mollard, Executive Director, North Shore Community Resources, dated May 1, 2019, regarding “North Shore Young Citizens’ Forum Engagement”, be received with thanks.

**CARRIED UNANIMOUSLY**

## **REPORTS**

7. North Shore Young Citizens' Forum Report – File: 10-4710-01-0001/2019

Report: Community Planner, and Coordinator, Community Development,  
May 29, 2019

Moved by Councillor Bell, seconded by Mayor Buchanan

PURSUANT to the report of the Community Planner and the Coordinator, Community Development, dated May 29, 2019, entitled “North Shore Young Citizens’ Forum Report”:

THAT Council receive the “North Shore Young Citizens’ Forum Report” and thank the North Shore Young Citizens’ Forum for their work;

THAT Council direct staff to review the recommendations of the North Shore Young Citizens’ Forum Report and bring a report to Council in Fall 2019 with priorities and proposed City actions in response to the recommendations;

AND THAT the City continue to support the efforts and engage with the North Shore Young Citizens Forum regarding civic issues, as available.

**CARRIED UNANIMOUSLY**

8. Community Emergency Preparedness Fund (CEPF) – New Resolution  
– File: 14-7130-01-0001/2019

Report: Director, North Shore Emergency Management, May 30, 2019

Moved by Mayor Buchanan, seconded by Councillor Girard

PURSUANT to the report of the Director, North Shore Emergency Management, dated May 30, 2019, entitled “Community Emergency Preparedness Fund (CEPF) – New Resolution”:

THAT Council reconsider the resolution approved in the Regular meeting of April 1, 2019 regarding CEPF 2018 Evacuation Route Planning;

AND THAT Council support the submission of an application to the 2018 Evacuation Route Planning Grant and overall grant management.

**CARRIED UNANIMOUSLY**



## **REPORTS – CONTINUED**

9. Petition for Local Area Service – Paved Lane Construction East of 1700 Block Fell Avenue – File: 11-5320-01-0001/2019

Report: Manager, Engineering Planning and Design, May 29, 2019

Moved by Councillor McIlroy, seconded by Councillor Valente

PURSUANT to the report of the Manager, Engineering Planning and Design, dated May 29, 2019, entitled “Petition for Local Area Service – Paved Lane Construction East of 1700 Block Fell Avenue”:

THAT “Local Area Service Parcel Tax Bylaw, 2019, No. 8711” (Paved Lane Construction East of the 1700 Block of Fell Avenue) be considered;

THAT (Funding Appropriation #1928) an amount of \$85,000 be appropriated from the Local Area Service Reserve Fund for the purpose of funding construction of a paved lane east of the 1700 block of Fell Avenue;

THAT should any of the amount remain unexpended as at December 31, 2022, the unexpended balance shall be returned to the credit of the Local Area Service Reserve Fund;

AND THAT a stormwater treatment device be installed on the 17<sup>th</sup> Street outfall at an estimated cost of \$75,000 funded from Project 55066 – Stormwater Quality Infrastructure.

**CARRIED UNANIMOUSLY**

## **BYLAW – FIRST, SECOND AND THIRD READINGS**

10. “Local Area Service Parcel Tax Bylaw, 2019, No. 8711”  
(Paved Lane Construction East of the 1700 Block of Fell Avenue)

Moved by Councillor Valente, seconded by Councillor Girard

THAT “Local Area Service Parcel Tax Bylaw, 2019, No. 8711” (Paved Lane Construction East of the 1700 Block of Fell Avenue) be given first and second readings.

**CARRIED UNANIMOUSLY**

Moved by Councillor Valente, seconded by Councillor Girard

THAT “Local Area Service Parcel Tax Bylaw, 2019, No. 8711” (Paved Lane Construction East of the 1700 Block of Fell Avenue) be given third reading.

**CARRIED UNANIMOUSLY**

## **REPORT**

11. Pier 7 Restaurant Food Primary Liquor Licence Amendment – Application for Extended Hours – File: 09-4320-50-0002/2019

Report: Manager, Business and Community Partnerships, May 29, 2019

Moved by Councillor McIlroy, seconded by Councillor Back

PURSUANT to the report of the Manager, Business and Community Partnerships, dated May 29, 2019, entitled “Pier 7 Restaurant Food Primary Liquor Licence Amendment – Application for Extended Hours”:

THAT Council support the proposed change in hours from 12:00 am to 1:00 am on Friday and Saturday nights for the Food Primary Liquor Licence for Pier 7 Restaurant on the basis that:

1. The Pier 7 Restaurant is located within The Shipyards waterfront destination with no adjacent residential uses;
2. The impact on the community will be minimal;
3. The potential for noise is minimal;
4. There were no concerns raised by the public; and
5. The proposed changes will not result in the establishment being operated in a manner that is contrary to its primary purpose;

AND THAT staff be directed to apply the following operational conditions to the Pier 7 Business Licence to limit any potential issues with the change in hours:

1. Post signage asking patrons to respect neighbours when leaving the restaurant;
2. Close the patios at 12:00 am;
3. Turn off music on the patios at 11:00 pm.

**CARRIED UNANIMOUSLY**

## **PRESENTATION**

Cloverley Traffic Calming Plan – Transportation Engineer

The Transportation Engineer and Deputy Director, Planning and Development, provided a PowerPoint presentation and responded to questions of Council.

## **REPORT**

12. Cloverley Traffic Calming Plan – File: 11-5460-30-0008/1

Report: Transportation Engineer, May 29, 2019

Moved by Buchanan, seconded by Councillor McIlroy

PURSUANT to the report of the Transportation Engineer, dated May 29, 2019, entitled “Cloverley Traffic Calming Plan”:

THAT Council receive the report of the Transportation Engineer, dated May 29, 2019, entitled ‘Cloverley Traffic Calming Plan’ for information;

THAT Council direct staff to engage with a third party traffic consulting firm to review current changes to-date and provide recommendations to achieve the objectives outlined in the above report by July 12, 2019;

THAT Council direct staff, in coordination with the Ministry of Transportation and Infrastructure, to determine a preferred location for the installation of a variable message sign for east bound traffic along Keith Road and East 3<sup>rd</sup> Street;

THAT Council direct staff to work with the District of North Vancouver and the Ministry of Transportation and Infrastructure to improve the signal timing at Keith Road and Brooksbank Avenue to allow for better traffic flow and reduce queuing;

THAT Council direct staff to begin traffic management consultation west of Queensbury Avenue and coordinate traffic calming measures with the Cloverley neighbourhood once the new measures are recommended;

THAT Council direct staff to monitor traffic conditions within the Cloverley neighbourhood on a regular basis;

AND THAT Council direct staff to work in partnership with the RCMP and coordinate a plan of enforcement for the Cloverley neighbourhood.

Moved by Councillor Bell, seconded by Mayor Buchan

THAT the motion be amended by adding the following wording to the second active clause:

“, taking into consideration no right turn restrictions from 3:00-6:00 pm onto Keith Road from Adderley, Cloverley, Shavington and Heywood Streets; no left turns from 3:00-6:00 pm onto 3<sup>rd</sup> Street from Heywood Street; and to install signage regarding no exit to Keith Road from 3:00-6:00 pm at the west end of those streets and at the entrance to the lane on 4<sup>th</sup> Street;”

Amendment motion, **CARRIED**

Councillor Back is recorded as voting contrary to the amendment motion.

Main motion, as amended, **CARRIED**

Councillor Back is recorded as voting contrary to the main motion.

## **PRESENTATION**

Integrated Planning Strategy – Manager, Planning and Development, and Deputy Director, Planning and Development

The presentation was removed from the agenda.

## **REPORT**

13. Integrated Planning Strategy – File: 13-6430-01-0001/2019

Report: Manager, Planning and Development, and Deputy Director, Planning and Development, May 29, 2019

Item 13 was removed from the agenda.

## **NOTICE OF MOTION**

14. Child Minding Services During Regular Council Meetings  
– File: 10-4750-01-0001/2019

Submitted by Councillor McIlroy

Moved by Councillor McIlroy, seconded by Mayor Buchanan

WHEREAS the City of North Vancouver is committed to continued improvement of citizen and civic engagement, transparency of City operations and Council decisions, and of the connection between members of Council and community:

THEREFORE BE IT RESOLVED THAT staff be directed to report back to Council on the feasibility and cost of child minding services being available at City Hall during Regular Council meetings.

**CARRIED UNANIMOUSLY**

## **PUBLIC CLARIFICATION PERIOD**

Nil.

## **COUNCIL INQUIRIES**

Nil.

## **NEW ITEMS OF BUSINESS**

Nil.

## **NOTICES OF MOTION**

Nil.

## **ADJOURN**

Moved by Councillor Back, seconded by Councillor McIlroy

THAT the meeting adjourn.

**CARRIED UNANIMOUSLY**

The meeting adjourned at 8:06 pm.

*“Certified Correct by the City Clerk”*

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CITY CLERK



**MINUTES OF THE SPECIAL REGULAR MEETING OF COUNCIL HELD IN  
CONFERENCE ROOM A/B, CITY HALL, 141 WEST 14<sup>TH</sup> STREET, NORTH  
VANCOUVER, BC, ON **TUESDAY, JUNE 11, 2019****

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**PRESENT**

**COUNCIL MEMBERS**

Mayor L. Buchanan  
Councillor H. Back  
Councillor D. Bell  
Councillor A. Girard  
Councillor T. Hu  
Councillor J. McIlroy  
Councillor T. Valente

**STAFF MEMBERS**

L. McCarthy, CAO  
K. Graham, City Clerk  
M. Epp, Director, Planning and Development  
R. Skene, Manager, Facilities and Real Estate  
T. Huckell, Committee Clerk/Secretary

The meeting was called to order at 7:46 pm.

**CITY CLERK'S RECOMMENDATION:**

Moved by Mayor Buchanan, seconded by Councillor Bell

THAT Council recess to the Special Committee of the Whole, Closed session,  
pursuant to the *Community Charter*, Section 90(1)(e) [land matter].

**CARRIED UNANIMOUSLY**

**ADJOURN IN CLOSED SESSION**




The meeting recessed at 7:47 pm and did not reconvene. A motion to adjourn was approved in the Committee of the Whole, Closed session, at 7:49 pm.

*"Certified Correct by the City Clerk"*

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CITY CLERK

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 Division Manager	 Director	 CAO
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The Corporation of **THE CITY OF NORTH VANCOUVER**  
**PLANNING & DEVELOPMENT DEPARTMENT**

**INFORMATION REPORT**

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To: Mayor Linda Buchanan and Members of Council

From: Larisa Lensink, Environmental Sustainability Specialist

Subject: 2019 COOL IT! CHALLENGE WINNERS

Date: June 5, 2019 File No: 11-5280-15-0001/2019

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**PURPOSE:**

The purpose of this report is to advise Council of the 2019 winners of the Cool It! Climate Leadership Training program delivered at local schools in partnership with the B.C. Sustainable Energy Association (BCSEA).

**DISCUSSION:**

The Cool It! Climate Leadership Training program engages City youth and their families on climate change and energy conservation, and empowers them to take action to reduce their household greenhouse gas emissions. The program results directly contribute to the City's community-wide emissions reduction targets.

**The Program**

The BCSEA ([www.bcsea.org](http://www.bcsea.org)) is a non-profit organization committed to promoting the adoption of sustainable energy and energy conservation in British Columbia. The BCSEA's Cool It! Program includes three main activities for students:

- 1) Educational materials and preparatory exercises for teachers to introduce their students to the key ideas;
- 2) A workshop delivered by Environmental Educators with thought-provoking discussion, interactive activities and animated videos to encourage students to connect the choices they make today with the futures they will build; and
- 3) A take-home contest involving the entire family in which they participate in behaviour changing activities that reduce their environmental impact for one month, such as energy and waste reduction and transportation alternatives.



In the spring 2019 term, five workshops were delivered to 122 students in grades four to seven at four elementary schools.

### **Greenhouse Gas Savings Achieved**

A major strength of the Cool It! program is that the contest is supported by pre- and post-workshop surveys, which are used to calculate the GHG emissions savings of students and families participating in the program. Overall, the spring 2019 program resulted in the projected savings of **83 tonnes** of carbon dioxide emissions, equivalent to taking 18 passenger vehicles off the road for a year. Lasting additional benefits are anticipated as students and their families sustain these behaviour changes.

### **Contest Winner**

All contest participants worked extremely hard to reduce their GHG emissions. The top prize for class-wide reductions went to **Ms. Cynthia Whyte's Grade 3/4 class at Larson Elementary** who reduced their emissions by **22 tonnes**, equivalent to the annual emissions of 5 passenger vehicles.

### **CONCLUSION:**

A number of outreach initiatives are currently underway in support of the City's energy and emission reduction goals. The BCSEA's Cool It! Climate Leadership Training program workshops are proving to be effective in achieving measurable emissions reductions in the community.

RESPECTFULLY SUBMITTED:



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Larisa Lensink, MREM  
Environmental Sustainability Specialist



 Division Manager	 Director	 CAO
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The Corporation of **THE CITY OF NORTH VANCOUVER**  
**PLANNING & DEVELOPMENT DEPARTMENT**

**INFORMATION REPORT**

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To: Mayor Linda Buchanan and Members of Council

From: Larisa Lensink, Environmental Sustainability Specialist

Subject: 2019 LIVING CITY AWARD RECIPIENTS

Date: June 5, 2019 File No: 01-0290-20-0005/2019

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**PURPOSE:**

The purpose of this report is to advise Council of the 2019 Living City Award winners.

**DISCUSSION:**

The Living City Award Program recognizes the efforts of individuals, schools, community groups and businesses demonstrating outstanding leadership in environmental sustainability. The awards are an important part of the City's environmental sustainability program and provide a means of supporting and fostering environmental leadership.

Nominations received for the 2019 Living City Awards were reviewed by members of the Advisory Planning Commission on May 8, 2019. The Commission unanimously recommended that the awards be given to the following recipients:

**North Shore Community Garden Society**

Founded in 2009, the North Shore Community Garden Society oversees the use and management of six community gardens in the City and District of North Vancouver. The Society fosters a sense of community at each of the gardens, where members contribute time and effort towards garden maintenance and learn together about sustainable gardening. The Society encourages members to grow organic produce and share the abundance with neighbours and the food bank. The gardens are showpieces in the community as greenspace for residents to enjoy and learn about urban agriculture. The Society's garden coordinators are gentle, informative and effective guides, and empower members to grow their own food in the City.



**David Roberts, Kathy Fung and Matt Dowds**

Owners of a 1912 craftsman bungalow in Grand Boulevard, David Roberts and Kathy Fung, pursued significant energy efficiency and green building upgrades to their home while preserving its heritage history. North Vancouver builder, Matt Dowds of Dowds Homes, used advanced buildings techniques and systems to realize the owners' vision. The home achieved Passive House Plus certification due to its high energy efficiency and generation of renewable energy through solar panels, and has low greenhouse gas emissions. Green building techniques were also used to repurpose old growth fir from the heritage structure and integrate water conservation features in the garden and driveway design, reducing its environmental impact. The initiative of Dave and Kathy, and expert work of Matt to create this high performance and low impact home, while preserving heritage elements, warrants recognition.

**Christine Miller and Luci Cadman**

In their respective roles as Executive Director and Education Coordinator with the North Shore Black Bear Society, Christine Miller and Luci Cadman work tirelessly to educate residents about attractant management to reduce human-bear conflict in the North Shore. Under Christine and Luci's leadership, the North Shore Black Bear Society canvasses high conflict areas and responds to inquiries from residents through a hotline and email, increasing awareness of bears and bear proofing in the community. Christine and Luci provide bear and wildlife education to residents at City events, to children and youth through school presentations, and to newcomers to Canada through visits to MOSAIC. The outstanding work of Christine and Luci through the North Shore Black Bear Society helps residents and visitors safely enjoy both the urban and natural assets of the North Shore.

**Sustainable Produce Urban Delivery**

Sustainable Produce Urban Delivery (SPUD) goes above and beyond to reduce and reuse packaging in their grocery orders to residents across B.C., including many in the City of North Vancouver. Groceries are delivered in reusable bins with reusable freezer jackets and cold packs, and minimal single-use packaging. SPUD recently piloted a reusable takeout container program where plastic containers were given to members to use for takeout food orders, and returned to SPUD with the reusable grocery bins to be washed before being given back to members to use again. The plastic containers used in the program were made from 50% recycled plastic as a lower-impact alternative to virgin plastic containers. Due to its success, SPUD is expanding the program to all members and hopes to inspire other food delivery services to transition to a circular economy model for packaging. SPUD strives to be a leader in sustainability and demonstrate their environmental values through innovative programs that lead by example in the community.

RESPECTFULLY SUBMITTED:



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Larisa Lensink, MREM  
Environmental Sustainability Specialist

# THE CORPORATION OF THE CITY OF NORTH VANCOUVER

## BYLAW NO. 8711

### **A Bylaw to authorize the establishment of a local area service for the construction of paved lane east of the 1700 Block of Fell Avenue.**

**WHEREAS** Ms. Pat Hodgson and others have petitioned Council to construct, as a local area service, the work hereinafter described, and the City Clerk has certified that the petition is sufficient and it is expedient to grant this request of the petition in the manner hereinafter provided:

**THEREFORE** the Council of The Corporation of the City of North Vancouver, in open meeting assembled, enacts as follows:

1. This Bylaw shall be known and cited for all purposes as “**Local Area Service Parcel Tax Bylaw, 2019, No. 8711**” (**Paved Lane Construction East of the 1700 Block of Fell Avenue**).
2. That the service will be the construction of a paved lane, to serve the east side of the 1700 Block of Fell Avenue between East 17<sup>th</sup> Street and the dead end north, being a total length of 108 meters, as a local area service under the provisions of Division 5 of the *Community Charter*.
3. That the City Engineer make such plans, profiles and specifications and furnish such information as may be necessary for the making of a contract for the execution of the work.
4. The work shall be carried on and executed by the City under the superintendence and according to the directions and orders of the City Engineer.
5. The cost of the work will be recovered as follows:

The owners' portion of the cost of the service shall be \$5,375.43 and will be recovered by a parcel tax to be levied on all properties in the local area over a period of ten years.

Any person whose lot is specially assessed may pay any annual installment of the frontage-tax imposed thereon, at any time before such installment falls due, and the Director of Finance is authorized to make the necessary adjustment in the interest charge in such cases.

READ a first time on the 10<sup>th</sup> day of June, 2019.

READ a second time on the 10<sup>th</sup> day of June, 2019.

READ a third time on the 10<sup>th</sup> day of June, 2019.

ADOPTED on the <> day of <>, 2019.

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MAYOR

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CITY CLERK



## U11 NVFC Euro Travel Team 2019

City of North Vancouver Delegation Meeting, Monday, June 17<sup>th</sup>

Presenters: Righ Walker, Ephraim Claro,

Finn Montgomery, and Szilard Puddiford



## NVFC & U11 Euro



- North Vancouver Football Club (NVFC) provided an opportunity for 13 elite North Vancouver U11 players to travel to two of the top soccer countries in the world – Spain (Valencia) and Italy (Verona)
- NVFC has approximately 4,000 players (from tots to adults) in our club, and when you consider their siblings, parents, grandparents, and friends that support our Club, the number of people on the North Shore connected to the Club is phenomenal.
- *NVFC's mission is to develop human potential through soccer. The Club's values are: Community, Accessibility, Progressiveness and Health.*

+ City of North Vancouver Delegation Meeting

## Itinerary



May 3 Arrive in Valencia

May 4 Training & Tour Valencia

May 5 La Liga: Levante vs Rayo Vallecano

May 6 Tour Villarreal facility, NVFC Euro '08 vs Villarreal CF

May 7 NVFC Euro '08 vs CD Benicasim, NVFC Euro '08 vs CD Castellon

May 8 To Bergamo, Italy - Training with AC Milan

May 9 NVFC Euro '08 vs U.S. Cremonese

May 10 Tour Padova, NVFC Euro '08 vs A.S. Cittadella

May 11 Serie B: Hellas Verona vs Foggia

May 12 Tour Sirmione & Verona

May 13 NVFC Euro '08 vs FeralpiSalo

May 14 NVFC Euro '08 vs Brescia Calcio

May 15 NVFC Euro '08 vs A.C.D. Sedriano



+ City of North Vancouver Delegation Meeting

## Overview



- Trip was over and above regular NVFC programming
- All families wanted their respective kids to earn their trip. They did.
- Great experience, lots of learning – about soccer as well as culture.

+

City of North Vancouver Delegation Meeting

## Community Support & Earning Our Trip



- We received so much support from family, friends, neighbours, people throughout North Vancouver who bought raffle tickets
- And so much support from local businesses that contributed auction items and donations
- Every player had minimum targets to reach to share in fundraising pool – ALL players worked hard and met their goals
- In addition, we are thankful for the contribution the City of North Vancouver made – it is very much appreciated.
- When we played there, it felt that we had our whole community supporting us.

# + City of North Vancouver Delegation Meeting

## Community Support & Earning Our Trip



+ City of North Vancouver Delegation Meeting

## Competition & Facilities



- We played some really good teams!
- Of our 8 games, we lost our first 6 and then won our last two.
- But even in the games we lost, we had fun – and we got better as our trip went on.
- We faced some pretty good academies – the teams were good, their fields and clubhouses were good, and our parents liked it because most had clubhouses with food and drinks.
- We also had a chance to train with pro coaches and meet pro players!



+ City of North Vancouver Delegation Meeting

# Competition & Learnings



## What We Learned



- If we want to compete against these teams, we need to practice more, and help our club become even better
- Outside of soccer, we found out that Spain and Italy were really nice places to visit. Everyone was super friendly.
- The Oranges were amazing in Spain. The Spaghetti Bolognese was amazing in Italy.
- For kids that are in French immersion, we found out that a lot of Spanish and Italian people can speak French – so communicating with locals was easier than we expected.



# + City of North Vancouver Delegation Meeting

## What We Learned



+

## City of North Vancouver Delegation Meeting

### Would we recommend this trip for next year's group?



- We're better soccer players because of it. And we're determined to get even better.
- We know if we want next year's team or any future years' team to do better than us, we can help make our club better by training hard and supporting our other teams.
- It was also a great feeling to be play in Europe knowing how many people supported us.
- Having the Canadian flag on the field with us and sharing North Vancouver pins with the other teams was a special feeling.



+ City of North Vancouver Delegation Meeting

Would we recommend this trip for next year's group?



## + City of North Vancouver Delegation Meeting

### Summary



- It was a great experience
- The boys worked hard and benefited from community support
- Lots of learning on how we can make our club better – from facilities and training, from inclusion and accessibility.
- Thank you to the City of North Vancouver for your support, we hope you continue to support future teams!

**Karla Graham**

---

**Subject:** NVFC Euro Team Delegation

From: Darla Johnston  
Sent: February-09-19 2:02 PM  
To: Karla Graham  
Subject: NVFC Euro Team Delegation

Hi Karla,

Just to follow up from our conversation on Thursday.

The NVFC Euro Team would like to present to council at the end of June as a Delegation.

This will give the team a chance to thank council for the donation and show them what an amazing trip they had.

A great idea to compensate for the miscommunication and missing the meeting.

Thanks for your help,

Darla Johnston  
NVFC Euro Team, Parent Volunteer  
628 West 17th Street, North Vancouver, BC



City of North Vancouver  
Delegation to Council  
June 17, 2017



NORTH SHORE



*The right to wind in their hair.*



# Enabling Service

- Base in Upper Lonsdale.
- Base in Lower Lonsdale.
- Classification – Mobility Aid.
- Alternative route – 21<sup>st</sup> W of Chesterfield.



NORTH SHORE

Please help mobility challenged people and  
the Mikes of North Vancouver to enjoy:

*The right to wind in their hair!*

Thank you.



NORTH SHORE

## Christine Baird

---

**Subject:**

FW: FW: Cycling Without Age North Shore - Delegation to Council

**From:** Ian Rose-Innes <[ian@cyclingwithoutage.ca](mailto:ian@cyclingwithoutage.ca)>

**Sent:** April-11-19 12:16 PM

**To:** Karla Graham <[kgraham@cnv.org](mailto:kgraham@cnv.org)>

**Cc:** Annwen Loverin <[annwen@silverharbourcentre.com](mailto:annwen@silverharbourcentre.com)>

**Subject:** Cycling Without Age North Shore - Delegation to Council

Good day Karla,

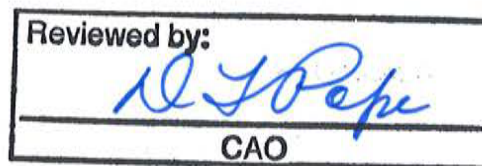
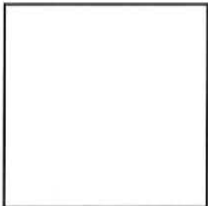
Annwen Loverin, the Executive Director at Silver Harbour, and I have returned this morning from a meeting with Mayor Linda Buchanan. She suggested that we request an opportunity to present the Cycling Without Age North Shore organization's plans and needs for support to Council. This support does not include a request for funding.

Please can you advise when an opportunity exists for this delegation to address Council?

Many thanks,

Kind regards,

Ian Rose-Innes







***NORTH SHORE***

Cycling Without Age North Shore  
c/o Silver Harbour Seniors' Activity  
Centre  
144 East 22nd Street  
North Vancouver, BC  
V7L 4L5

[northshore@cyclingwithoutage.ca](mailto:northshore@cyclingwithoutage.ca)

Mayor and Council.

City of North Vancouver,

141 West 14th Street,

North Vancouver,

BC V7M 1H9

**Re: Presentation to Council – June 17, 2019**

Your Worship,

We respectfully request an opportunity to present the current initiative to establish Cycling Without Age on the North Shore to yourself and Council on the subject date.

Yours truly,

Ian Rose-Innes – Director

**City of North Vancouver**

**Council Meeting June 17, 2019**

**Draft Resolution Proposed by Cycling Without Age North Shore**

**BACKGROUND:**

Cycling Without Age North Shore plans to offer rides to mobility challenged people living in care homes and in their own homes. These rides would desirably take place around the Green Necklace and along the Spirit Trail. Bases are required where a trishaw can be stored and batteries can be recharged. These should be near the Green Necklace or Spirit Trail and close to where persons transporting the mobility challenged person would go to undertake activities, such as an exercise class or shopping, while the mobility challenged person enjoys a ride on a trishaw. Seating and refreshments should be close at hand.

Classification of a trishaw as a mobility aid would allow it to traverse a sidewalk, where the road is unsafe, when transporting passengers fetched from their own homes or to visit children's' play parks.

A short section of the Green Necklace, in 21st Street immediately West of Chesterfield Avenue, is too steep to allow the trishaws to navigate it. One solution is to make a deviation to 19<sup>th</sup> Street and a return along the West sidewalk of Chesterfield Avenue.

To enable these activities the following is needed:

**In Upper Lonsdale – A base** with the characteristics described above near the Green Necklace.

**In Lower Lonsdale – A base** with the characteristics described above near the Spirit Trail.

**Classification of a Cycling Without Age trishaw as a mobility aid** to allow occasional use of sidewalks where it is unsafe to ride in the road together with motorized vehicles.

**An alternative route** to avoid the steep section of trail in 21st Street.

**RECOMMENDATION:**

**PURSUANT** to the above considerations.

**THAT** staff identify sites suitable as operating bases for Cycling Without Age North Shore in Upper and Lower Lonsdale.

**THAT** staff investigate the classification of Cycling Without Age trishaws as mobility aids to allow them to traverse sidewalks.

**AND THAT** staff investigate a route to allow trishaws to navigate around steep sections of the Green Necklace.

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# Rail~Volution

Building Livable Communities  
with Transit

September 8 – 11, 2019 ▼ Hyatt Regency Vancouver

[railvolution.org](http://railvolution.org)

**VANCOUVER, BC, CANADA**

[#railvolution](https://twitter.com/railvolution)





## **A multimodal, livable city & region**

### **Rail~Volution Vancouver**

**September 8 – 11, 2019**

Hyatt Regency Vancouver

## **Metro Vancouver**

**Connected by nature. Livable by design.**

A band of cities in a sea of green. Come to Vancouver and see how intentional planning and development have shaped not only downtown, but suburban locations. Consistently rated one of the most livable places in the world, the region is a perfect laboratory for the 25th annual Rail~Volution conference.

Metro Vancouver showcases dense, mixed-used development and abundant public and green spaces, all served by multimodal transportation options. The region is a global gateway and the largest port in Canada – a place immigrants, as well as First Nations communities, call home. Metro Vancouver captures the arc of a new century: defining the future of mobility and development, even as it grapples with the challenges of climate change, affordability, imminent automation, and competition between modes.

**Comparable in size to Charlotte, San Antonio or Pittsburgh, Metro Vancouver led all US and Canadian cities in transit ridership growth in 2017.**

Half of all new development occurs within the region's Frequent Transit Network, which includes the autonomous SkyTrain system. Vancouver strives to integrate all modes and new development, with approximately US\$5.2 billion (CA\$7 billion) of investment in transit corridors, bus expansion, transit stations, roads, cycling and pedestrian infrastructure.





# Rail~Volution

Building Livable Communities  
with Transit

TransLink

**Join us in this spectacular setting.**

The annual Rail~Volution conference focuses on the interplay of transportation (especially transit and related multimodal options), land use and development in building livable communities. Join more than 1,200 practitioners, leaders and advocates from communities across the United States and Canada. Share best practices, real-world solutions and innovative approaches to the challenges that face cities and regions of every size.

**Transit. Land Use. Community Development.**

# Join us in Metro Vancouver

Connect with livability professionals from across the United States, Canada and the world.

## Who Attends Rail~Volution?

Local, regional, national and international stakeholders involved in building livable communities

transit and multimodal planners

business, nonprofit and community leaders

elected officials

activists

community-based organizations

developers

transit agencies

transit-oriented development (TOD) managers

new mobility and technology innovators

transportation engineers

architects

urban designers

consultants

communications and engagement professionals

housing and transportation advocates

financial professionals

foundation officers ... and you!

## Join them and deepen your knowledge of:

Transit planning, multimodal planning, corridor planning, station area planning and design.

Project management and construction management for different modes.

Technology and integration of new mobility options with transit.

Community engagement, outreach tools, communication, data, campaigns and marketing. Policies and strategies to combat displacement.

Transit-oriented development, trail- and bike-oriented development, joint development, housing and neighborhood development and placemaking.

Community development financing, capital financing, gap financing, affordable housing, advocacy for transit, transit equity.





**97%**

**OF ATTENDEES SURVEYED SAY THE  
RAIL~VOLUTION 2018 CONFERENCE  
WAS EXCELLENT OR GOOD.**

## Here's what else they say:

“

Each year we meet people who are further along in the process than we are. It is helpful to learn from their experiences – good and bad.”

“

I actually think I most appreciated the sessions that raised questions in my mind ... How do we accommodate transportation modes that occupy physical space really differently than our current modes? Considering questions like that is super-helpful as we consider our options moving forward.”

“

I learned about a number of exciting things happening in other cities that I'd like to make happen in my own.”

“

I enjoy interacting with the built environment of the host city. I love that Rail~Volution tends to explore a vast diversity of cities that all contribute to the educational process.”



## How do attendees put their conference experience to work?

### Here's what 2018 attendees said:

Re-evaluate infrastructure approaches and planning priorities for our agency.

Use the tools and networks to enhance my ability to do meaningful public engagement work related to greener forms of transportation.

Guide my professional work with technical information learned from attending sessions.

Influence transit-oriented development in my community.

Use strategies regarding public-private partnerships.

Explore new revenue streams for transit in my community.

Bring information from the outreach sessions to my own work.

Connect with new people for future business opportunities.

Bring new ideas and examples of what works in other places to the work I do.

Explore new pilot programs based on presentations at the conference.

Make more-informed decisions as a transit board member.

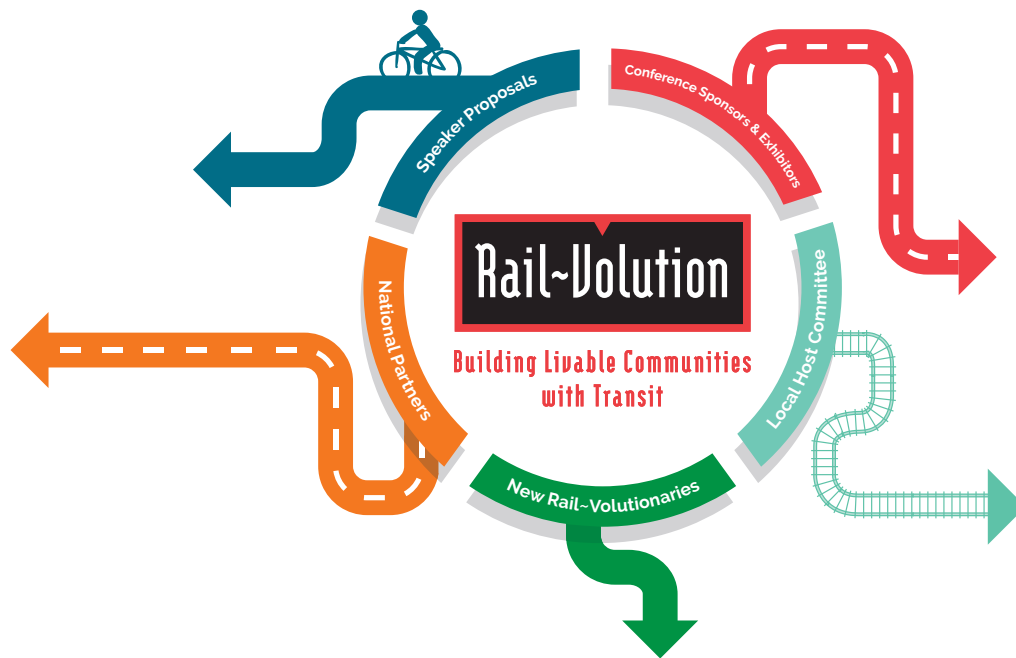
Continue to learn about scooter and bike-share programs for my station design.

Meet with legislators and agencies and discuss transit.



# Conference Elements

**27 mobile workshops, 75+ sessions, plenaries, networking and social events.**



**From new professionals to established experts: Perspectives that challenge. Information you can use.**

The annual Rail~Volution conference is the product of our network. Rail~Volution Partners – livability professionals from across the US and Canada – put together conference sessions, drawing on the 350+ topics and presenters submitted in our Call for Speakers. They work to include a diversity of perspectives, geographies and backgrounds. The local host committee organizes mobile workshops, pre-conference events, the Welcome Reception and regional day. The New Rail~Volutionaries, a group formed to welcome new leaders in the livability movement, organizes networking events and innovative learning opportunities.





# Rail~Volution 2019

## Metro Vancouver

At this year's conference (the first outside the US) we'll learn from a global leader in livability, planning, development and urban design. The Vancouver region will share its stories through pre-conference events, the Welcome Reception, 27 Mobile Workshops and Vancouver Regional Day.

**We're excited to have the support of this terrific local host committee for the 2019 conference:**

### Co-Chairs

Sarah Ross, **TransLink**

Gary Andrishak, **IBI Group**

Mark Seinen, **TransLink**

June Kinloch, **IBI Group**

Michelle Babiuk, **TransLink**

Iona Bonamis, **City of Vancouver**



Defining the cities  
of tomorrow



The original artwork for  
Rail~Volution Vancouver is by:

## Susan Point

*a Musqueam Coast Salish artist*

I have attempted to capture the essence of Vancouver while embracing all of our livability features, within my unique style and point of view. By characterizing the tall towers with many windows front and center within my design, I am using them as a metaphor of windows to the past, present and future."

– **Susan Point**

Read her full artist statement  
at [railrevolution.org/news](http://railrevolution.org/news)



TransLink



TransLink

## See and Learn in Metro Vancouver

New development centered on transit across the region

Cities of Vancouver, Burnaby, Surrey, North Vancouver, Richmond and more

Innovative approaches to affordable housing and gentrification

First Nations community planning

Tower-podium built form of “Vancouverism”

Greenways and nearby nature (thanks to intentional planning)

Bus, BRT and rail networks and the busiest bus corridor in North America!

SkyTrain – autonomous since 1985

SeaBus – transit to North Vancouver

Multimodal design, including car and bike share (but no ride-hailing – yet)

Station design, upgrades and integration of modes

The only freeway-free downtown in North America

Placemaking and tactical urbanism

Walkable neighborhoods

Innovative urban design

### SUCCESS!

In 2012, the City of Vancouver set a goal that 50% of all trips would be on sustainable modes by 2020. They reached the goal in 2015! Region-wide, 27% of trips are by transit, walking, bicycle or sharing.



TransLink



# Rail~Volution

## 25 Years Strong

Over the past 25 years, the world has changed around us – and because of us. As we look to the next 25 years and experience firsthand the intentional decisions that have made Metro Vancouver a global leader in livability, it's time to both reaffirm core values and ask fundamental questions about our communities – but in a new way.

**Transportation decisions affect our land uses; the choices we make about mobility and development can either make places better or contribute to decline.**

**How can we make our communities develop and grow while still maintaining the quality of life and character that all residents enjoy and value?**

### RAIL~VOLUTION IS:



#### A MOVEMENT

Our roots are in transit, but our movement encompasses a wide range of livability, equity and emerging mobility issues.



#### A NETWORK

We work across fields, at the cutting edge of land use, housing, community development and transportation. Each year, more than 1,200 people from 300 communities come together to share ideas, breakthroughs, frustrations and inspiration.



#### A RESOURCE

The four days of the Rail~Volution conference provide a diversity of ideas and approaches that boost your capacity to collaborate, innovate, communicate and lead.

We believe major transit investments are transformative, with the potential to make communities stronger — economically, socially and environmentally.

**Come to Rail~Volution to shape the next revolution in building livable communities.**

Check [www.railvolution.org](http://www.railvolution.org) for complete conference and registration information.

Also check out the Rail~Volution podcast, resources and news.

## Friday, September 6

5:00 PM	
6:00 PM	Pre-Conference Events
7:00 PM	
8:00 PM	

All Aboard  
The Polar  
West Coast  
Express

## Saturday, September 7

8:00 AM	
9:00 AM	
10:00 AM	
11:00 AM	
12:00 PM	Conference Registration Open
1:00 PM	
2:00 PM	
3:00 PM	
4:00 PM	
5:00 PM	
6:00 PM	
7:00 PM	New Rail-Volutionaries Welcome Event
8:00 PM	
9:00 PM	

Victoria:  
The Inner  
Harbour  
(overnight)

A Moveable  
Feast: The  
Richmond  
Night  
Market

## Curate Your Conference

Boost your networks – both social and professional – at pre-conference events!

75+ workshop sessions & 3 plenary sessions

27 Mobile Workshops – Sunday through Wednesday!

**New Rail-Volutionaries know how to network!** Don't miss PK Slam + Mentorship Speed Networking + Idea Exchanges + R-V 101.

## Sunday, September 8

7:00 AM	Conference Registration Open 7:00 am - 6:00 pm
8:00 AM	
9:00 AM	
10:00 AM	
11:00 AM	
12:00 PM	
1:00 PM	Mobile Workshops 1 – 3
2:00 PM	
3:00 PM	
4:00 PM	
5:00 PM	
6:00 PM	
7:00 PM	Welcome Reception

Grouse  
Grind: Nature's  
StairMaster to  
Breathtaking  
Views

Edge  
of the City:  
Suspension  
Bridge and  
Gondola

Whistler  
Village: The  
West Coast's  
Summer &  
Winter  
Resort

## Monday, September 9

7:00 AM	Conference Registration Open 7:00 am - 6:00 pm
8:00 AM	Continental Breakfast
9:00 AM	Opening Plenary
10:00 AM	
11:00 AM	
12:00 PM	
1:00 PM	
2:00 PM	
3:00 PM	
4:00 PM	
5:00 PM	
6:00 PM	

Idea Exchanges  
with the New  
Rail-Volutionaries

Lunchtime  
Workshops,  
Including TOD  
Managers' Forum

Consultant's  
Corner

## Rail-Volution

Building Livable Communities  
with Transit

## Conference at a Glance

### Tuesday, September 10

7:00 AM	Conference Registration Open 7:00 am - 6:00 pm
8:00 AM	Continental Breakfast
9:00 AM	Plenary Session
10:00 AM	
11:00 AM	
12:00 PM	
1:00 PM	
2:00 PM	
3:00 PM	
4:00 PM	
5:00 PM	
6:00 PM	
7:00 PM	
8:00 PM	
9:00 PM	
10:00 PM	

Lunch on  
Your Own

Consultant's  
Corner

### Wednesday, September 11

7:00 AM	Conference Registration Open 7:00 am - noon
8:00 AM	PK Slam Recovery Run
9:00 AM	Continental Breakfast
10:00 AM	Morning Workshops
11:00 AM	
12:00 PM	Closing Plenary & Plated Lunch
1:00 PM	
2:00 PM	
3:00 PM	
4:00 PM	
5:00 PM	

A New  
Rail-Volutionaries  
Event



The Polygon Gallery



The Polygon Gallery

## Welcome Reception at The Polygon Gallery

Sip a cocktail and watch the sunset from Metro Vancouver's newest art space, The Polygon Gallery. Start your evening amidst sweeping views of skyline and mountains during your 12-minute SeaBus crossing to the historic Lonsdale Quay Market, steps away from the gallery. The Polygon, designed by Patkau Architects, is the architectural heart of the Shipyard redevelopment transforming North Vancouver's waterfront by integrating mixed-use development with working shipyards.

**DATE:** Sunday, September 8  
**TIME:** 7:00 pm – late

## Public Square, Exhibitor Showcase and Reception

It's all happening in the Rail-Volution Public Square. Breakfast, breaks and box lunch pickup. Creative activities. Exhibitor booths and presentations. Live podcast interviews. The Square is buzzing throughout the conference. Wrap up your Monday at the Exhibitor Reception at the Square, complete with Mentorship Speed Networking with the New Rail-Volutionaries.

## Vancouver Regional Day

Regional day programming provides an opportunity for the local region to delve into issues that specifically affect them. Local conversations and local solutions. Open to the public - separate fees apply. Stay tuned for details later this summer.



Karen Lee Photography



# WHAT BROAD TOPICS WILL YOU ▼ EXPLORE AT RAIL~VOLUTION 2019?

## **INNOVATIONS IN MOBILITY**

First- and last-mile solutions, micromobility and shared-use mobility, innovative and on-demand partnerships, street design, big data, transit-network companies (TNCs) and ride-hailing, innovation and disruption, hyperloop.

## **TRANSIT-ORIENTED COMMUNITIES**

Transit-oriented development (TOD) in multiple settings (urban infill to corridors to historic suburbs), policies, planning for growth, Developers Roundtable, financing, fundamentals of real estate, value capture, public-private partnerships (P3s), TOD Managers Forum, best practices, placemaking, mixing uses, parking.

## **PROMOTING EQUITABLE COMMUNITIES**

Mobility justice, community engagement, equity and service evaluation, equity as a decision-making tool, innovations in housing, new partnerships, displacement and gentrification, zoning, placemaking, women-friendly cities, diversity of voices.

## **THE EVOLVING TRANSIT AGENCY**

Project funding, ridership strategies, affordable housing, climate change, homelessness, Communications and Outreach Managers Forum, social media, safety, innovative governance, next-generation leadership, equity and enforcement, TNCs.

## **COMMUNITY NEEDS SHAPING TRANSIT**

Station design, right of way, BRT, streetcars, electric buses, high-speed rail, light rail, wayfinding, first- and last-mile with walking and bicycling, universal design, complete streets, planning processes, outreach tools, tactical implementation, making the case, mobility hubs.

## **HOW DO YOU LIKE TO LEARN?**

Workshops provide many different ways to learn about a wide variety of ideas: debates, discussions, panels, real-life case studies, Q & As, success (or failure) stories. You don't just attend, you participate!

## **AICP & CIP Certification Maintenance**

The American Institute of Certified Planners and the American Planning Association approve close to 200 hours of Rail~Volution workshops and sessions for certification maintenance credits each year. The conference also is eligible for Canadian Institute of Planning Organized/Structured CPL (continuous professional learning) units. Qualification for this year's workshops is underway. Details available on our website in late summer.

# Mobile Workshops

Get the Rail~Volution perspective on Metro Vancouver. Explore first hand Vancouver and several neighboring communities – all reshaping themselves with transit and multimodal options. Learn how they're creating compact, diverse, people-friendly communities.

Sunday through Wednesday across Metro Vancouver.  
Separate fees apply.

See full descriptions of Mobile Workshops at:  
[www.railvolution.org/mobileworkshops](http://www.railvolution.org/mobileworkshops)

## SUNDAY, SEPTEMBER 8

### 1 Rail to Trail: Cycling the Arbutus Greenway

**TIME:** 10:30 am – 1:00 pm

**MODE:** Bike

**FEE:** US\$70

### 2 The Art of Transit

**TIME:** 11:00 am – 3:00 pm

**MODE:** SkyTrain + Walking

**FEE:** US\$45 (Includes lunch)

### 3 Vancouver's Worst Zoning

**TIME:** noon – 4:00 pm

**MODE:** Bus + Walking

**FEE:** US\$45

## MONDAY, SEPTEMBER 9

### 4 Build It and They Will Come: Cycling for Everyone

**TIME:** 10:00 am – 1:00 pm

**MODE:** Bike

**FEE:** US\$70 (Includes lunch)

### 5 More Complex than Mortar and Brick: Understanding Heritage in the Changing City

**TIME:** 10:00 am – 3:00 pm

**MODE:** Charter Bus + Walking

**FEE:** US\$45 (Includes lunch)

### 6 Jazzing Up the Bus Exchange

**TIME:** 10:30 am – 2:30 pm

**MODE:** Charter Bus

**FEE:** US\$45 (Includes lunch)



**7 Driving the Driverless Train:  
The SkyTrain OMC****TIME:** 11:00 am – 3:00 pm**MODE:** SkyTrain + Walking**FEE:** US\$45 (Includes lunch)**8 Coquitlam Centre: The Past  
and Future****TIME:** 11:30 am – 4:30 pm**MODE:** SkyTrain + Walking**FEE:** US\$60 (Includes lunch)**9 Vancouverism: Past, Present  
and Future****TIME:** noon – 4:00 pm**MODE:** SkyTrain + Walking**FEE:** US\$60 (Includes lunch)**10 Livable + Workable? Planning for  
Goods, Industry and People****TIME:** noon – 4:30 pm**MODE:** SkyTrain + Charter Bus + Walking**FEE:** US\$60 (Includes lunch)**11 A Walkable and Equitable City:  
Can it be Done?****TIME:** 12:30 pm – 4:30 pm**MODE:** SkyTrain + Walking**FEE:** US\$45**12 Granville Island: Parking, Parks  
and the Journey****TIME:** 1:00 pm – 4:30 pm**MODE:** Bus + Ferry + Walking**FEE:** US\$45**13 Musqueam First Nation:  
Recognizing Indigenous  
Culture in an Urban Context****TIME:** 10:00 am – 3:00 pm**MODE:** Charter Bus + Walking**FEE:** US\$75 (Includes lunch)**14 Transit Accessibility: From  
the Users' Perspectives****TIME:** 10:00 am – 3:00 pm**MODE:** SkyTrain + Bus + Walking**FEE:** US\$45 (Includes lunch)**15 Bus Bingo: Diesel, Trolley,  
Battery or Natural Gas****TIME:** 10:30 am – 2:00 pm**MODE:** Bus + Walking**FEE:** US\$45 (Includes lunch)**16 Burnabyism: The Distinct TOD  
of Vancouver's Neighbor****TIME:** 11:00 am – 3:00 pm**MODE:** SkyTrain + Bus + Walking**FEE:** US\$45 (Includes lunch)**17 Affordable TOD:  
Planning for Us All****TIME:** 11:30 am – 3:30 pm**MODE:** SkyTrain + Walking**FEE:** US\$45 (Includes lunch)**18 Complete Campuses:  
Transit and the City at UBC****TIME:** noon – 5:30 pm**MODE:** SkyTrain + Bus + Walking**FEE:** US\$60 (Includes lunch)

The mobile workshops were fantastic. They were educational, but also allowed you to experience what was happening locally – not always the case at conferences.”

– happy person, from 2018 conference survey

**19 Bikes in the Burbs: Long Trips and Last Miles****TIME:** 12:30 pm – 5:00 pm**MODE:** Bike**FEE:** US\$70**20 The Expo Line: Three Tales of TOD Community Planning****TIME:** 1:00 pm – 5:00 pm**MODE:** SkyTrain + Walking**FEE:** US\$60**21 Back to the Future: Expo Line Station Upgrades****TIME:** 2:00 pm – 5:00 pm**MODE:** SkyTrain + Walking**FEE:** US\$45**22 The Freeway Not Built: History, Community and a Better Area Plan****TIME:** 2:30 pm – 4:00 pm**MODE:** SkyTrain + Walking**FEE:** US\$45**23 Vancouver's Broadway: Taking Center Stage****TIME:** 7:30 am – 11:30 am**MODE:** SkyTrain + Bus + Walking**FEE:** US\$45**24 Surrey City Centre: The Region's Second Downtown****TIME:** 8:00 am – 11:30 am**MODE:** SkyTrain + Walking**FEE:** US\$45**25 Transit-Integrated Development: Canada Line Lessons****TIME:** 8:00 am – 11:30 am**MODE:** SkyTrain + Walking**FEE:** US\$60**26 An Olympic Win: The Athletes' Village as TOD****TIME:** 8:30 am – 11:30 am**MODE:** Bus + Ferry + Walking**FEE:** US\$60**27 Moving and Lingerin': Public Transit, Public Spaces****TIME:** 9:00 am – 11:30 am**MODE:** SkyTrain + Walking**FEE:** US\$45



# Pre-Conference EVENTS

## All Aboard the Polar West Coast Express!

Take Vancouver's commuter rail line, the West Coast Express, out to Moody Centre Station in Port Moody, a former mill town and original West Coast terminus of the Canadian Pacific Railway. Hear from a local urban transit expert about the history of rail in Metro Vancouver, the land use changes that have come with it and the early changes the recent LRT expansion have brought to the area. Afterwards enjoy a pint at one of the new craft breweries. Ride the SkyTrain back to downtown Vancouver. Note: Pint not included in price.

**TIME:** Friday, September 6  
5:00 pm – 9:00 pm

**MODE:** Commuter Rail, Walking

**FEE:** US\$20

## Victoria: The Inner Harbour

Victoria's compact size and a key City policy prioritizing walking, cycling and transit give this historic city one of the best modal splits in the country. Experience it yourself on an overnight adventure to British Columbia's capital city. Take a walking tour of Victoria's best public spaces integrating new AAA cycling, pedestrian and transit infrastructure, surrounded by historic Old Town, the iconic Inner Harbour and Legislative Precinct, and a rapidly urbanizing downtown core. After the tour, enjoy several hours in Victoria's downtown on your own.

**TIME:** Saturday, September 7, 8:00 am through  
Sunday, September 8, 3:30 pm

**MODE:** Bus, Ferry, Walking

**FEE:** US\$120

(includes transportation and  
meals; does not include hotel)

## A Moveable Feast: The Richmond Night Market

Just steps from the Bridgeport Canada Line station is a vibrant, multicultural experience: the Richmond Night Market. The Night Market features over 100 food stalls and more than 200 retail vendors. Learn about the history of this market and the opportunities of activating an under-utilized space near a major rapid transit station. Hear from TransLink about plans for a rapid transit station along the Canada Line to be funded by development. Meet other attendees in this casual, learning atmosphere while indulging in some of the stalls' offerings. Note: Food not included. Most stalls are cash-only. ATMs available at the market.

**TIME:** Saturday, September 7  
4:45 pm – 9:00 pm

**MODE:** SkyTrain

**FEE:** US\$20



## Pre-Conference EVENTS (cont.)

### Grouse Grind: Nature's Stair- Master to Breathtaking Views

Walk, run, or crawl your way up the infamous Grouse Grind. At a modest 1.8 miles (2.9 kilometers), this hike is considered short by Vancouver standards — until you factor in more than 2,800 feet (800 meters) of elevation gain and nearly 3,000 steps. Not for the faint of heart! Join us for a sweaty morning and bragging rights. You'll be rewarded at the top with a breathtaking view of Metro Vancouver, hang time with a working wind turbine and live Canadian grizzlies; lumberjack show and several dining options at the top. The hike up takes 1-2 hours. Fee includes gondola ride back down the mountain but bring extra for activities and meals up on top.

**TIME:** Sunday, September 8  
8:30 am – return via transit on  
your own schedule; allow just  
over an hour for the bus-  
SeaBus return trip.

**MODE:** SeaBus, Bus, Hiking, Gondola

**FEE:** US\$25

### Edge of the City: Suspension Bridge and Gondola

Explore Vancouver's North Shore mountains, just minutes from downtown. Ride over the Lions Gate Bridge to the Capilano Suspension Bridge Park. Take a guided tour across the 450-foot (137-meter) bridge suspended 230 feet (70 meters) over the Capilano River. Explore the Treetops Adventure or the terrifying Cliffwalk. After a few hours in Cap's Canyon, hop the shuttle for a five-minute ride to Grouse Mountain. Take a gondola up 3,937 feet (1,200 meters) in 12 minutes, with breathtaking views across and breathless hikers (conquering the Grouse Grind trail) below. At the top, sweat-free, eat lunch and explore the Eye of the Wind wind turbine, a grizzly bear sanctuary, and a variety of summer shows – or just kick back and enjoy the view. Fee includes lift ticket, some activities at the top and a free shuttle back to Waterfront Station.

**TIME:** Sunday, September 8  
9:00 am – return via transit  
on your own schedule

**MODE:** SeaBus, Bus, Walking, Gondola

**FEE:** US\$60

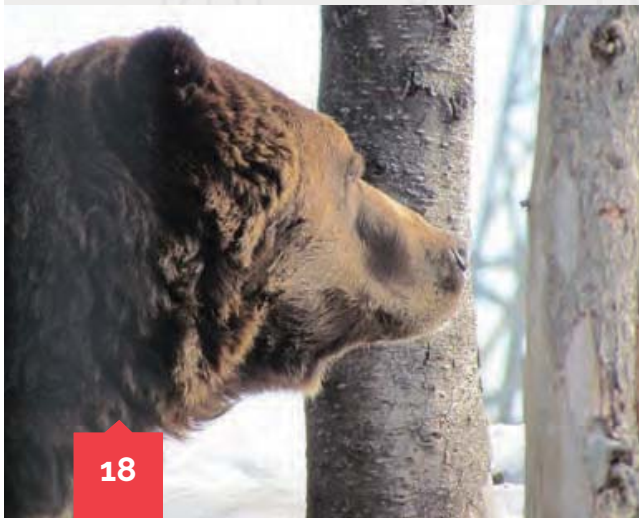
### Whistler Village: The West Coast's Summer and Winter Resort

Take a daytrip to Whistler, one of North America's largest ski resorts and a key venue for the 2010 Winter Olympics. Relax in Whistler's pedestrian village, nestled at the base of the mountains. Explore local First Nations culture at the Squamish Lil'wat Cultural Centre or visit the Audain Art Gallery, home to an impressive array of First Nations masks and contemporary British Columbian art. On the 1.5-hour return drive via the scenic Sea to Sky Highway, watch the sun set over the Pacific Ocean.

**TIME:** Sunday, September 8  
9:30 am - 5:30 pm

**MODE:** Charter Bus, Walking

**FEE:** US\$60





# New Rail~Volutionaries Events

**The New Rail~Volutionaries is a national networking group formed to make the conference more accessible, engaging and meaningful for new and emerging professionals.**

**Find out more: [railvolution.org/new-railvolutionaries](http://railvolution.org/new-railvolutionaries)**

## Rail~Volution 101

How can you make the most of Rail~Volution 2019? Get the scoop from the New Rail~Volutionaries.

**DATE:** Monday, September 9

**TIME:** 10:00 am – 12:00 pm

## Idea Exchanges

Riveting topics in an open forum. Grab a box lunch and join in on Idea Exchange discussions facilitated by the New Rail~Volutionaries.

**DATE:** Monday, September 9

**TIME:** 12:00 pm – 1:30 pm

## Mentorship Speed Networking at the Exhibitor Reception

Fast-paced. Concise. Focused. Establish a mentoring relationship with an industry expert or emerging leader – speed-dating style!

**DATE:** Monday, September 9

**TIME:** 5:00 pm – 6:30 pm



## Rail~Volution PK Slam at Vogue Theatre

A funny name for a fun- and fact-filled evening! Meet us at The Vogue, an art deco theatre recognized as a National Historic Site of Canada, just blocks from the conference hotel. Grab a drink. Grab a seat. Find out why this Tuesday night tradition is a favorite every year.

**DATE:** Tuesday, September 10

**TIME:** 8:00 pm – 10:00 pm

Vogue Theatre  
918 Granville Street  
Vancouver

**@VogueTheatre**



# Conference Details

Register at: [www.railvolution.org/registernow](http://www.railvolution.org/registernow)

## RATES

### EARLY BIRD

For best rates register by August 16

### AUGUST 17

And After

#### Private Sector - General Rate

US\$695

US\$745

#### Nonprofit, Government, Advocate Rate

US\$595

US\$645

#### Student Rate (Valid ID Required)

US\$225

US\$225

#### One-Day Rate

US\$525

US\$525

For registration questions, please contact:

**Megan Kallestad**

**EMAIL:** [gts@mngts.org](mailto:gts@mngts.org) **PHONE:** 952.885.7605

M–F, 8:00 am – 4:00 pm Central Daylight Time

Sending a group of 10 or more from one city or region? Contact Dan Bartholomay about special delegation benefits.

**Dan Bartholomay** Rail~Volution CEO

**EMAIL:** [danb@railvolution.org](mailto:danb@railvolution.org)

## CANCELLATION

There is a US\$100 nonrefundable fee for cancellations made between August 17 and September 7. No refunds for cancellations of any kind after September 7.

## SCHOLARSHIPS

Rail~Volution offers a limited number of scholarships. Applications open May 17 and close July 11. Scholarship awards will be announced in late July.

**More details and application:**

[www.railvolution.org/scholarships](http://www.railvolution.org/scholarships)

## TRAVEL TO VANCOUVER

Vancouver is served by Vancouver International Airport (YVR), with several flights to and from the US each day. Travelers from the US also have the option of flying to Seattle or other West Coast cities and traveling across the border on Amtrak or intercity bus.



US residents traveling to Canada will need a current passport. Please also be aware of Canadian laws related to previous DWI or DUI convictions. Find more information and tips for travel here: [railvolution.org/conference-information](http://railvolution.org/conference-information)

## CONFERENCE LOCATION

Join us at the Hyatt Regency Vancouver, centrally-located in downtown. Walk to the Seawall along the harbor, the historic Gastown neighborhood, Stanley Park and False Creek. The hotel is across the street from the Burrard Station on the Expo Line (convenient for travelers arriving at Pacific Central Station via Amtrak or intercity bus) and a few blocks away from Vancouver City Centre station (on the Canada Line from the Vancouver International Airport). There is a bike-share station on Melville Street, next to the hotel.

## HOTEL RESERVATIONS

### Hyatt Regency Vancouver

655 Burrard Street, Vancouver V6C 2R7  
British Columbia, Canada

**ONLINE:** [railvolution.org/bookaroom](http://railvolution.org/bookaroom)

**PHONE:** toll-free 1-800-233-1234

The Hyatt Regency will offer a special group rate of CA\$299 (roughly US\$225) plus taxes and fees per night, available until August 16 or rooms are sold out. For reservations, please call toll free 1.800.233.1234 and identify yourself as a Rail~Volution conference attendee using the block code **G-RVOL**.



# Sponsors & Exhibitors

The annual Rail~Volution conference would not be possible without our generous sponsors. Conference registration covers less than half of the total expenses. Join these organizations and support the movement to build livable communities with transit.

## THANK YOU TO RAIL~VOLUTION SPONSORS!

### DIAMOND

**AECOM**

**BOMBARDIER**

**HNTB**



Defining the cities  
of tomorrow

**TRANS LINK**

### PLATINUM

**HR**

**Stacy and  
Witbeck**

**HOLLAND**  
PARTNER GROUP

**wsp**

### GOLD

**lyft**



**Stantec**

**ZGF**

### SILVER

**Michael Baker  
INTERNATIONAL**

**TransLoc®**

### BRONZE

**ALSTOM**

**CIMA+**  
Engineering

**CUBIC™**



**McElhanney**

**optibus**

### LIVABILITY

Vix Technology

### RAIL~VOLUTION EXHIBITORS

Big thanks to these companies for being the first to book space to bring their products and services to the Rail~Volution Public Square and Exhibitor Showcase. There's still time to join them! Reach the stakeholders who direct transportation and development decisions in more than 300 communities across the US and Canada.

**BROOKVILLE**

**EPIC**

METALS 50 YEARS

**MetroQuest**  
Engagement optimized

**optibus**

**SIEMENS**



**Swiftly**

**URBAN  
FOOTPRINT**

**Find out more about sponsor  
and exhibitor opportunities:**

**[www.railvolution.org](http://www.railvolution.org)  
sponsorshipexhibitor**

For full benefits – and to be included in the Rail~Volution program – make sponsorship and exhibitor commitments by June 28.

Our Partners are the backbone of Rail~Volution. They are genuinely passionate about livability, equitable development and transit. Find out more about our partners at: [www.railvolution.org/partners](http://www.railvolution.org/partners)

AECOM  
Alliance for Metropolitan Sustainability  
American Public Transportation Association  
City of Charlotte, North Carolina  
Dallas Area Rapid Transit  
David Evans and Associates, Inc.  
Dialog  
GB Placemaking  
HDR  
Hennepin County, Minnesota  
Herzog  
HNTB  
IBI Group  
InfraStrategies  
Jacobs  
Kimley-Horn  
Local Initiatives Support Corporation (LISC)  
Low Income Investment Fund (LIIF)  
Maryland Transit Administration (Baltimore, Maryland)  
Metro (Los Angeles, California)  
Metro (Portland, Oregon)  
Metropolitan Atlanta Rapid Transit Authority

Metropolitan Council/Metro Transit (Minneapolis-St. Paul, Minnesota)  
Metropolitan Transportation Commission (Bay Area, California)  
Miami-Dade County  
Move Oahu Forward/Pacific Resource Partnership (Honolulu, Hawaii)  
Otak  
Parametrix  
Pittsburgh Community Reinvestment Group (PCRG)  
City of Portland, Oregon  
Port Authority of Allegheny County (Pittsburgh, Pennsylvania)  
Regional Transportation District (Denver, Colorado)  
San Francisco Bay Area Rapid Transit District  
Sound Transit (Seattle, Washington)  
South Florida Regional Transportation Authority (Pompano Beach, Florida)  
Transdev  
TransLink (Vancouver, British Columbia, Canada)  
TriMet (Portland, Oregon)  
Valley Metro (Phoenix, Arizona)  
Trinity Metro (Fort Worth, Texas)  
WSP USA



Building Livable Communities  
with Transit

**1624 Harmon Place, Suite 206  
Minneapolis, MN 55403  
USA**

Return Service Requested

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The Corporation of **THE CITY OF NORTH VANCOUVER**  
**ENGINEERING, PARKS AND ENVIRONMENT DEPARTMENT**

**REPORT**

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To: Mayor Linda Buchanan and Members of Council

From: Brian Willock, Manager, Engineering Planning and Design

Subject: MOSQUITO CREEK BRIDGE REPLACEMENT – FUNDING  
REALLOCATION

Date: June 5, 2019 File No: 11-5400-02-0001/1

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*The following is a suggested recommendation only. Please refer to Council Minutes for adopted resolution.*

**RECOMMENDATION:**

PURSUANT to the report of the Manager, Engineering Planning and Design, dated June 5, 2019, entitled "Mosquito Creek Bridge Replacement – Funding Reallocation":

THAT \$1,480,000 of existing funding from Project 51162 B-Line: Queensbury to Heywood be reallocated to fund the Mosquito Creek Bridge Replacement;

THAT "Development Cost Charge (Transportation) Reserve Fund Bylaw, 2018, No. 8690, Amendment Bylaw, 2019, No. 8724" (Funding Reallocation) be considered;

THAT \$1,000,000 of existing funding from Project 53226: Moodyville Neighbourhood Transportation Improvements be reallocated to fund the Mosquito Creek Bridge Replacement;

THAT "Development Cost Charge (Transportation) Reserve Fund Bylaw, 2018, No. 8625, Amendment Bylaw, 2019, No. 8723" (Funding Reallocation) be considered;

THAT \$200,000 of existing funding from Project 51168: 2019 Pavement Management be reallocated to fund the Mosquito Creek Bridge Replacement;

THAT \$190,000 of existing funding from Project 53240: East 3<sup>rd</sup>: Queensbury to Kennard be reallocated to fund the Mosquito Creek Bridge Replacement;



THAT “Development Cost Charge (Transportation) Reserve Fund Bylaw, 2018, No. 8623, Amendment Bylaw, 2019, No. 8722” (Funding Reallocation) be considered;

THAT \$350,000 of existing funding from Project 56084: Watermain Replacement East 21<sup>st</sup>: Lonsdale to St. Georges be reallocated to fund watermain replacement associated with the Mosquito Creek Bridge Replacement;

THAT \$180,000 of existing funding from Project 56085: Watermain Replacement St. Georges: East 21<sup>st</sup> to East 22<sup>nd</sup> be reallocated to fund watermain replacement associated with the Mosquito Creek Bridge Replacement;

AND THAT \$200,000 of existing funding from Project 51089: Bridge Rehabilitation be reallocated to fund the Mosquito Creek Bridge Replacement.

#### **ATTACHMENTS:**

1. Development Cost Charge (Transportation) Reserve Fund Bylaw, 2018, No. 8623, Amendment Bylaw, 2019, No. 8722 ([CD#1789736](#))
2. Development Cost Charge (Transportation) Reserve Fund Bylaw, 2018, No. 8625, Amendment Bylaw, 2019, No. 8723 ([CD#1789737](#))
3. Development Cost Charge (Transportation) Reserve Fund Bylaw, 2018, No. 8690, Amendment Bylaw, 2019, No. 8724 ([CD#1789740](#))

#### **BACKGROUND:**

When constructed in 1952, the Marine Drive Bridge over Mosquito Creek was one of the first examples of a post-tensioned reinforced concrete structure in North America. This type of reinforced concrete structure relies on pre-stressed steel tendons embedded inside the concrete girders.

Inspections conducted in 2015 revealed significant corrosion and damage to three girders where concrete had broken away and exposed the corroded steel tendons. Additional non-destructive testing and investigation of the remaining girders was inconclusive. Given the age of the structure as well as the uncertainty of the condition of the post-tensioned tendons in the other girders, the engineering consultant recommended replacement of the bridge. A structural engineering consultant has and will continue to inspect the bridge periodically to ensure it remains safe for use until its replacement is complete.

A new, wider bridge will improve pedestrian, cycling, and transit connections (including bus priority lanes) across Mosquito Creek and will also be designed to meet current seismic and flood protection requirements while considering impacts of climate change.

The Marine Drive Bridge over Mosquito Creek has been prioritized for replacement with total allocated funding of \$9,000,000 from senior government and the City as shown in Table 1 below.

Table 1: Existing Funding	
Build Canada – Small Communities Fund	\$3,870,000
TransLink Grant Funding	\$1,650,000
CNV 2018 Financial Plan	\$3,480,000
<b>Total</b>	<b>\$9,000,000</b>

Construction tenders closed April 26, 2019 with a low bid of \$10,529,219, which exceeds the available budget. City policy requires funding to be allocated before awarding a contract.

An additional revelation affecting the project schedule concerns private utility relocation. TELUS Communications has infrastructure on the existing bridge and despite being consulted early and continuously, TELUS recently indicated they now require three months to temporarily remove their lines from the bridge. Bridge work cannot begin with the TELUS lines in place and will push bridge construction beyond the 2019 window for in-stream work in Mosquito Creek.

To accommodate the delay due to the TELUS utility relocation, and to keep the project moving forward, staff recommend dividing construction into two phases and reallocating funds from existing projects while also continuing to pursue additional funding from senior government.

Phase 1 will proceed in 2019 and comprise utility relocations (TELUS, LEC, and water) as well as procurement and manufacture of the prefabricated bridge elements. Phase 2 in 2020 will comprise bridge construction, intersection improvements, and trail construction.

## DISCUSSION:

The original schedule had planned for all in-stream work to be completed in a single season. This was an ambitious schedule given the need to demolish half of the existing bridge at a time to allow for the efficient construction of the new bridge and continuous traffic movement through the area. Dividing construction over two seasons will reduce overall project risk by simplifying the logistics and construction sequencing required for both the utility relocations and the bridge construction. Additional benefits of proceeding with the project in phases are as follows:

- utility relocations will be complete and costs known before finalizing the 2020 budget;
- the bridge contract costs are known, the contractor is secured, and pre-fabricated components can be procured early to reduce schedule risk;
- reduce risks to schedule for “in-stream work” by spreading the work over two seasons; and
- an opportunity for the contractor and design consultant to work together to scrutinize the project to find efficiencies and potential scope reductions.



Staff considered the option to cancel the tender and retender the work. This is not recommended because a second tender process is unlikely to yield a substantially different contract price and construction costs continue to escalate. There is a high likelihood that the price will increase in the future, so staff recommend proceeding with the low bidder.

## FINANCIAL IMPLICATIONS:

Total available funding is \$9,000,000 (including funds spent to date) and is not sufficient to proceed with the project. City policy requires that project funding be available and appropriated before a contract can be awarded.

To proceed with the project and award the contract to the low bidder, staff recommend reallocating \$3,600,000 from existing projects to provide total interim project funding of \$12,600,000 in 2019. An additional \$2,400,000 will be required during either the 2019 Revised Financial Plan, or the 2020 Financial Plan. The revised budget is summarized in Table 2.

<b>Table 2: Revised Budget</b>	
2019	
Design (includes \$800,000 already spent and \$200,000 encumbered)	\$1,000,000
Utility Relocations	\$1,000,000
Bridge Tender Award	\$10,600,000
<b>Subtotal 2019</b>	<b>\$12,600,000</b>
2020	
Contract Administration and Inspection	\$400,000
Contingency (15%)	\$2,000,000
<b>Subtotal 2020</b>	<b>2,400,000</b>
<b>Total</b>	<b>\$15,000,000</b>

The proposed funding reallocations are summarized in Table 3.

<b>Table 3: Summary of Proposed Reallocations for 2019</b>			
Project	Description	Rationale	Amount
51162	B-Line: Queensbury to Heywood	Funded by TransLink's B-Line project	\$1,480,000
53226	Moodyville Neighbourhood Transportation Improvements	Improvements will be delivered by adjacent developments	\$1,000,000
51168	2019 Pavement Management	Pavement rehabilitation adjacent the bridge can be funded from 2019 Pavement Management	\$200,000
53240	East 3 <sup>rd</sup> : Queensbury to Kennard	Funded by TransLink's B-Line project	\$190,000

56084	Watermain Replacement East 21 <sup>st</sup> Lonsdale to St. Georges	Defer project and include in 2020 water utility budget	\$350,000
56085	Watermain Replacement St. Georges: East 21 <sup>st</sup> to East 22 <sup>nd</sup>	Defer project and include in 2020 water utility budget	\$180,000
51089	Bridge Rehabilitation	Defer routine maintenance items and include in 2020 budget	\$200,000
		<b>Total</b>	<b>\$3,600,000</b>

These projects were selected either because the funding may not be required, or because the work can be deferred to a future year. For example, in budgeting for the B-Line during the summer of 2017, the scope of work to be funded by TransLink was unknown so staff budgeted for improvements as part of the City's financial plan. With the B-Line design complete and scope defined, TransLink has awarded the construction contract. Much of the City funding set aside for the B-Line will not be required and can be reallocated to the bridge replacement.

Similarly, the timing of developments in the Moodyville neighbourhood was unknown and staff budgeted for civil works adjacent to properties that might not redevelop as well as for relocation of the existing BC Hydro transmission lines. With the pace of redevelopment much faster than anticipated, the scope of work the City will have to fund is much less than anticipated and therefore the funding can be reallocated. Also, the street design has been refined and BC Hydro has reduced their estimated costs, therefore some of the funding can be reallocated.

Additional funding can be reallocated by deferring projects to future years. For example, the two watermain replacement projects are part of the plans for servicing the new Harry Jerome Community Recreation Centre and neighbourhood lands, and can be deferred to 2020. Also, the paving required as part of the bridge reconstruction can be funded from the 2019 pavement management budget.

In addition to the above-mentioned amount of \$2,400,000 in new funding to be included in 2020 to fully fund the Mosquito Creek Bridge Replacement project, the deferral of the above-mentioned projects will require the inclusion of the following amounts in the 2020-2029 Financial Plan to replenish funding of those projects:

Water Utility Funding: \$530,000 for projects 56084 and 56085;  
General Capital: \$50,000 for project 51089.

By proceeding with contract award in June 2019, the City will be committed to providing additional funds in 2020 through some combination of additional reallocations from existing projects, senior government funding, the revised 2019-2028 Financial Plan, and/or the 2020-2029 Financial Plan. The City continues to communicate with the Federal Government in hopes they will increase their contribution to this important project. The total existing and proposed funding required to proceed with the project is summarized in Table 4.



<b>Table 4: Summary of Existing Funds and Additional Funding Required</b>	
<b>Existing Funding</b>	
Build Canada – Small Communities Fund	\$3,870,000
TransLink Grant Funding (External)	\$1,650,000
CNV 2018 Financial Plan	\$3,480,000
<b>Subtotal</b>	<b>\$9,000,000</b>
<b>Requested Reallocated Funding</b>	
Reallocation from existing projects 2019	\$3,600,000
Additional funding required 2020	\$2,400,000
<b>Total</b>	<b>\$15,000,000</b>

#### **INTER-DEPARTMENTAL IMPLICATIONS:**

This report was prepared in collaboration with the Finance Department.

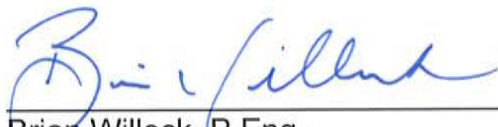
As this bridge replacement involves a variety of technical components, transportation engineering, utilities upgrades, and parks planning, the project will be delivered by a multidisciplinary team with the Engineering, Parks and Environment Department and with the assistance of outside consultants.

#### **CORPORATE PLAN AND/OR POLICY IMPLICATIONS:**

This proposed work supports the following goals and objectives of the City's 2014 Official Community Plan:

- 2.1.4 Reduce crossing barriers at locations such as intersections, creeks, highways and rail crossings so that walking and cycling are more convenient and attractive;
- 2.3 Support a safe, accessible, resilient, and affordable transportation system;
  - 2.3.1 Maintain the existing transportation infrastructure in good repair;
  - 2.3.9 Design and adapt transportation infrastructure to be resilient to environmental changes and natural disasters such as extreme weather events.

RESPECTFULLY SUBMITTED:

  
\_\_\_\_\_  
Brian Willock, P.Eng.,  
Manager, Engineering Planning and  
Design

THE CORPORATION OF THE CITY OF NORTH VANCOUVER

BYLAW NO. 8722

**A Bylaw to Amend “Development Cost Charge (Transportation)  
Reserve Fund Bylaw, 2018, No. 8623”**

The Council of The Corporation of the City of North Vancouver, in open meeting assembled, enacts as follows:

1. This Bylaw shall be known and cited for all purposes as **“Development Cost Charge (Transportation) Reserve Fund Bylaw, 2018, No. 8623, Amendment Bylaw, 2019, No. 8722” (Funding Reallocation)**.
2. An amount of \$94,050 is hereby reallocated from the “3<sup>rd</sup> Street Transit and Active Transportation Improvements” project for the purpose of funding the “Mosquito Creek Bridge Replacement” project.

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MAYOR

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THE CORPORATION OF THE CITY OF NORTH VANCOUVER

BYLAW NO. 8723

**A Bylaw to Amend “Development Cost Charge (Transportation)  
Reserve Fund Bylaw, 2018, No. 8625”**

The Council of The Corporation of the City of North Vancouver, in open meeting assembled, enacts as follows:

1. This Bylaw shall be known and cited for all purposes as **“Development Cost Charge (Transportation) Reserve Fund Bylaw, 2018, No. 8625, Amendment Bylaw, 2019, No. 8723” (Funding Reallocation)**.
2. An amount of \$495,000 is hereby reallocated from the “Moodyville Neighbourhood Transportation Improvements” project for the purpose of funding the “Mosquito Creek Bridge Replacement” project.

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THE CORPORATION OF THE CITY OF NORTH VANCOUVER

BYLAW NO. 8724

**A Bylaw to Amend “Development Cost Charge (Transportation)  
Reserve Fund Bylaw, 2018, No. 8690”**

The Council of The Corporation of the City of North Vancouver, in open meeting assembled, enacts as follows:

1. This Bylaw shall be known and cited for all purposes as **“Development Cost Charge (Transportation) Reserve Fund Bylaw, 2018, No. 8690, Amendment Bylaw, 2019, No. 8724” (Funding Reallocation)**.
2. An amount of \$732,600 is hereby reallocated from the “Marine-Main Corridor: Queensbury to Heywood” project for the purpose of funding the “Mosquito Creek Bridge Replacement” project.

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


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The Corporation of **THE CITY OF NORTH VANCOUVER**  
**PLANNING & DEVELOPMENT DEPARTMENT**

**REPORT**

To: Mayor Linda Buchanan and Members of Council

From: Jennifer Draper, Deputy Director, Planning & Development  
Christopher French, Planning Assistant, Transportation

Subject: BIKE VALET AT COMMUNITY EVENTS – POLICY UPDATES

Date: June 5, 2019 File No: 16-8330-05-0001/1

*The following is a suggested recommendation only. Refer to Council Minutes for adopted resolution.*

**RECOMMENDATION:**

PURSUANT to the report of the Deputy Director, Planning and Development and Planning Assistant, Transportation, dated June 5, 2019, entitled "Bike Valet at Community Events – Policy Updates":

THAT the Shipyards & Foot of Lonsdale Events Policy (C59B) and Policy for Events in Parks and Public Open Spaces (C57C) be updated to the proposed wording outlined in Attachment 1.

**ATTACHMENTS:**

1. Council Report dated March 20, 2019: Bike Valet at Community Events (doc [#1791005](#))
2. Proposed Amendments to Event Policies for Sustainable Transportation (doc [#1778536](#))

## **PURPOSE**

The purpose of this report is to present for Council consideration, amendments to select City event policies (C57C, C59B) to encourage sustainable and active transportation at regional events hosted at City venues.

## **BACKGROUND**

On April 1, 2019, staff brought forward a report on the provision of bike valets at community events in the City of North Vancouver. In addition to a number of recommendations to promote bike valet services, staff received Council direction to amend two of the City's event policies to encourage and support sustainable transportation to venues:

THAT Council direct staff to amend The Shipyards and Foot of Lonsdale Events Policy (C59B) and Policy for Events in Parks and Public Open Spaces (C57C) to require promotion of active transportation modes.

## **DISCUSSION**

When hosting an event at a City venue, an organizer must abide by the City's event policy for that respective venue. The two policies that govern these events are the Shipyards & Foot of Lonsdale Events Policy (C59B), and the Policy for Events in Parks and Public Open Spaces (C57C).

Per the direction received from Council in April 2019, the following summary of changes are proposed:

- Require the promotion of sustainable and active transportation modes for events at the Shipyards & Foot of Lonsdale; and for regional events
- Encourage promotion of sustainable and active transportation modes for all events through the Policy for Community Events in Parks and Public Open Spaces

Reference is to be made to Attachment 2 for the recommended changes to the two applicable policies.

## **FINANCIAL IMPLICATIONS**

With the adoption of the policy updates, the City will be providing some staff time to support and promote the uptake of sustainable transportation options, including bike valet services, with event organizers. Furthermore, staff will continue to seek out funding and partnerships with local businesses for bike valet services in support of City hosted events.

## INTER-DEPARTMENTAL IMPLICATIONS

The recommended changes to the policies set forth in the appendix were put together in collaboration with staff from the Departments of Planning & Development; Engineering, Parks & Environment; Community & Partner Engagement; and the North Vancouver Recreation & Culture Commission.

## CORPORATE PLAN AND/OR POLICY IMPLICATIONS

Changing the selected City event policies to encourage attendees to commute to large, regional events via sustainable modes such as cycling, walking or transit is in keeping with the goals and objectives of the 2014 Official Community Plan including:

**Goal 2.1** - Prioritize walking, cycling, transit and good movement over single-occupancy vehicles;

**Objective 2.1.7** - Work with partners to encourage and promote the numerous benefits of active transportation, including health, social and economic benefits, especially amongst young people;

**Objective 2.1.8** - Work with partners, including TransLink, employers in the City and their labour representatives on transportation demand management measures that encourage walking, cycling, and the use of public transit;

**Objective 2.3.8** - Encourage transportation options that reduce fossil fuel use, such as walking, cycling, transit, carpooling, and low-emission vehicles.




RESPECTFULLY SUBMITTED:

  
\_\_\_\_\_  
Jennifer Draper  
Deputy Director, Planning & Development

  
\_\_\_\_\_  
Christopher French  
Planning Assistant – Transportation





 Division Manager	 Director	 CAO
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The Corporation of **THE CITY OF NORTH VANCOUVER**  
**PLANNING & DEVELOPMENT DEPARTMENT**

**REPORT**

To: Mayor Linda Buchanan and Members of Council

From: Jennifer Draper, Manager, Transportation Planning  
Christopher French, Planning Assistant, Transportation

Subject: BIKE VALET AT COMMUNITY EVENTS

Date: March 20, 2019 File No: 16-8330-05-0001/1

*The following is a suggested recommendation only. Refer to Council Minutes for adopted resolution.*

**RECOMMENDATION:**

PURSUANT to the report of the Manager, Transportation Planning and the Planning Assistant, Transportation, dated March 20, 2019, entitled "Bike Valet at Community Events":

THAT Council direct staff to amend the event permit application to encourage organizers to incorporate and promote active and sustainable modes of transportation;

THAT Council direct staff to amend The Shipyards and Foot of Lonsdale Events Policy (C59B) and Policy for Events in Parks and Public Open Spaces (C57C) to require promotion of active transportation modes;

THAT Council direct staff to work with event organizers for regional events in seeking funding opportunities and partnerships with local businesses to provide for bike valet services;

AND THAT Council direct staff to support events that are space constrained through the temporary use of public space for bike valet services, where feasible.

## PURPOSE

The purpose of this report is to provide Council with recommended approaches for the encouragement, support and provision of bike valet services at events in the City of North Vancouver. These approaches are derived from a sampling of progressive cities in Metro Vancouver, Canada and the United States where similar efforts are being made to promote active and healthy transportation options for residents and visitors.

## BACKGROUND

In January 2019, Council directed staff to explore the possibility of bike valet services for events being held in the City of North Vancouver:

WHEREAS the City of North Vancouver and Council have a goal of being “the healthiest small city in the world”;

WHEREAS the City of North Vancouver hosts many exciting, well-attended events, including the Shipyards Night Market, Summer Sessions, Fun City, Caribbean Days, and many more;

WHEREAS often at these events existing bike racks become oversubscribed and safe storage places for families with scooters or strollers do not currently exist;

WHEREAS bike valet is a service much like a coat check where people can leave a bike, stroller or scooter in a safe location watched over by volunteers or paid staff for a nominal fee or donation;

AND WHEREAS other municipalities in Metro Vancouver, including Vancouver and Surrey, support or require bike valet services at municipal events;

THEREFORE BE IT RESOLVED THAT staff investigate and report back to Council regarding how City of North Vancouver events could include valet services, including identifying costs, potential locations, funding opportunities and partnerships with local businesses and organizations.

The City currently promotes active and sustainable transportation through a variety of programmatic and promotional measures, including videos, events and content on the City’s website. However, limited information to promote bike valet services is available for event organizers who apply through the City’s general Event Permit Application and North Vancouver Recreation & Culture Commission’s Community & Trail Event Application.

The two policies that govern event permits and requirements in City parks and venues are The Shipyards and Foot of Lonsdale Events Policy (C59B) and Policy for Events in Parks and Public Open Spaces (C57C). Under Policy C59B, event organizers are encouraged to advertise transit and other sustainable transportation options for their event. The venues under this policy are a short walk from the Spirit Trail and Lonsdale Quay transit exchange. There are no such guidelines in Policy C57C.



## DISCUSSION

Event organizers looking to host their events at City venues<sup>1</sup> first apply for permits through the City Venue Booking Application form. This form asks for information related to the organization, event, estimated number of attendees and details on a zero waste plan. No formal request for consideration of sustainable transportation planning related to the event is made by the City through this application form; however, mention is made in the Shipyards & Foot of Lonsdale Events policy.

The City of North Vancouver hosted 46 regional events at our facilities and public spaces in 2018 and it is expected this number will continue to grow, as our City increasingly becomes an attractive destination for the region and beyond. Although events bring many benefits to our City, including vibrancy, economic spin-off effects for local businesses and social connectivity, they can also strain our transportation network, causing frustration for event attendees and adjacent communities. Having predictable alternative options, such as a bike valet, can help people make informed choices on accessing events.

### *What is bike valet?*

A bike valet service is hired by event planners and organizers to provide secure bicycle parking for attendees. Similar to a coat check, attendees give their bike to the valet and receive a ticket stub for redemption upon pick up. Bike valet services allow for additional items such as helmets, locks and panniers to be left securely with the bike. Depending on the operator, other forms of sustainable transportation equipment, such as strollers, skateboards and push-scooters may be checked as well.

Local bike valet operators typically do not charge users of the service, increasing its overall attraction to event attendees. However, they will charge event planners and organizers for the provision and operation of their bike valet service.

### *Benefits of bike valet*

Bike valet services improve event experience, operations and attendance by reducing motor vehicle trips and parking demand, and raise the visibility of cycling to events. In addition, bike valet services reduce the number of bicycles locked to street poles, fences, and trees and prevent attendees who choose to cycle from walking their bike through crowded spaces. This also lowers the risk of bike theft while attendees are enjoying the event. Further, bike valet services produce additional sources of publicity by advertising their attendance at events through their internal social media and online networks.

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<sup>1</sup> Except for the Pipe Shop – Quay Property Management handles Applications and site visits. Quay Property Management does not have any set initiatives to encourage event organizers to promote sustainable transportation.

### *Implementation in other cities*

At present, no municipalities in Metro Vancouver require bike valet services at events, regardless of event size. However, some encourage bike valets in various ways through policy or partnerships. The following table outlines initiatives from a sampling of locations around the Lower Mainland and in North America:

Location	Initiatives
Granville Island, Vancouver, BC	<ul style="list-style-type: none"><li>• Daily bicycle valet operated in summer months</li><li>• Initiated in response to high rates of bike theft</li><li>• Bike theft rate dropped by 70% since inception</li></ul>
City of Vancouver, BC	<ul style="list-style-type: none"><li>• Asks organization on permit application to demonstrate environmental responsibility</li><li>• Web page with information and encouragement for provision of bike valet services</li><li>• "Green Your Event" resource guide that includes bike valet and sustainable transportation information</li></ul>
City of Richmond, BC	<ul style="list-style-type: none"><li>• Operate a bike valet service called "Wheel Watch" with a cohort of City of Richmond volunteers</li><li>• Event planners submit application forms to book venues; staff identify and select events where bike valet may be appropriate and successful</li><li>• Operations carried out by volunteers keeps costs low</li><li>• 1,200 bikes parked at 12 events in the City of Richmond in Summer 2018</li></ul>
City of San Francisco, California, USA	<ul style="list-style-type: none"><li>• Requires events expecting more than 2,000 people to provide secure active transportation parking</li><li>• This is usually a bike valet, mainly operated by the local San Francisco Bicycle Coalition</li></ul>
Portland Bureau of Transportation, Oregon, USA	<ul style="list-style-type: none"><li>• Encourages and recommends secure bicycle parking at large community events</li><li>• Encourages organizers to apply for permits to use on-street spaces for secured bicycle parking if there is not enough space at the event venue</li><li>• Provides guidelines for site specifications</li></ul>

San Francisco and Portland make reference to event size in setting their requirements. Large, or sometimes referred to as 'regional' events, are those expected to draw attendees from other jurisdictions. Events of this scale can typically predict a larger number of attendees which requires a more strategic approach to offering viable, alternative transportation options. Furthermore, this scale of event is more likely able to financially support providing a bike valet service.



### *Bike valet in the City of North Vancouver*

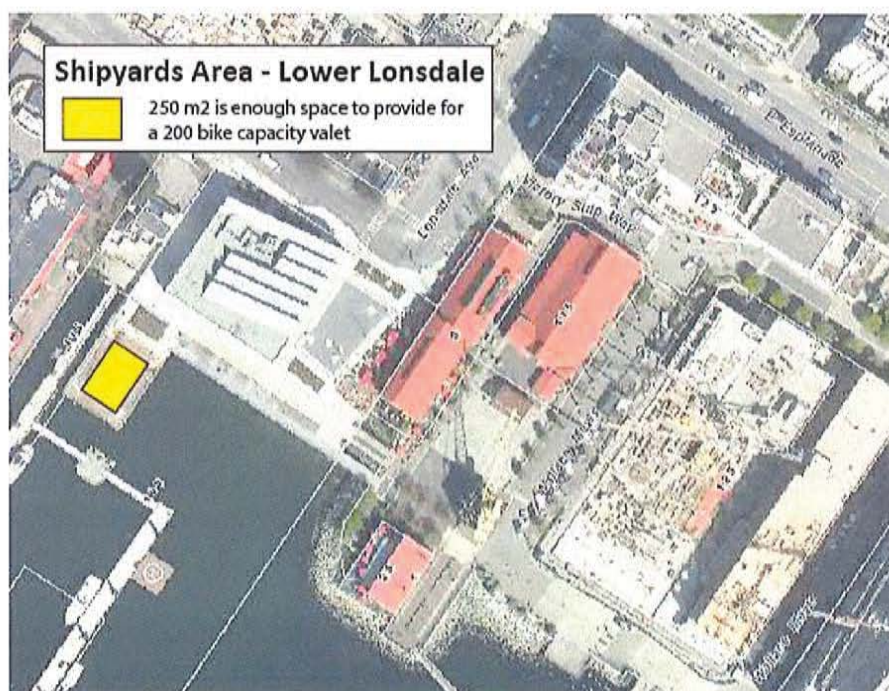
In the past, the City has secured funding from TransLink's TravelSmart program to offer bike valet services for events such as Shipped and Slide the City. Despite the provision of a bike valet service, these events have not attracted high demand, with less than 50 bikes parked per event<sup>2</sup>. Should the City encourage bike valets at regional events, careful consideration for the elements that most strongly influence the success of the service must be made. These include:

- Promotion of the bike valet;
- Convenient placement, ideally near the venue entrance; and
- Easy access to information on cycling routes.

### *Space Requirements*

The required amount of space for a bicycle valet varies based on the expected number of bikes and accessories. The Portland Bureau of Transportation recommends that event organizers reserve space for bike parking equal to a minimum of 2% of the total expected crowd attendance. Past events in the City that have provided bike valet have seen approximately 1% of attendees use the service. This is near consistent with the City's cycling mode share of 2%, but cycling mode share varies seasonally and attendees who do not normally cycle may opt to ride their bike to an event.

Cycle Toronto, a not-for-profit bike valet service provider in Toronto, recommends that the valet site be highly visible near the main entrance to the event and that it be a minimum of 6 m wide. Cycle Toronto's recommended footprint for 200 bikes would be approximately 250 m<sup>2</sup>.



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<sup>2</sup> Except for Slide the City in 2015, which operated alongside Car Free Day and saw 158 bikes parked.



### *Potential Locations*

Popular regional events in the City include the Shipyards Night Market, Summer Sessions, Fun City Festival, Caribbean Days and Canada Day. The majority of these events take place at venues in the Lower Lonsdale area, including Shipbuilders Square, Waterfront Park or on street along Lonsdale Avenue. Space is often limited at these venues, but nearby locations on public space such as street right of way or City-owned sites may be suitable for hosting a bike valet. For example, while the space at Shipyards Night Market is very limited, nearby Cates Deck may serve as an adequate location with proper temporary signage to direct cyclists. Should Council proceed with the recommendation, staff would work in identifying potential locations when working with event applicants.

### *Costs*

Costs for bike valet vary between public and private operators and depend on other factors such as event duration and the number of bikes and accessory devices. Staff have sampled a number of privately operated bike valets at regional events in the Lower Mainland and have estimated that, depending on attendance, costs can vary from \$550 to \$1,800 per event.

Alternatively, a municipal program such as "Wheel Watch", a mobile bike valet operated by the City of Richmond, incur initial start up costs balanced by low operational costs. Richmond's start up costs were approximately \$7,500 for a system capable of storing 40 bicycles. Annual costs thereafter range from \$300-\$750 for insuring and repairing the trailer, repairs to equipment if necessary and staff time at events. Two people are required to operate a valet service with a capacity of 40 bikes.

Staff are aware that escalating requirements and associated costs for events can impact the viability of events. It is therefore recommended that staff work with applicants on large or regional events to encourage and support the provision of bike valet services.

### *Recommended Approach*

Transportation Planning staff are actively seeking partnerships with local business who may be interested in sponsoring bike valet services at regional events in the City. Transportation Planning staff can also support and encourage the provision of bike valet services at regional events in a few simple, yet impactful ways. These include:

- Amending the City's event application form to require the applicant to describe their approach to encouraging and accommodating sustainable travel;
- Growing awareness and knowledge on the provision of bike valet services by developing online content, including guidelines that detail considerations and benefits;
- Liaising in the identification of funding and partnership opportunities to improve the financial viability of providing bike valet services; and
- Assisting event organizers in the siting of bike valets, including identification of street-use permits as a tool when event space may be constrained.

Staff will also update the Shipyards and Foot of Lonsdale Events Policy (C59B) and Policy for Events in Parks and Public Open Spaces (C57C) to require promotion of active transportation modes to events in addition to transit and include guidelines detailing information about bike valet services.

To evaluate the success of this approach, staff from Community & Partner Engagement will support Transportation Planning staff by sharing data on the number of events that host bike valet services and the number of bikes and accessory mobility devices parked. If bike valet services become more prevalent and successful at regional events in the City after staff enact these initiatives, staff may ask Council for further direction to explore other opportunities to support active transportation to events. These may include, but are not limited to:

- Developing a requirement in event policies for bike valet services at events based on expected attendance; and
- Exploring the possibility of developing a municipally operated bike valet service.

## **FINANCIAL IMPLICATIONS**

Some staff time is required on an on-going basis to support event planners. Staff may also work with event organizers to secure funding and partnerships with local businesses for bike valet services.

If event planners and organizers cite concerns about space constraints at their event, staff may direct them to apply for a street use permit.

## **INTER-DEPARTMENTAL IMPLICATIONS**

This report was put together with input from the departments of Planning & Development; Community & Partner Engagement and Engineering, Parks and Environment.

Staff from Community & Partner Engagement will support Transportation Planning staff by sharing data on the number of events that host bike valet services and the number of bikes and accessory mobility devices parked. This information will be used in an effort to monitor and evaluate the effectiveness of the recommended approaches and report back to Council.

## **CORPORATE PLAN AND/OR POLICY IMPLICATIONS**

Having a bike valet at large events such as the Shipyards Night Market, Summer Sessions and Caribbean Days would promote the City's 51 km of marked and signed bicycle facilities. Further, bike valet services can enable a broad range of people to replace single-occupant vehicle trips to events, relieving congestion and improving safety.

Developing guidelines and recommendations for bike valet services at community events will require minor amendments to Policy C59B and Policy C57C.



The proposal to encourage bike valet services at events is in keeping with the goals and objectives of the 2014 Official Community Plan including:

**Goal 2.1** - Prioritize walking, cycling, transit and good movement over single-occupancy vehicles;

**Objective 2.1.1** - Invest in cycling and pedestrian networks and facilities to make these more attractive, safer and convenient transportation choices for all ages and abilities with an aim to increase these ways of traveling over single-occupant vehicle use;

**Objective 2.1.7** - Work with partners to encourage and promote the numerous benefits of active transportation, including health, social and economic benefits, especially amongst young people;

**Objective 2.1.8** - Work with partners, including TransLink, employers in the City and their labour representatives on transportation demand management measures that encourage walking, cycling, and the use of public transit

**Goal 2.3** - Support a safe, accessible, resilient, and affordable transportation system;

**Objective 2.3.8** - Encourage transportation options that reduce fossil fuel use, such as walking, cycling, transit, carpooling, and low-emission vehicles;

RESPECTFULLY SUBMITTED:

  
for Jennifer Draper  
Manager, Transportation Planning

  
Christopher French  
Planning Assistant, Transportation



## **Proposed Amendments to City Event Policies (C59B, C57C)**

### **Proposed Amendment to The Shipyards & Foot of Lonsdale Event Policy (C59B)**

Currently, under section 3.0, event organizers are encouraged to advertise sustainable transportation to their event. The policy reads:

*“Attendee Transportation:*

*Event organizers are encouraged to advertise transit and other sustainable transportation options for their event. These venues are a short walk, via the Spirit Trail from the SeaBus terminal.”*

The proposed changes to this section, under the same heading, are as follows:

*“Attendee Transportation:*

*Event organizers are required to advertise transit, cycling, walking and other sustainable transportation options for their event. These venues are easily accessible by the Spirit Trail, and located close to Lonsdale Quay SeaBus Terminal and Transit Exchange.*

*If hosting a regional event, event organizers are encouraged to provide convenient, secure bicycle parking for attendees, such as a bike valet. For more information about bike valet services, please visit [www.cnv.org/BikeValet](http://www.cnv.org/BikeValet)”*

### **Proposed Amendment to the Policy for Community Events in Parks and Public Open Spaces (C57C)**




The policy does not include any information or encouragement about sustainable transportation. Because a lot of these community events may be smaller in comparison to those that happen at the Foot of Lonsdale, the proposed amendment does not require, but encourages advertisement initiatives for sustainable transportation.

The following amendment would be inserted under section 8.0 after the description of the requirements for a zero waste plan and responsibilities to leave park venues free of litter. The policy would read:

*“ ...*

- Any additional clean-up costs borne by the City will be deducted from the damage deposit. The event organizer will be invoiced for any additional costs for park repairs that result from the event’s operation.*
- Community event organizers are encouraged to advertise transit, cycling and other sustainable transportation options for their event. Many of the City’s parks and public open spaces are in close proximity to transit and sustainable transportation networks such as the Spirit Trail and Green Necklace.*
- If hosting a large, community event, event organizers are encouraged to provide convenient, secure bicycle parking for attendees, such as a bike valet. For more information about bike valet services, please visit [www.cnv.org/BikeValet](http://www.cnv.org/BikeValet) ”*



 Division Manager	 Director	 CAO
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The Corporation of **THE CITY OF NORTH VANCOUVER**  
**PLANNING & DEVELOPMENT DEPARTMENT**

**REPORT**

To: Mayor Linda Buchanan and Members of Council

From: David Johnson, Development Planner

Subject: REZONING APPLICATION: 151 EAST KEITH ROAD (URBAN SYSTEMS / IMH 151 E KEITH APARTMENTS LTD)

Date: May 6, 2019 File No: 08-3360-20-0403/1

*The following is a suggested recommendation only. Refer to Council Minutes for adopted resolution.*

**RECOMMENDATION:**

PURSUANT to the report of the Development Planner, dated May 6, 2019, entitled "Rezoning Application: 151 East Keith Road (Urban Systems) / IMH 151 E Keith Apartments LTD)":

THAT second reading of "Zoning Bylaw, 1995, No. 6700, Amendment Bylaw, 2018, No. 8618" (Urban Systems / IMH 151 E Keith Apartments LTD, 151 East Keith Road, CD-702) be rescinded;

THAT "Zoning Bylaw, 1995, No. 6700, Amendment Bylaw, 2018, No. 8618" (Urban Systems / IMH 151 E Keith Apartments LTD, 151 East Keith Road, CD-702) be amended and given second reading, as amended, and referred to a Public Hearing;

THAT second reading of "Housing Agreement Bylaw, 2018, No. 8622" (Urban Systems / IMH 151 E Keith Apartments LTD, 151 East Keith Road, CD-702, Rental Housing Commitments) be rescinded;

THAT "Housing Agreement Bylaw, 2018, No. 8622" (Urban Systems / IMH 151 E Keith Apartments LTD, 151 East Keith Road, CD-702, Rental Housing Commitments) be amended and given second reading, as amended, and referred to a Public Hearing;

THAT additional offsite works and community amenities listed in the May 6, 2019 report in the section "Community Amenities and Infrastructure Improvements" be secured, through agreements at the applicant's expense and to the satisfaction of the Director of Planning and Development, as conditions of rezoning;

AND THAT the Mayor and City Clerk be authorized to execute all necessary covenants and legal agreements required to secure the commitments outlined in the report.

## **ATTACHMENTS:**

1. Context Map (doc#[1767383](#))

### **Updated Proposal**

2. Revised Project Summary Sheet from plans dated February 4, 2019 (doc#[1765369](#))
3. Revised Architectural and Landscaping Plans, dated February 4, 2019 (doc#[1773974](#))
4. Updated Public Consultation Summary dated April 11, 2019 (doc#[1773897](#))
5. Affordability Statement dated February 6, 2019 (doc#[1779267](#))
6. Sustainability Statement dated January 2018 (doc#[1780126](#))

### **Background**

7. Staff Report dated March 7, 2018 (doc#[1776566](#))
8. Architectural and Landscaping Plans dated February 27, 2018 (doc#[1625895](#))

### **Bylaws**

9. Tracked changes version of "Zoning Bylaw, 1995, No. 6700, Amendment Bylaw, 2018, No. 8618" (doc#[1628354-v2](#))
10. Tracked changes version of "Housing Agreement Bylaw, 2018, No. 8622" (doc#[1626592-v2](#))

## **PURPOSE**

The purpose of this report is to present for Council's consideration, the return of a Development Application to rezone 151 East Keith Road (Attachment #1) from High Density Apartment Residential 1 (RH-1) to Comprehensive Development 702 (CD-702). The application is requesting the addition of two rental infill buildings, securing all units, both new and those within the existing residential tower, as rental in perpetuity and partnering with Hollyburn Family Services for the provision of 4 units accommodating vulnerable seniors. The return of this application is to amend the proposal due to the result of not achieving Third Reading at the April 23, 2018 Council meeting.

## **BACKGROUND**

On March 12, 2018, Council considered an application for the subject site to add three rental infill buildings to the existing residential rental tower (Attachments #7 and #8). The three buildings comprised of:

- 33 studio units;
- 5 two bedroom townhouse units; and
- 2 three bedroom townhouse units.

Council granted the proposal First and Second Reading and scheduled a Public Hearing date of April 23, 2018.

At the April 23<sup>rd</sup> Public Hearing, the majority of the submissions received expressed opposition to the proposal, citing concerns with proposed parking as well as the infill building setbacks as not keeping with the neighbourhood character. Other items that were mentioned in opposition were the proposed density and construction noise that the proposal



would bring, citing reference to the recently completed apartment building directly east of the subject site at 161 East Keith Road. Those in support identified the provision of rental housing and affordable rental units as key items. Other items of support were access to local amenities, that there was to be no displacement of existing tenants, and some felt the allowable density was appropriate.

After the conclusion of the Public Hearing, the application was not given Third Reading.

The applicant made changes to the proposal, as described in this report, and is seeking Council consideration.

## DISCUSSION

### Site Context and Surrounding Use

The subject site is located in the Lower Lonsdale area along the south side of East Keith Road, across from Victoria Park, between Lonsdale Avenue to the west and St. George's Avenue to the east. It is also a double frontage lot with East 6<sup>th</sup> Street directly to the south (Attachment #1).

The site currently contains a 15 storey residential tower with 89 rental apartment units, which was completed in 1975. The parking structure surrounds the tower and serves to support resident parking. From the East Keith Road side, the roof of the parkade creates a landscaped plaza.

A description of the buildings surrounding the subject site is outlined in Table 1 below.

**Table 1. Surrounding Uses**

Direction	Address	Zoning	OCP	Description
North (across the south arm of East Keith Road)	Victoria Park	Public Use and Assembly 1 (P-1)	Parks, Recreation and Open Space	Park Space
South (across East 6 <sup>th</sup> Street)	141 and 165 East 6 <sup>th</sup> Street	Medium Density Apartment Residential 1 (RM-1)	Residential Level 5. Max. 1.6 FSR with 1.0 FSR bonus density.	Two three-storey rental apartment buildings of 54 units each.
East	161-165 East Keith Road, and 160 E 6 <sup>th</sup> Street	Comprehensive Development 651 (CD-651)	Residential Level 6. Max. 2.3 FSR with 1.0 FSR bonus density.	16 storey 93 unit apartment building
West	123 East Keith Road	Comprehensive Development 155 (CD-155)	Residential Level 6. Max. 2.3 FSR with 1.0 FSR bonus density.	17 storey, 32 unit apartment building.

### Proposal

The main changes to the current proposal are:

- The removal of the three unit, two storey building located between East Keith Road and the tower at the northeast corner of the site;



- The units of that building were moved to the proposed southwest building and modified to three bedroom units to increase the total number of three bedroom units being proposed to six; and
- To address accessibility, the applicant is proposing a ramp be installed from East Keith Road and would be directed along the northeastern part of the site to give access not only to the existing tower, but between the two proposed infill buildings.

The drawings for the revised proposal can be found in Attachment #3.

The density of the current tower is 2.29 times the lot area (FSR), just under the Official Community Plan (OCP) maximum of 2.30. With the OCP allowing Council to consider an additional 1.0 FSR Density Bonus to the site, the applicant is proposing to use 0.88 FSR of that additional density to permit the addition of 40 rental housing units to the site.

The rental units within the existing tower are not secured. Through the proposed rezoning, the applicant has agreed to secure 100% of the units in the existing tower along with the 40 proposed units.

The applicant is not proposing replacing or undertaking any major renovations to the tower, other than upgrades to individual units when they become vacant. As a result, the applicant has stated that no displacement would result from the construction of the proposed infill buildings.

#### Building #1

This building is located at the northwest corner of the property and is a four storey rental apartment building containing 31 studio units. The orientation of the building toward East Keith Road provides the main access to the lobby. The ground floor units fronting Keith Road will have direct street access from their units.

To encourage social interaction amongst the residents as well as encouraging physical activity, this building proposes a number of elements that are outlined in the City's Active Design guidelines, including:

- A proposed indoor amenity room located adjacent to the lobby;
- To encourage stairwell use, the main stairway is visible upon entering the lobby as well as from the exterior of the building;
- A secondary stairway is also in a prominent location at the end of the hallway and is glazed for easy visibility; and
- A rooftop terrace, to promote social interaction for all residents of the property.

#### Building #2

This building is a four storey walk-up townhouse building located at the southwest corner of the subject site and contains nine units, including:

- Three studio units with direct outdoor access to the interior courtyard; and
- Six, three bedroom units with their own individual access to the same interior courtyard as the studio units. The other three bedroom units would be directly accessed from East 6<sup>th</sup> Street.

No rooftop decks are proposed with this building. The proposed setbacks of the infill buildings from the property lines remain unchanged.

### Public Realm

The application seeks to improve the interface between the site and one of the City's most prominent civic spaces and streets. Presently, the site's interface with the public realm consists of the parking structure and landscaping; this interface does not support an enhanced pedestrian environment. The proposed mid-rise infill buildings are ground related, orient their main unit entrances to the street and provide an active edge and street wall to the public realm. In doing this, it significantly improves the pedestrian experience and views into and out of the public realm. Consistent with many great and large parks across the country, there is a need to provide a sense of enclosure (or to delineate the space) through the provision of a street wall. The proposed development incorporates a mid-rise form that is human scaled, which will create a sense of enclosure by providing a street wall along Victoria Park.

### Proposed Parking and Access

The application is proposing to use the current parking structure to accommodate the off-street parking for both the proposed 40 units as well as the existing tower. The proposed 77 off-street parking stalls meets the minimum Zoning Bylaw requirement of 77 parking stalls.

The parkade will also house the required number of bicycle parking for the proposed infill units as well as the recycling and garbage enclosure. The enclosures for the current tower are contained within that building.

## **PLANNING ANALYSIS**

### *2014 Official Community Plan*

The Official Community Plan identifies the subject site as Residential Level 6 (R6), which permits a residential development of 2.3 times the size of the site (FSR) with the potential of a 1.0 FSR density bonus to be granted at the discretion of Council for an overall density of 3.30 FSR. The OCP also limits the maximum height of a building to 46 metres (151 feet) tall.

The combined density of the existing tower with the proposed infill buildings would result in a FSR of 3.17 and will not exceed the OCP maximum.

The City's Official Community Plan is centered on a set of guiding principles and a sustainable city framework. Proceeding with the proposed zoning and redevelopment of the subject site will advance many of these goals and objectives, particularly the following:

- 1.1.1 Plan for growth in the City's population, dwelling units and employment in keeping with the projections in Metro Vancouver's regional Growth Strategy;
- 1.3.1 Ensure that new development is compatible with the established urban form of the City, reflecting the primacy of the Lonsdale Regional City Centre and the transition through mid- and low rise buildings to lower density neighbourhoods;



- 1.3.5 Encourage design excellence in developments through carefully considered, high quality architecture and landscaping, with varied designs which are interesting, sensitive and reflective of their surroundings;
- 1.3.10 Encourage active, healthy lifestyles and the opportunity for more social connections through planning and active design principals that encourage physical activity and contribute to enhanced walkability and active streets, sidewalks and public spaces;
- 1.5.4 Prioritize the development and revitalization of affordable rental housing and use density bonusing and use density bonusing and density transfers to incentivise the retention, renewal and/or replacement of rental units as a public benefit;

The proposal as presented will not require an amendment to the OCP.

*Zoning Bylaw 1995, No. 6700*

The property is currently zoned High Density Apartment Residential 1 (RH-1). An amendment to the Zoning Bylaw is required to permit the proposal as presented and is reflected in Zoning Amendment Bylaw 8618 (Attachment #9). Additionally, current zoning requirements are outlined in the Project Fact Sheet.

Using the RH-1 Zone as a base, the proposal is in conformance with the Zoning Bylaw with the exceptions to permit a density of 3.17 FSR, to allow a minimum parking stall width of 2.4 metres (8.0 feet), and setbacks to the proposed buildings as outlined in the following table:

**Table 2.** *Proposed Building Setbacks*

Building	Setback	Distance
Building #1	North setback	2.4 m (8.0 ft.)
	West setback	3 m (10.0 ft.)
		1.6 m (5.41 ft.) for stairwell
Building #2	South setback	1.6 m (5.33 ft.)
	West setback	6 m (23.17 ft.)

The requested setback variances are to support the proposed building locations for a low-rise development within a property whose current zoning supports greater building setback distances for high-rise buildings. The current setback requirement of 7.6 metres (25 feet) from all property lines would not support low-rise infill structures in addition to an existing tower. Additionally, the setback variances are needed to create a consistent street wall and minimize gaps between buildings; the consistent street wall helps to support walking.

The proposed setback from the proposed infill buildings is supportable in multi-family areas with side yard setbacks for more recent low and mid-rise buildings ranging from 2.4 metres (8.0 feet) to 3.6 metres (12.0 feet) from the property line. The projection of the proposed stairwell in Building #1 is limited to the width of the stairway and is a minor projection from the overall depth of the building.

The separation between the proposed Building #1 and the existing residential tower to the west at 123 East Keith Road is 15.0 metres (49.2 feet), there is slight projection of a stairway within that setback.

The current zoning requirements can be found in the Project Face Sheet in Attachment #2.



The proposed parking variance is due to the applicant proposing to use the existing parking structure. At the time it was built, the required parking stall width was 2.44 metres (8 feet). Since the City received this application, the Zoning Bylaw was amended to increase the width to 2.5 metres (8.2 feet). This would affect the spacing of the existing columns supporting the structure of the parkade. For the applicant to achieve their proposed number of parking stalls, the variance is necessary.

To secure the existing residential tower and the proposed additional units, the Comprehensive Development Zone in Amendment Bylaw No. 8618 limits the use to Rental Apartment Residential.

#### *Housing Action Plan*

To achieve the requested 0.88 FSR Density Bonus for this proposal, the City's Density Bonus and Community Benefits Policy, as well as the City's Housing Action Plan, would require that 10% of the new units be rented at 10% below market rates in perpetuity (Mid-Market units). As the existing tower is not being replaced or undergoing a major renovation, the 89 current units are not included with the Mid-Market unit count.

To meet the requirement, the applicant is partnering with Hollyburn Family Services to provide four studio units (10% of the total number of new units) at rates less than what the City's Mid-Market units could achieve by being able to tap into the SAFER subsidy to rent to vulnerable seniors. The applicant has updated their Affordable Housing statement (Attachment #5) to outline this proposal, and this is reflected in the Housing Agreement, which will secure these rates in perpetuity (Attachment #10). It should be noted that rents would revert to mid-market rental rates in accordance with City Policy if the contract with Hollyburn Family Services is terminated.

The updated Affordability Statement outlines the rate difference between the City's Mid-Market units and what the SAFER subsidy will be able to provide using 2018 numbers. The chart below outlines the difference between the two rates as well as market rates.

	April 2019 market rents*	2018 mid- market rents	Percentage difference from market	SAFER subsidized rates	Percentage difference from market
Studio	\$1,500	\$1,029	31% below	\$800	47% below

\* Current market rents were obtained from a new market rental development in Central Lonsdale, advertised on Craigslist, in April 2019

To address the Family-Friendly Housing Section of the Housing Action Plan, the applicant is proposing 6 of the 40 new units be three bedroom units to give families unit type options. The existing tower contains primarily one and two bedroom units.

#### **Community Amenities/Infrastructure**

The City's *Density Bonus and Community Benefits Policy*, in conjunction with the Official Community Plan, allows Council to consider a density bonus beyond the OCP limit of 2.30 FSR, up to a maximum of 3.30 FSR in the Residential Level 6 area.

The policy provides a number of community benefit options for projects seeking additional density and seeks to ensure the City receives value for additional density granted. The value could be in the form of a cash contribution or some form of amenity, such as an all rental apartment building to be secured in perpetuity plus a form of affordability in



accordance with the City's Housing Action Plan. In this case, to achieve the desired 0.88 FSR bonus density for the new infill buildings, the applicant has agreed to secure the existing residential tower and the proposed additional 40 units as rental in perpetuity, as well as partnering with Hollyburn Family Services to secure four units using the SAFER rental rate for vulnerable seniors.

As the proposed rental rates for the four Mid-Market units would exceed what the City would achieve through acquiring the same number of units through the Housing Action Plan, the proposal meets the intent of the Policy and no community amenity cash contribution is required for this development proposal.

For comparison, a cash contribution in accordance with the Policy for this density bonus would be valued at approximately \$3.45 million, as outlined in Table 2 below. The City would receive rental housing in lieu of a cash contribution.

**Table 3.** *Estimated Value of Community Benefits through Density Bonusing*

Density Value Calculation	Value
Density Addition from 2.29 FSR to 2.30 FSR / OCP Density (@ \$20 / sq. ft.)	\$5,600
Density Bonus to 0.88 FSR / OCP Density Bonus (@ \$140 / sq. ft.)	\$3,449,354
<b>Total Value of Community Benefits</b>	<b>\$3,454,954</b>

Staff also seeks Council's endorsement for a financial contribution of \$25,000 from the applicant to help finance a public Electric Vehicle Charging Station within the City. This item is not specifically required under the authority of City bylaws, but could be made a requirement of rezoning under the authority of Council. This contribution would be payable prior to adoption of rezoning.

## COMMUNITY CONSULTATION

The applicant held a Developer Information Session (DIS) to invite the public to gather information and comment on the most recent proposal. The event was held on March 27, 2019 where 46 people attended. At the meeting, a total of 28 comment sheets were submitted. The applicant provided a report of the meeting that includes comments provided on that day. Staff included comments that were received directly or after the event (Attachment #4).

The feedback received showed that most were opposed to the application for the following reasons:

- The building setbacks from the property lines along the west property line, as well as fronting East Keith Road and East 6<sup>th</sup> Street are too small;
- Lack of off-street parking;
- Increased traffic in the neighbourhood; and
- Low to mid-rise building is not in character with the neighbourhood or the streetscape.

Those who supported the application (and some in opposition) liked that there would be more rental units, and some below market units would be provided. Some liked the overall appearance of the buildings.

Should Council grant the amended second reading to proposed Bylaw No. 8618 and No. 8622, a Public Hearing will be required.

## **ADVISORY BODY INPUT**

### **Advisory Design Panel**

The updated proposal was not forwarded to the Advisory Design Panel as the overall architecture and landscaping had not significantly changed from the last time Council viewed them. In addition, the overall scope of the proposal has been reduced due to the elimination of one of the infill buildings along the east side of the subject site.

For further information on the items and resolutions, please refer to the March 7, 2018 staff report (Attachment #7).

## **FINANCIAL IMPLICATIONS**

A financial contribution of \$25,000 is to be received prior to adoption of rezoning that will go towards the installation of a public Electric Charging Station in the City.

A cash contribution for additional density is not required for rental property as noted in the Community Amenities/Infrastructure section above.

## **SUSTAINABILITY COMMENTS**

The subject site is one half-block from Lonsdale Avenue and has good access to public transit. Victoria Park (which contains a portion of the Green Necklace) is across the street. The proposed development would be using unused portions of the site on top of an existing parkade while maintaining enough space for landscaping.

The applicant will also be:

- Connecting the infill buildings to Lonsdale Energy Corporation (LEC) and providing the infrastructure for the future connection of the existing residential tower to LEC;
- Contributing towards an Electric Vehicle Charging Station in the City;
- Using high-performance windows in the building envelope;
- Using energy saving certified appliances; and
- Installing low consumption water fixtures.

The applicant has filled out the Sustainable Development Guideline Checklist as part of their application to identify their intent on achieving the City's sustainability goals. These are shown in Attachment #6.

## **INTER-DEPARTMENTAL IMPLICATIONS**

Staff's review of the revisions showed no changes to their previous comments and all issues would be managed through the Building Permit process.

RESPECTFULLY SUBMITTED:

  
David Johnson  
Development Planner







# PROJECT SUMMARY SHEET

DEVELOPMENT APPLICATION

151 East Keith Road



## SITE CHARACTERISTICS

OCP Designation	Residential Level 6 (R6)
Existing Zoning	High Density Apartment Residential 1 (RH-1)
Site Area	2,601.1 sq. m (27,998 sq. ft.)

## FLOOR AREA AND HEIGHT

	Existing Zoning (RH-1)	Official Community Plan	Proposed
Floor Space Ratio	Maximum 2.60 FSR or 6,762.86 sq. m (72,795 sq. ft.)	OCP - 2.30 FSR 5,982.53 sq. m (64,395.4 sq. ft.) Density Bonus - 1.0 FSR 2,601.1 sq. m (27,998 sq. ft.)  <b>Total Maximum</b> <b>8,583.63 m² (92,393.4 sq. ft.)</b> <b>3.30 FSR</b>	Existing Building 5,962.98 sq. m (64,185.0 sq. ft.) 2.29 FSR Proposed Addition 2,288.11 sq. m (24,629.0 sq. ft.) 0.88 FSR  <b>Proposed Total</b> <b>8,251.09 sq. m (88,814.0 sq. ft.)</b> <b>3.17 FSR</b>
Total Lot Coverage	50%	N/A	40.1%
Principal Building Height (maximum)	30.48 m (100.0 ft.)	46.0 m (151.0 feet)	Building 1 - 16.13 m (52.91 feet) Building 2 - 13.26 m (43.51 feet)

## SETBACKS

	Existing Zoning (RH-1)	Proposed
North (East Keith Road)	7.62 m (25.0 ft.)	2.40 m (7.83 ft.)
East Side Yard	7.62 m (25.0 ft.)	29.36 m (96.32 ft.)
West Side Yard	7.62 m (25.0 ft.)	1.65 m (5.41 ft.)
South (East 6th Street)	7.62 m (25.0 ft.)	1.63 m (5.33 ft.)

## BICYCLE PARKING

	Required	Proposed
Residential (stalls for additional units)	60 secured / 6 short term	62 secured / 6 short term

## VEHICLE PARKING

	Required	Proposed
Total Vehicle Parking (stalls)	Existing and Proposed: 77 (includes 13 visitor)	77 (includes 13 visitor)

Numbers based on plans dated February 4, 2019

#1765369

# PROPOSED RESIDENTIAL INFILL DEVELOPMENT

151 EAST KEITH ROAD  
NORTH VANCOUVER B.C.



REISSUED FOR REZONING APPLICATION - FEBRUARY 4th, 2019



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## PROJECT TEAM

### DEVELOPER

STARLIGHT INVESTMENTS  
1400 - 3280 BLOOR STREET WEST,  
CENTRE TOWER, TORONTO ON M8X 2X3

Founded in 2011, Starlight Investment Ltd. is a real estate asset management firm with a primary focus on multifamily rental properties in over 400 properties across Canada and the Southern United States. Starlight's innovative approach to the properties under its management is reflected in its commitment to excellence in quality design and construction for both existing assets and new infill development projects alike.

### ARCHITECT

BHA ARCHITECTS  
205-1628 WEST 1ST AVE.  
VANCOUVER BC V6J 1G1

BHA Architects (formerly Burrowes Huggins Architects) was founded in 1991 and since that time has been actively involved in a variety of developments, and worked with many local and regional development companies. BHA completed work includes a number and variety of Whistler buildings including the Four Seasons Resort and Residences, the Athlete's Lodge in Cheakamus Village and a mix of resort condominiums in the Blackcomb Benchlands.

BHA Architects has longstanding experience in the Lower Mainland with mixed-use and ground-oriented housing projects including street-oriented townhouses, stacked townhouses, 4 to 6 storey mixed-use retail residential projects, purpose-built rental housing and not for profit housing projects.

### LANDSCAPE ARCHITECT

CONNECT LANDSCAPE ARCHITECTURE  
2305 HEMLOCK STREET  
VANCOUVER BC V6H 2V1

Connect Landscape Architecture (formerly Sharp & Diamond) is a recognized leader in the design and transformation of healthy vibrant communities. Our vision is simple - Connect people to their environment through meaningful design. Connect Landscape Architecture draws inspiration from the natural landscape while embracing built form and infrastructure.

### PLANNING CONSULTANT

URBAN SYSTEMS  
550-1090 HOMER STREET  
VANCOUVER BC V6B 2W9

Urban Systems is a multi-disciplinary consulting firm founded in 1975. At our 14 offices across 5 provinces, we bring spirit in service for vibrant communities and have been recognized as one of the Best Workplaces in Canada for 12 years running. With over forty years of land development experience, we assist clients in moving through the different stages of development as quickly and efficiently as possible. Urban Systems promotes sustainable development methods that add efficiency, improve performance, and provide marketing value to development projects. Our integrated approach to land development has resulted in some of the most celebrated development projects in Western Canada. We provide the full range of services required to take projects from start to finish, including land use planning and design, civil engineering, landscape architecture, legal survey, and construction services. We assemble a customized team of professionals for every new assignment to design and construct all aspects of each unique site.

# PROJECT STATISTICS

Project Name:	RESIDENTIAL INFILL DEVELOPMENT				SF	SM	ACRES	HA	Units Per Acre	Units Per HA	Date:	9-Jan-17	
Project Number:	P424				Gross Site Area:	27997.90 sf	2601.09 sm	0.64 acres	0.260 ha	n/a	n/a	Revised:	
Client:	STARLIGHT INVESTMENTS												
TOTAL FSR SUMMARY (ALL NEW BUILDINGS)													
					Amenity Provided:					Site Coverage:			
					Required:					n/a			
					Provided:					166 sf 15.42 sm			
										Site Area 27997.9 sf			
Total GFA					27998 sf	1.00 for	24629 sf	2288.11 sm	Actual FSR	0.88 for	Existing Footprint	4432 sf	15.90%
Storage					0 sf	0.00 sm					New Footprint	6780 sf	24.22%
Total FSR					24629 sf	2288.11 sm	0.88 for				Total Coverage	40.12%	
BUILDING ONE AREAS (APARTMENT BUILDING)													
LEVEL		RES LOBBY	CIRCULATION	RESIDENTIAL	AMENITY	GFA	EFF	NO UNITS	STORAGE	FSR	GFA CHECK		
1		(NOTE: PROJECT LEVEL 3 ON FLOOR PLANS)	104 sf	1013 sf	2823 sf	166 sf	4105 sf	68.77 %	7	0 sf	3939 sf	4105 sf	
2		(NOTE: PROJECT LEVEL 4 ON FLOOR PLANS)	0 sf	977 sf	3220 sf	0 sf	4197 sf	76.72 %	0	0 sf	4197 sf	4197 sf	
3		(NOTE: PROJECT LEVEL 5 ON FLOOR PLANS)	0 sf	977 sf	3217 sf	0 sf	4194 sf	76.70 %	0	0 sf	4194 sf	4194 sf	
4		(NOTE: PROJECT LEVEL 6 ON FLOOR PLANS)	0 sf	977 sf	3217 sf	0 sf	4194 sf	76.71 %	0	0 sf	4194 sf	4194 sf	
5		(NOTE: PROJECT LEVEL 7 ON FLOOR PLANS)	0 sf	428 sf	0 sf	0 sf	428 sf	0.00 %	0	0 sf	428 sf	428 sf	
TOTAL		104 sf	4371 sf	12477 sf	166 sf	17118 sf	72.89 %	31 units	0 sf	16952 sf	Less amenities & site	17118 sf	
		9.66 sm	406.08 sm	1159.14 sm	15.42 sm	1590.32 sm			0.00 sm	1574.89 sm		1590.30 sm	
BUILDING TWO AREAS (TOWNHOUSE BUILDING)													
TOWNHOUSE UNIT TYPE		UNIT NO	UNIT TYPE	FLOOR	LOWER	MID	UPPER	TOTAL	NO UNITS	TOTAL GFA	STORAGE	TOTAL FSR	
A3		125	STUDIO TOWNHOUSE	405 sf	0 sf	0 sf	405 sf	37.67 sm	1 units	405 sf	37.67 sm	0 sf	405 sf
A4		124	STUDIO TOWNHOUSE	409 sf	0 sf	0 sf	409 sf	37.99 sm	1 units	409 sf	37.99 sm	0 sf	409 sf
A5		126	STUDIO TOWNHOUSE	409 sf	0 sf	0 sf	409 sf	37.99 sm	1 units	409 sf	37.99 sm	0 sf	409 sf
B4		128	2 STOREY TOWNHOUSE (3 BR)	405 sf	0 sf	629 sf	1033 sf	96.00 sm	1 units	1033 sf	96.00 sm	0 sf	1033 sf
B5		127	2 STOREY TOWNHOUSE (3 BR)	408 sf	0 sf	634 sf	1042 sf	96.84 sm	1 units	1042 sf	96.84 sm	0 sf	1042 sf
B6		129	2 STOREY TOWNHOUSE (3 BR)	408 sf	0 sf	634 sf	1042 sf	96.84 sm	1 units	1042 sf	96.84 sm	0 sf	1042 sf
C1		122	3 STOREY TOWNHOUSE (3 BR)	296 sf	405 sf	405 sf	1106 sf	102.70 sm	1 units	1106 sf	102.70 sm	0 sf	1106 sf
C2		121	3 STOREY TOWNHOUSE (3 BR)	299 sf	408 sf	408 sf	1115 sf	103.59 sm	1 units	1115 sf	103.59 sm	0 sf	1115 sf
C3		123	3 STOREY TOWNHOUSE (3 BR)	299 sf	408 sf	408 sf	1115 sf	103.59 sm	1 units	1115 sf	103.59 sm	0 sf	1115 sf
TOTALS								9 units	7677 sf	0 sf		7677 sf	
										713.21 sm		713.21 sm	
PROJECT SUMMARY TOTALS													
NEW INFILL PROJECT TOTALS								40 units		24629 sf FSR	2288.11 sm	0.88 for	
EXISTING BUILDING PROJECT TOTALS								89 units		64185 sf FSR	5962.98 sm	3.17 for	
TOTALS FOR NEW INFILL AND EXISTING								129 units		88814 sf FSR	8251.09 sm	2.29 for	

# PARKING

## PARKING STATISTICS

			DATE	27-Jul-16
			REVISED	29-Oct-18
TOTAL PARKING PROVIDED BY EXISTING BUILDING				
	EXISTING REGULAR STALLS	H/C STALLS		TOTAL
LEVEL 1	51 stalls	0 stalls		51 stalls
LEVEL P1	53 stalls	0 stalls		53 stalls
TOTAL	104 stalls	0 stalls		104 stalls

## SUMMARY OF UNITS/PARKING

EXISTING BUILDING	
2 BEDROOM UNITS	28 units
1 BEDROOM UNITS	60 units
STUDIO	1 units
TOTAL UNITS EXISTING BUILDING	89 units
NEW BUILDING	
STUDIO APARTMENTS	34 units
TOWNHOUSE UNITS	6 units
TOTAL UNITS NEW BUILDING	40 units
TOTAL UNITS EXISTING AND NEW	
TOTAL PARKING PROVIDED	129 units
	77 stalls
	0.60 stalls per unit

# ZONING SUMMARY

ZONING SUMMARY - INFILL DEVELOPMENT		PROJECT		KEITH ROAD RESIDENTIAL INFILL DEVELOPMENT	
PROJECT NO.	P424	IMPERIAL	METRIC	VALUE	NOTES
1	CIVIC ADDRESS:				
	151 East Keith Road, City of North Vancouver		LOT: B	BLOCK: 114	DL: 274      PLAN: 15049
	PID: 007-697-481				
2	APPLICATION DESCRIPTION:				
	To construct a residential infill development consisting of 2 buildings and 40 rental units.				
3	LOT AREA				
	EXISTING	27997.90 sf	2601.09 sm		
	PROPOSED	27997.90 sf	2601.09 sm		
4	ZONING				
	EXISTING	RH-1			
	PROPOSED	CD			
5	OFFICIAL COMMUNITY PLAN (OCP):				
	As per the OCP, the site is Residential Level 6 (High Density) - refer to Schedule A Land Use Map.				
	Existing OCP Density (FSR) = 2.3 with a Maximum Bonus (FSR) of up to 1.0. 2.3 + 1.0 = 3.3 Total Permitted FSR				
6	GROSS FLOOR AREA				
	PERMITTED: 1.0 Bonus FSR	27998.00 sf	2601.09 sm		
	PROPOSED: 0.88 FSR	24629.00 sf	2288.11 sm		= 0.88 FSR
7	RESIDENTIAL UNIT MIX				
	STUDIO APARTMENTS: 31 units (includes 14 Adaptable Level 2 Units)				
	TOWNHOUSE UNITS: 9 units (includes 3 Studio Townhouses, 3 2-Storey Townhouses and 3 3-Storey Townhouses)				
	TOTAL: 40 Units				
8	VEHICLE PARKING				
	Refer to Parking Statistics				
9	BICYCLE PARKING				
	REQUIRED	PROPOSED			
	LONG TERM:	60 (40 units x 1.5)		62.00	
	SHORT TERM:	6.00		6.00	

# UNIT SUMMARY BUILDING 1

## BUILDING 1 STRATA LOT UNIT SUMMARY

Floor	Unit #	Type	Area (sf)	Area (sm)	Storage (sf)	Storage (sm)	Net Area (sf)	Net Area (sm)	Balcony (sf)	Balcony (sm)
1	101	A6	414 sf	38.13 sm	0 sf	0.00 sm	414 sf	38.13 sm	0 sf	0.00 sm
1	102	A1	404 sf	37.53 sm	0 sf	0.00 sm	404 sf	37.53 sm	0 sf	0.00 sm
1	103	A1	400 sf	37.21 sm	0 sf	0.00 sm	400 sf	37.21 sm	0 sf	0.00 sm
1	104	A2	404 sf	37.53 sm	0 sf	0.00 sm	404 sf	37.53 sm	0 sf	0.00 sm
1	105	D2	404 sf	37.53 sm	0 sf	0.00 sm	404 sf	37.53 sm	0 sf	0.00 sm
1	106	D3 (ADAPTABLE)	400 sf	37.21 sm	0 sf	0.00 sm	400 sf	37.21 sm	0 sf	0.00 sm
1	107	D3 (ADAPTABLE)	400 sf	37.21 sm	0 sf	0.00 sm	400 sf	37.21 sm	0 sf	0.00 sm
Level 1 Totals:			2823 sf	262.36 sm	0 sf	0.00 sm	2823 sf	262.36 sm	0 sf	0.00 sm
2	201	D4 (ADAPTABLE)	407 sf	37.81 sm	0 sf	0.00 sm	407 sf	37.81 sm	0 sf	0.00 sm
2	202	D3 (ADAPTABLE)	400 sf	37.21 sm	0 sf	0.00 sm	400 sf	37.21 sm	0 sf	0.00 sm
2	203	D1	400 sf	37.21 sm	0 sf	0.00 sm	400 sf	37.21 sm	0 sf	0.00 sm
2	204	D1	400 sf	37.21 sm	0 sf	0.00 sm	400 sf	37.21 sm	0 sf	0.00 sm
2	205	D2	404 sf	37.53 sm	0 sf	0.00 sm	404 sf	37.53 sm	0 sf	0.00 sm
2	206	D2	404 sf	37.53 sm	0 sf	0.00 sm	404 sf	37.53 sm	0 sf	0.00 sm
2	207	D3 (ADAPTABLE)	400 sf	37.21 sm	0 sf	0.00 sm	400 sf	37.21 sm	0 sf	0.00 sm
2	208	D3 (ADAPTABLE)	404 sf	37.53 sm	0 sf	0.00 sm	404 sf	37.53 sm	0 sf	0.00 sm
Level 2 Totals:			3220 sf	299.25 sm	0 sf	0.00 sm	3220 sf	299.25 sm	0 sf	0.00 sm
3	301	D4 (ADAPTABLE)	407 sf	37.78 sm	0 sf	0.00 sm	407 sf	37.78 sm	0 sf	0.00 sm
3	302	D3 (ADAPTABLE)	400 sf	37.21 sm	0 sf	0.00 sm	400 sf	37.21 sm	0 sf	0.00 sm
3	303	D1	400 sf	37.21 sm	0 sf	0.00 sm	400 sf	37.21 sm	0 sf	0.00 sm
3	304	D1	400 sf	37.21 sm	0 sf	0.00 sm	400 sf	37.21 sm	0 sf	0.00 sm
3	305	D2	404 sf	37.53 sm	0 sf	0.00 sm	404 sf	37.53 sm	0 sf	0.00 sm
3	306	D2	404 sf	37.53 sm	0 sf	0.00 sm	404 sf	37.53 sm	0 sf	0.00 sm
3	307	D3 (ADAPTABLE)	400 sf	37.21 sm	0 sf	0.00 sm	400 sf	37.21 sm	0 sf	0.00 sm
3	308	D3 (ADAPTABLE)	401 sf	37.29 sm	0 sf	0.00 sm	401 sf	37.29 sm	0 sf	0.00 sm
Level 3 Totals:			3217 sf	298.98 sm	0 sf	0.00 sm	3217 sf	298.98 sm	0 sf	0.00 sm
4	401	D4 (ADAPTABLE)	407 sf	37.78 sm	0 sf	0.00 sm	407 sf	37.78 sm	0 sf	0.00 sm
4	402	D3 (ADAPTABLE)	400 sf	37.21 sm	0 sf	0.00 sm	400 sf	37.21 sm	0 sf	0.00 sm
4	403	D1	400 sf	37.21 sm	0 sf	0.00 sm	400 sf	37.21 sm	0 sf	0.00 sm
4	404	D1	400 sf	37.21 sm	0 sf	0.00 sm	400 sf	37.21 sm	0 sf	0.00 sm
4	405	D2	404 sf	37.53 sm	0 sf	0.00 sm	404 sf	37.53 sm	0 sf	0.00 sm
4	406	D2	404 sf	37.53 sm	0 sf	0.00 sm	404 sf	37.53 sm	0 sf	0.00 sm
4	407	D3 (ADAPTABLE)	400 sf	37.21 sm	0 sf	0.00 sm	400 sf	37.21 sm	0 sf	0.00 sm
4	408	D3 (ADAPTABLE)	401 sf	37.29 sm	0 sf	0.00 sm	401 sf	37.29 sm	0 sf	0.00 sm
Level 4 Totals:			3217 sf	298.98 sm	0 sf	0.00 sm	3217 sf	298.98 sm	0 sf	0.00 sm
Overall Totals:			12477 sf	1159.57 sm	0 sf	0.00 sm	12477 sf	1159.57 sm	0 sf	0.00 sm

151 East Keith Road | Proposed Infill Development



# DESIGN RATIONALE

## PROJECT DESCRIPTION

The project proposes two new purpose-built infill rental buildings and a new ramp/pathway for barrier-free access on the property at 151 East Keith Road. The existing site incorporates a 14-storey purpose-built rental tower (1972) and a 2-storey underground parking garage. The site is located on the south side of Keith Road, south of Victoria Park and two blocks east of Lonsdale. The existing rental tower is flanked by a 14-storey condominium tower to the west and a 14-storey condominium tower to the east.

## SITE ATTRIBUTES

The site affords a variety of opportunities that support the addition of new rental housing: The site fronts onto Victoria Park and offers a significant opportunity for street-oriented housing that faces the park. The site slopes to the south and affords an unequalled opportunity for views across Burrard Inlet and a full south exposure. The existing tower is sited to the east of the site and as such enables additional structures to be located to the west. The slope of the site enables the new infill buildings to incorporate a large amount of ground-oriented housing, a preferred model for families.

## SITE PLANNING AND DISPOSITION

The site plan incorporates two new structures and a new ramp / pathway that provides barrier-free access from East Keith Road to the existing tower and throughout the site.

**Building 1:** a 4-storey apartment building fronting East Keith Road.

**Building 2:** a 4-storey ground-oriented townhouse building fronting East 6th Street.

Buildings 1 and 2 are separated with an internal courtyard which enables access to six stacked townhouses in Building 2, while access to three stacked townhouses in Building 1 is from East 6th Street. Building 1 is accessed from Keith Road and has a central elevator lobby which also provides barrier-free access to the parkade levels from inside and outside the building.

## UNIT TYPE AND DISPOSITION

The project incorporates a combination of studio units and 3-bedroom units. Building 1 contains 31 studio suites with ground-level north-facing suites having access directly from Keith Road. Building 2 incorporates three studio townhouse units and six 3-bedroom townhouses, all with ground level or courtyard level access.

When combined with the 1 and 2-bedroom suites in the existing tower, the overall project achieves a more varied mix of unit types.

## LANDSCAPE AND OPEN SPACE

The existing landscaping above the parking garage including the mature trees on site along Keith Road will have to be replaced in order to construct the new buildings. The proposal, however, includes a new ramp / pathway that provides barrier-free access from Keith Road to the existing tower as well as to a new landscaped courtyard central to the site. Here, a new indoor/outdoor elevator available to all residents can be used for access to the parkade levels. Furthermore, new exterior stairs to the east of Building 2 allow pedestrian travel from East 6th Street through the site to East Keith Road and Victoria Park. The north-facing ground level units in Building 1 will have small, street-oriented garden spaces fronting Victoria Park while the south-facing ground level units in Building 2 will incorporate landscaped gardens facing East 6th Street. Building 1 will have a rooftop garden and amenity space while the 3-bedroom units in Building 2 will have generous south-facing roof terraces.

## ENGINEERING CONSIDERATIONS

The proposed three infill buildings are constructed above an existing 2-storey concrete parkade. The parkade, built in 1973, is constructed with a flat slab, 12" x 24" columns and perimeter foundation walls. A number of constraints and challenges arise:

- The floor to floor height is 8'-6" which results in barely 2m clear headroom. As such, there is no practical way to add anything into the parking level ceiling space. Any encumbrances such as structural elements, ductwork, pipes, additional lighting etc. would restrict the headroom to less than 2m which is both unacceptable and non-code compliant.
- All new structural loads can only be supported directly above the columns.
- Any major intervention or renovation of the existing parking structure would necessitate a building code upgrade which is both impractical and unfeasible.

The project addresses the "design constraints" outlined above as follows:

- Construction of a structural podium below the footprint of each new building. This space allows the structural loads to be transferred to the existing columns and provides an interstitial space for the plumbing and mechanical services.
- The limited intervention into the existing parking garage includes a new elevator servicing both levels of the parking structure and provision of bicycle storage adjacent to each existing exit stair.
- Ongoing discussions are in progress between the City of North Vancouver Engineering Department, the Lonsdale Energy Corporation and MCW Mechanical Consultants to develop a strategy for servicing the new infill buildings. LMDG Building Code Consultants are preparing a building code concept report to address 1) the existing tower, 2) the existing parking garage, and 3) the new infill buildings. This report has been included with the rezoning / development permit application.

Both mechanical and electrical consultant reports are attached to the application.

## ARCHITECTURAL CHARACTER

The new buildings provide a new, contemporary streetscape for the East Keith Road and East 6th Street frontages. These are new "homes on the park". As such, the expression of the massings adopts a "row house" approach utilizing a portal or balcony enclosure to identify each unit, or in the case of Building 1, each unit module. The buildings are crafted in a West Coast but modern character utilizing simple planes, flat roofs, generous overhangs and contemporary glazed railings. A simple palette of materials include panelized walls (HardiePanel), horizontal siding, and metal siding and soffits with a woodgrain finish. The glazing is contemporary in its fenestration. All the soffits where visible to the street are clad in natural wood coloured material with associated trim. In sum, the proposed buildings together with the updated landscaping successfully provide a new, refreshing and contemporary streetscape facing Victoria Park.





## AREA PLAN

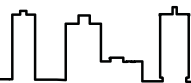
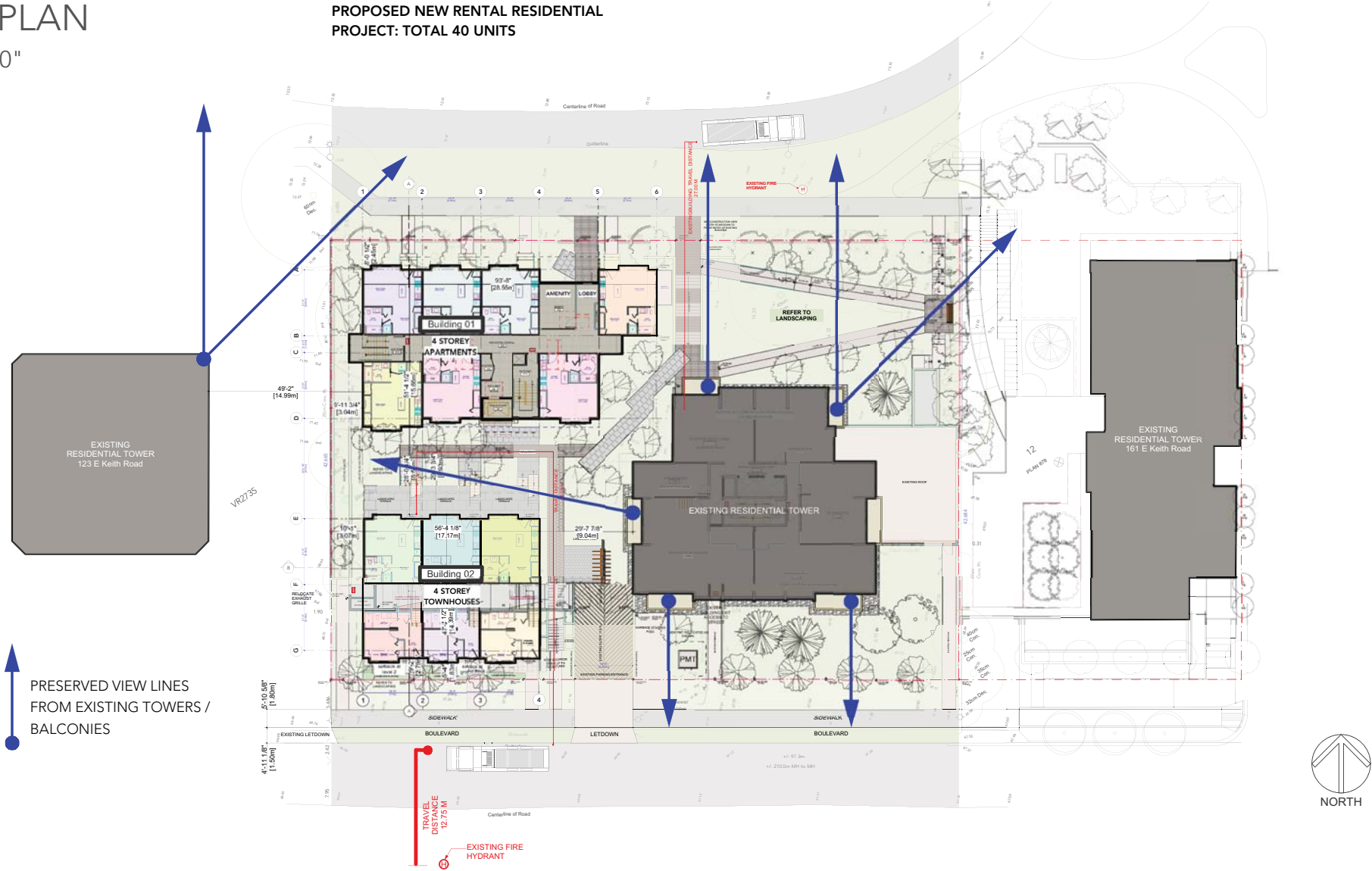
1" = 200'-0"



SITE PLAN

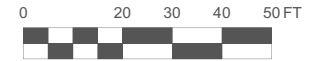
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PROPOSED NEW RENTAL RESIDENTIAL  
PROJECT: TOTAL 40 UNITS

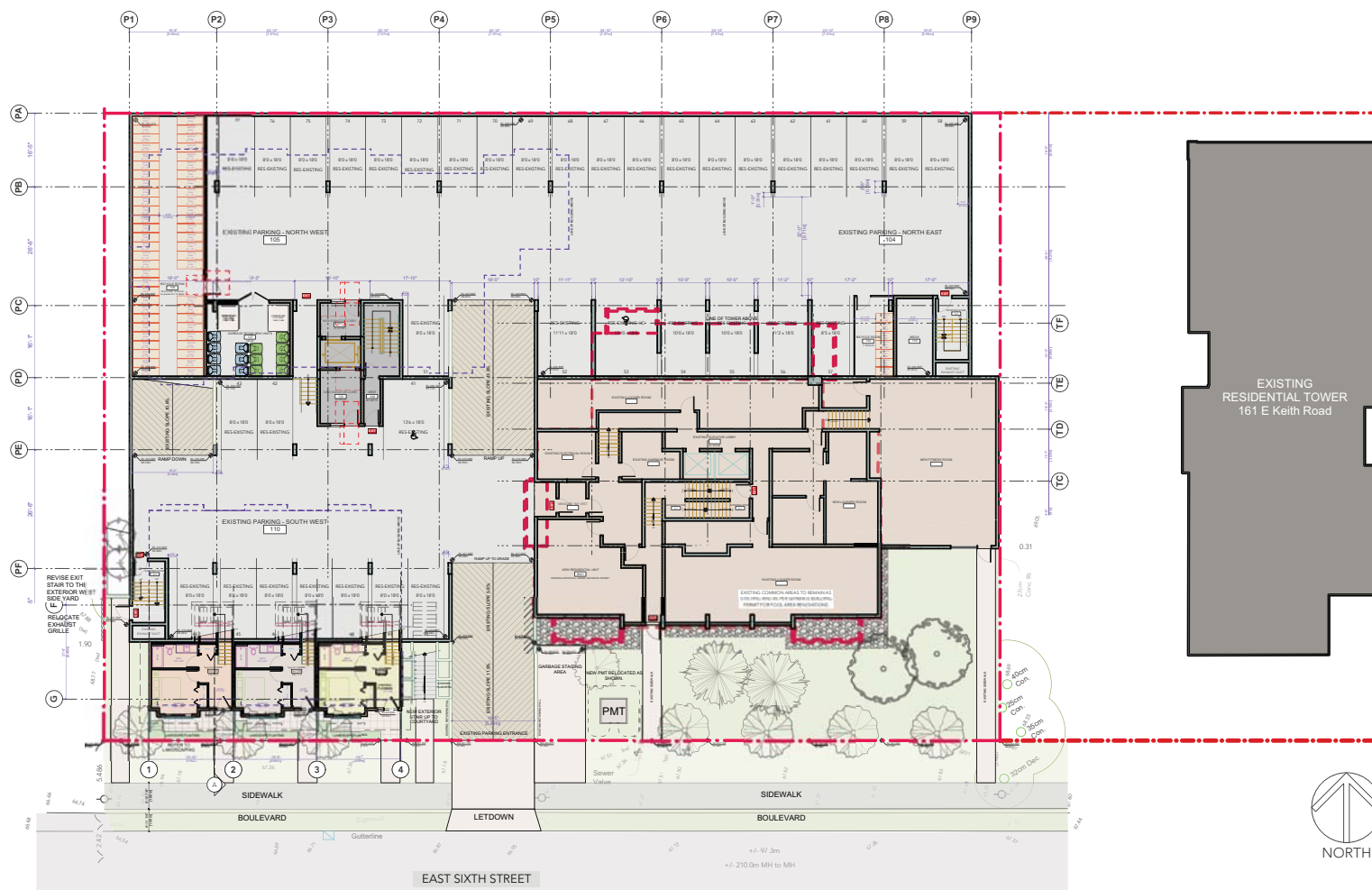




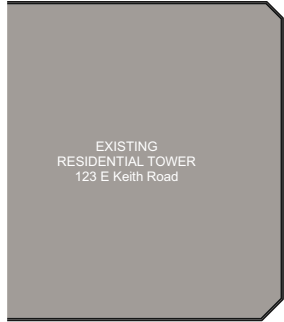
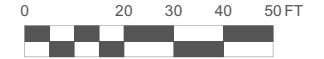
# P1 PLAN



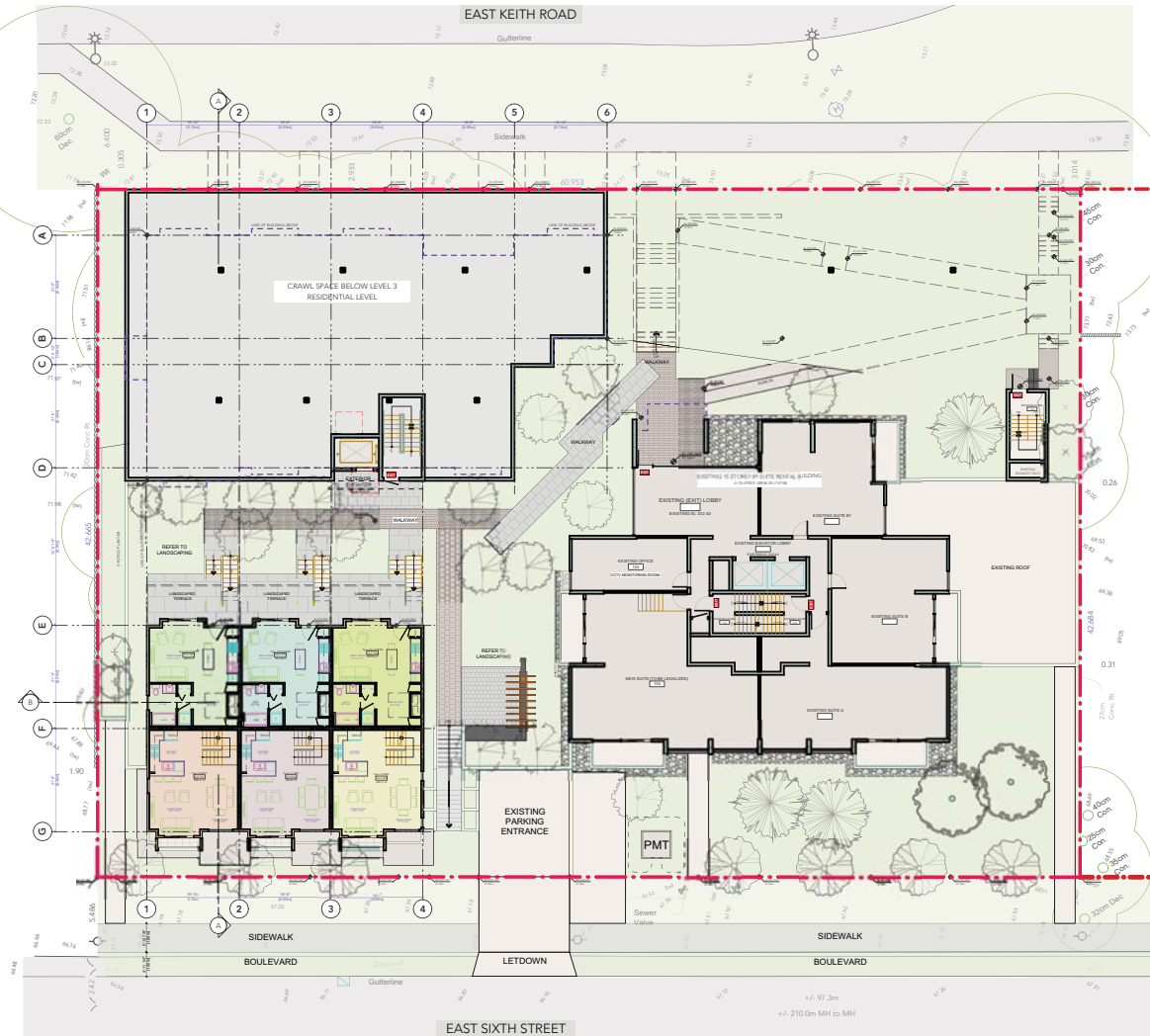




# LEVEL 2 PLAN



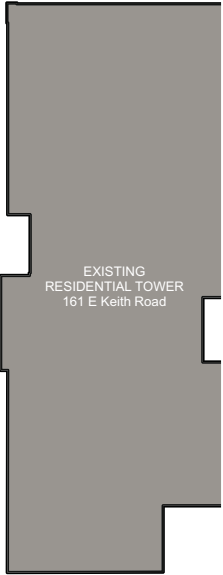
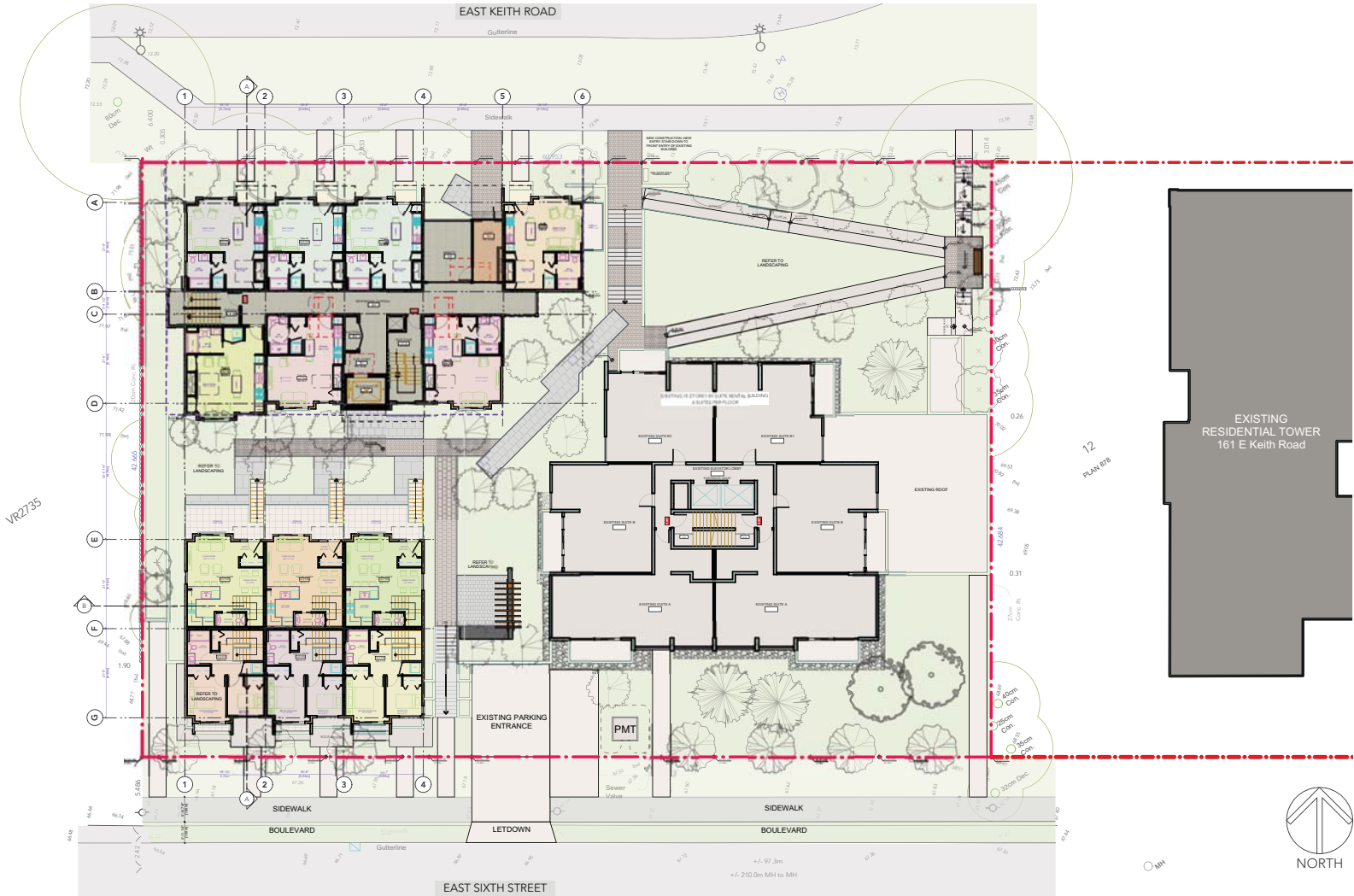
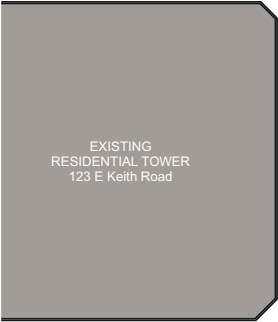
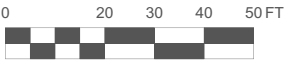
VR2735



12  
PLAN 878

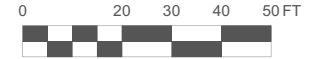


# LEVEL 3 PLAN (Upper Ground)

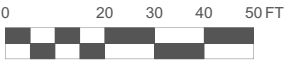




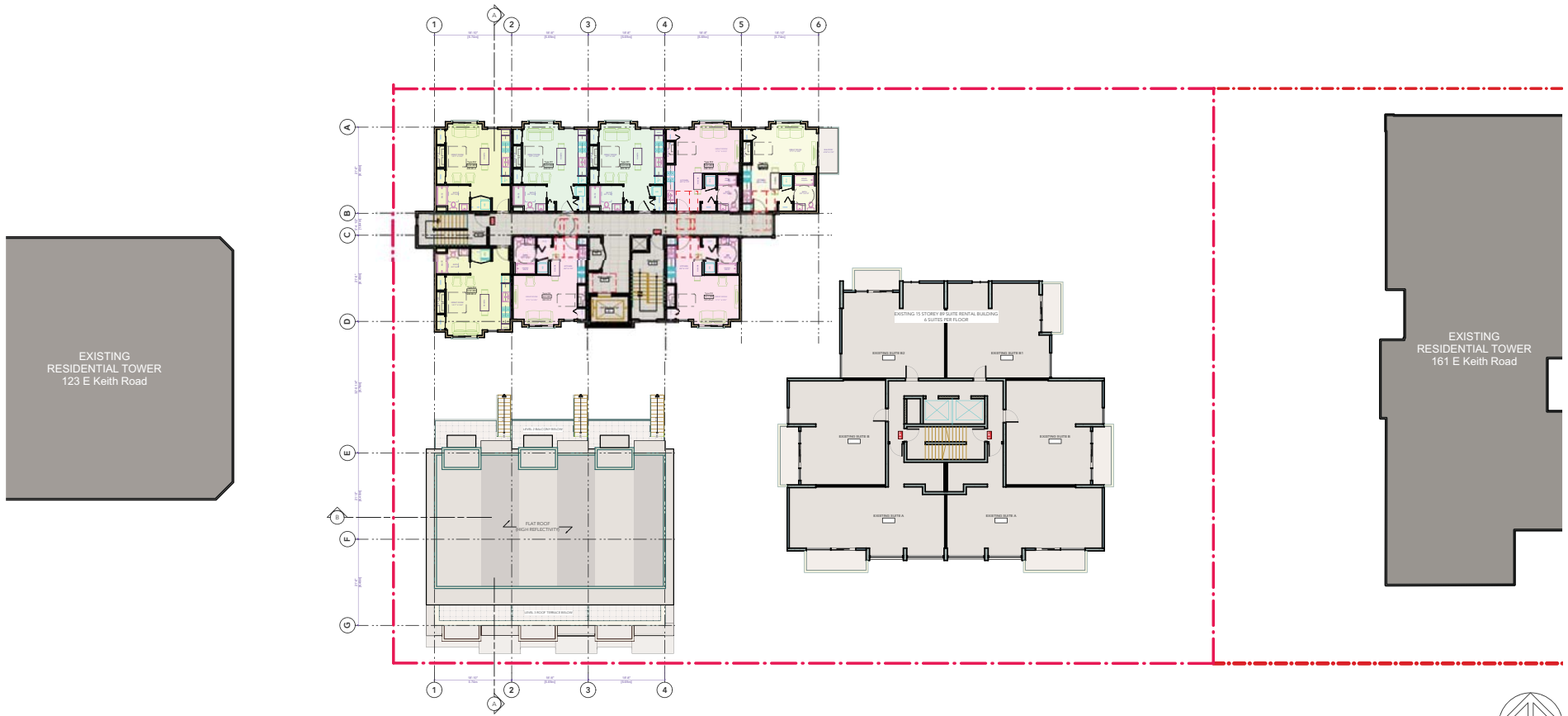
# LEVEL 4 PLAN



LEVEL 5 PLAN

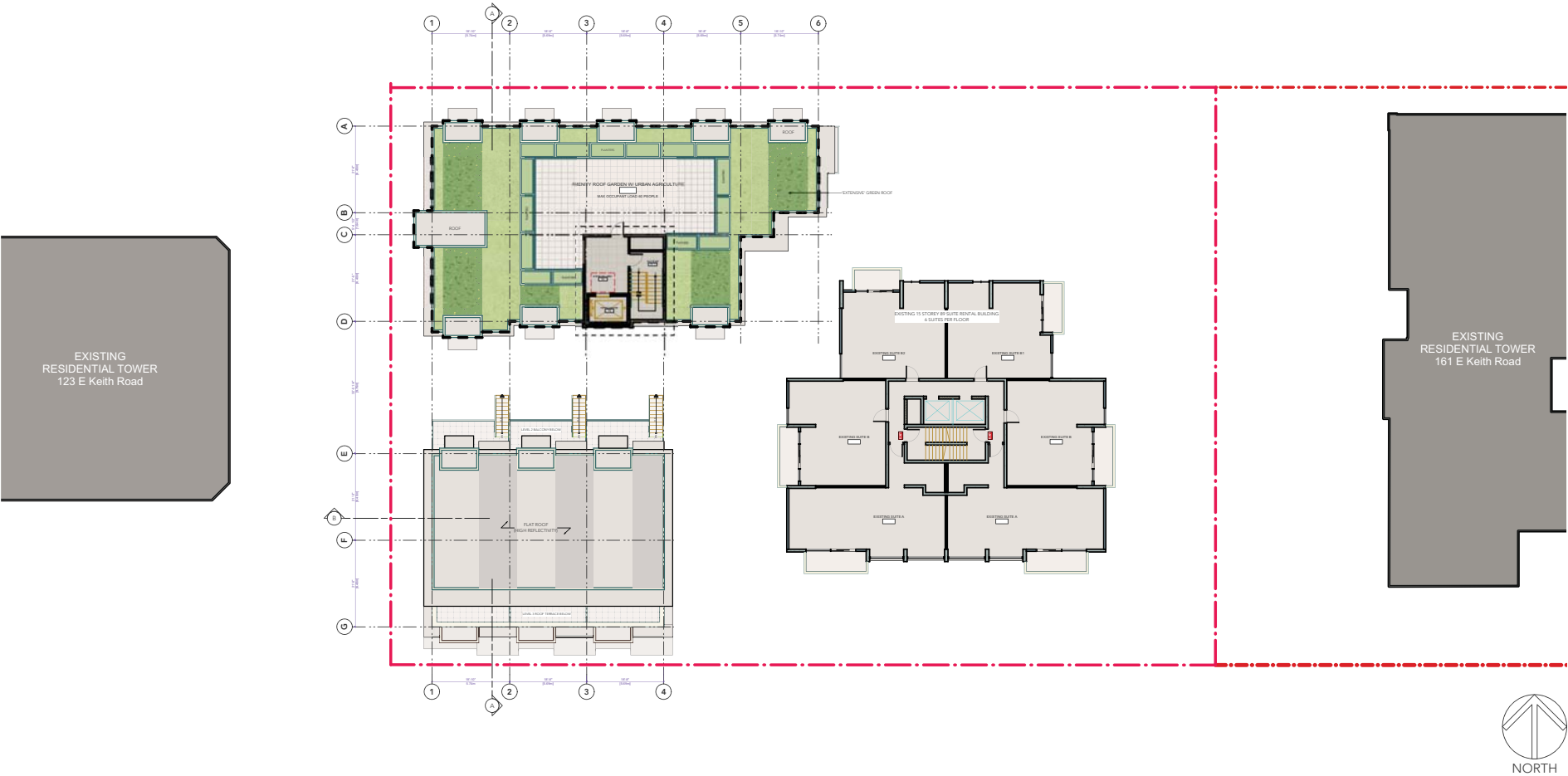
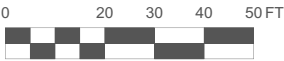


# LEVEL 6 PLAN





# LEVEL 7 (Roof) PLAN

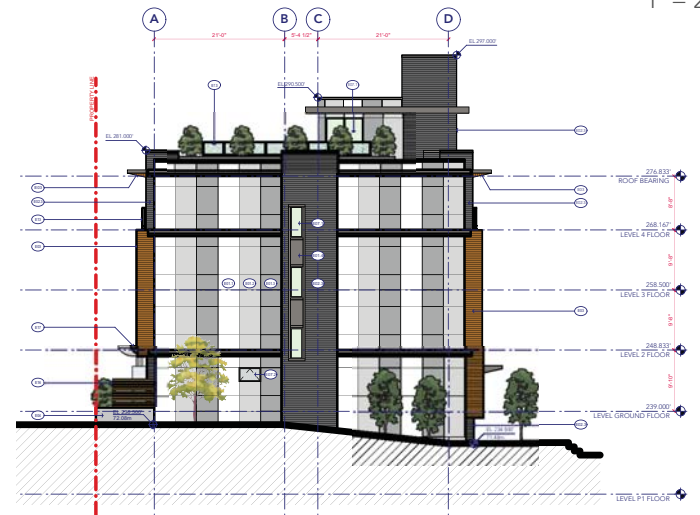


# ELEVATIONS - BUILDING 1

1" = 20'-0"



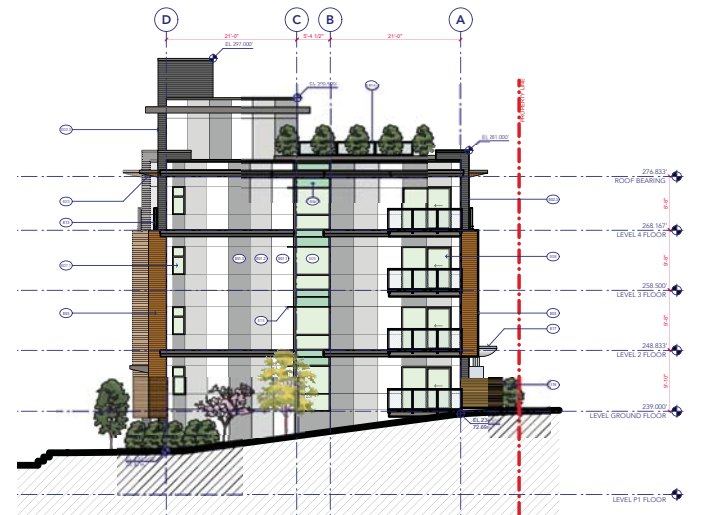
BUILDING 1 - SOUTH ELEVATION (COURTYARD)



BUILDING 1 - WEST ELEVATION



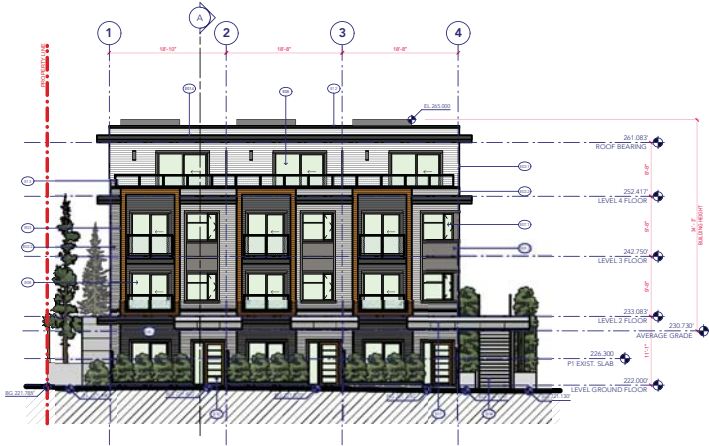
BUILDING 1 - NORTH ELEVATION (E. KEITH ROAD)



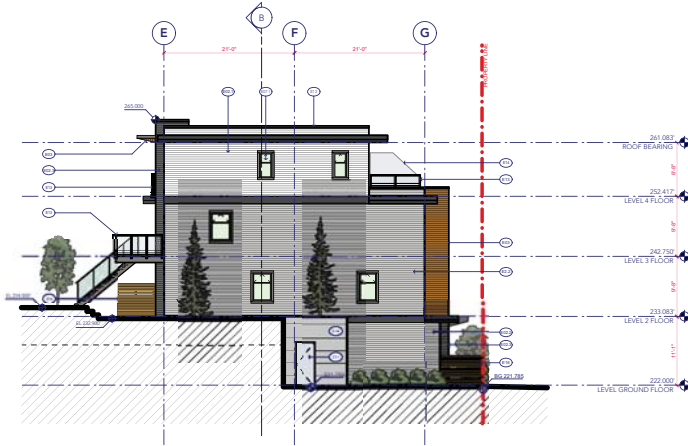
BUILDING 1 - EAST ELEVATION

# ELEVATIONS - BUILDING 2

1" = 20'-0"



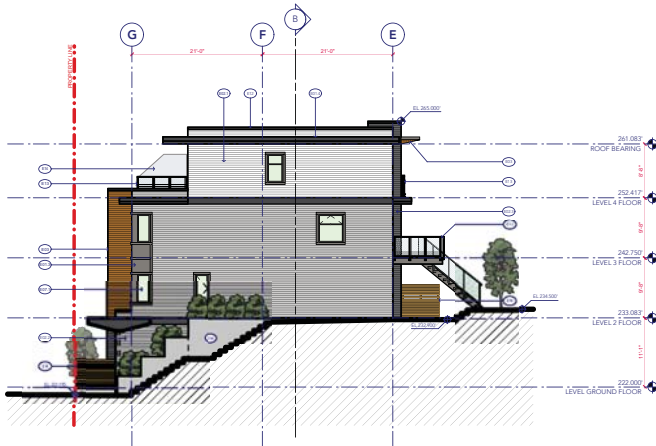
BUILDING 2 - SOUTH ELEVATION (EAST 6TH STREET)



BUILDING 2 - WEST ELEVATION



BUILDING 2 - NORTH ELEVATION (COURTYARD)

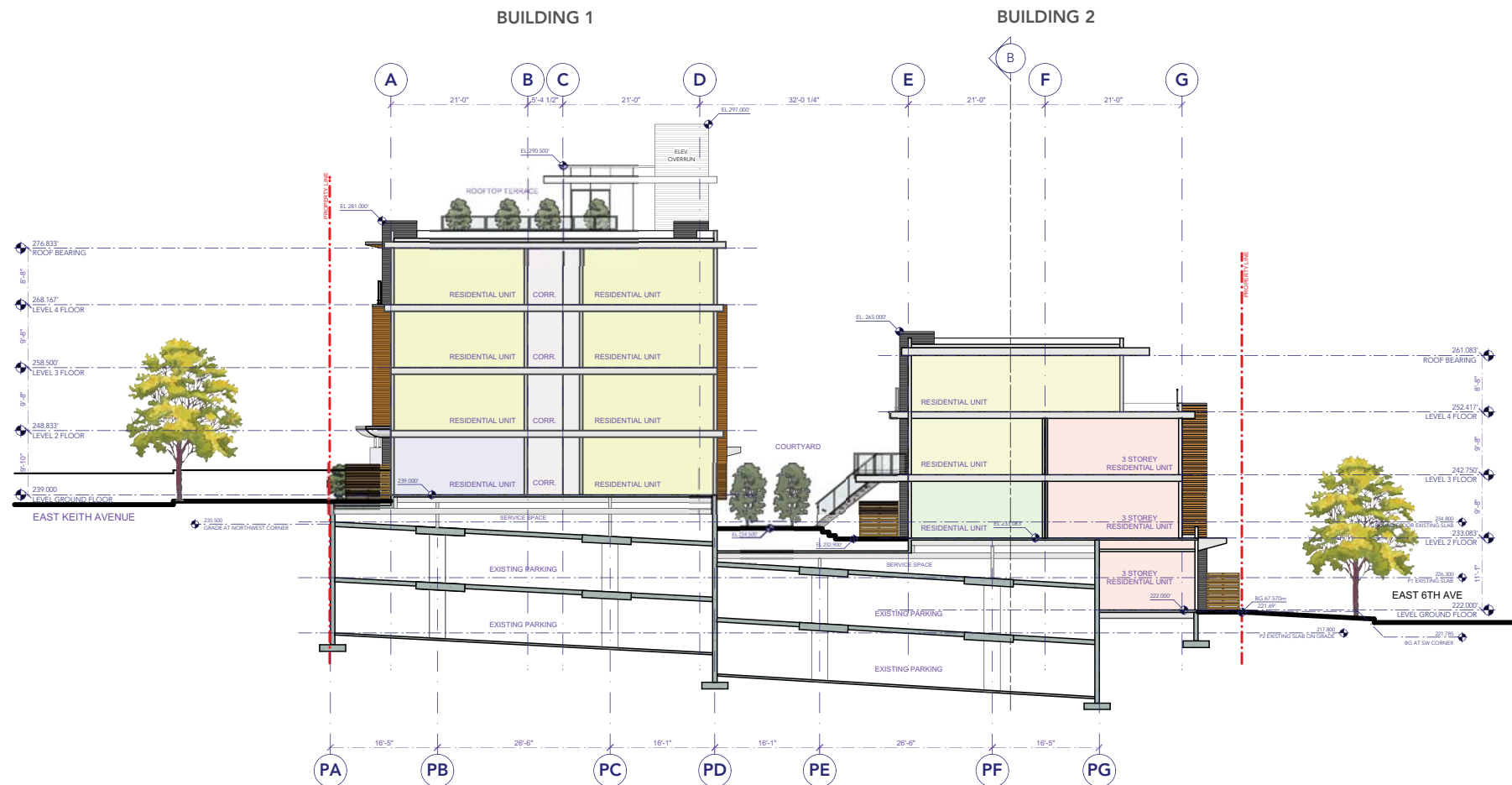


BUILDING 2 - EAST ELEVATION



# CROSS SECTION A-A

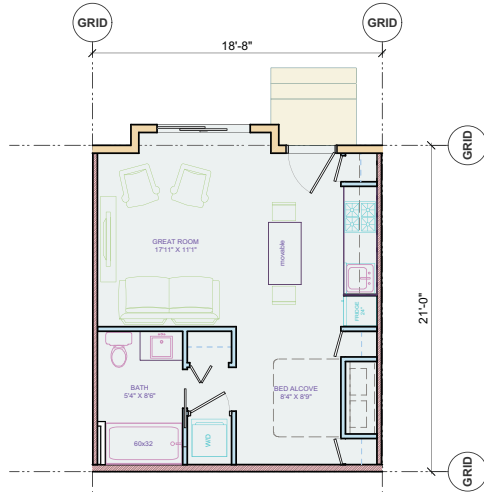
1/16" = 1'-0"



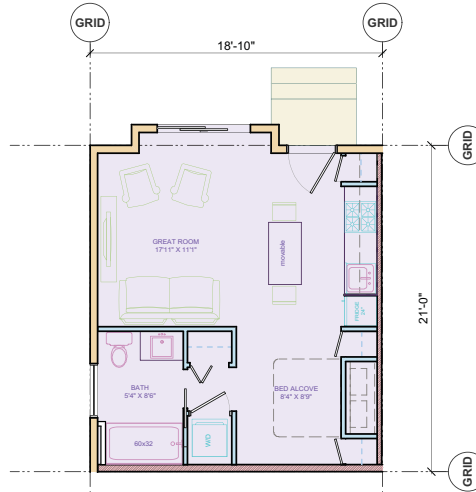
$$1/16'' = 1'-0''$$


# UNIT PLANS - APARTMENT & TOWNHOUSE STUDIOS

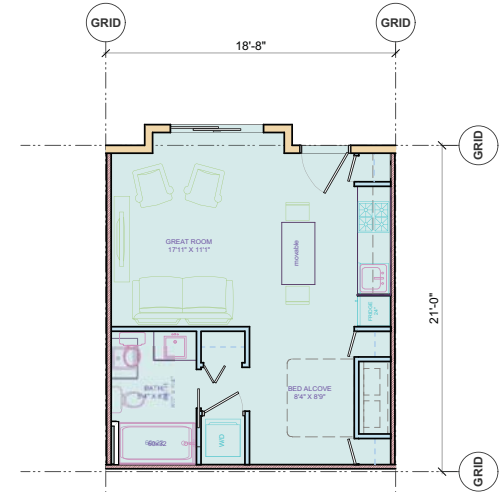
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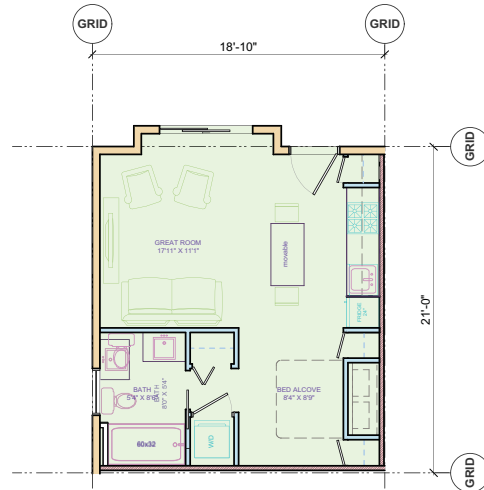
UNIT TYPE A1 - APARTMENT STUDIO (INTERNAL)



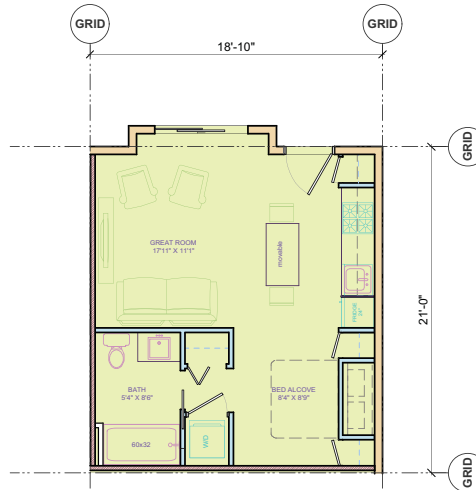
UNIT TYPE A2 - APARTMENT STUDIO (END)



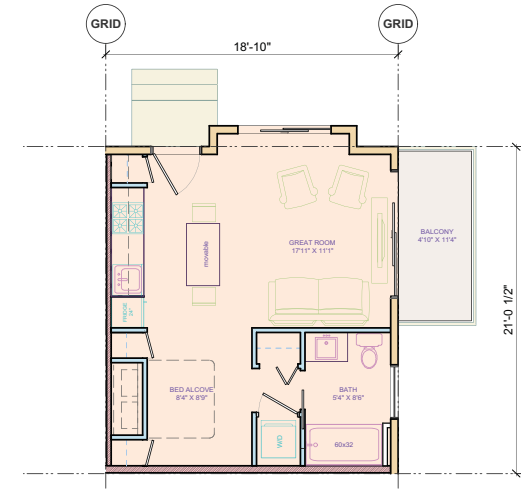
UNIT TYPE A3 - TOWNHOUSE STUDIO (END)



UNIT TYPE A4 - TOWNHOUSE STUDIO (END)



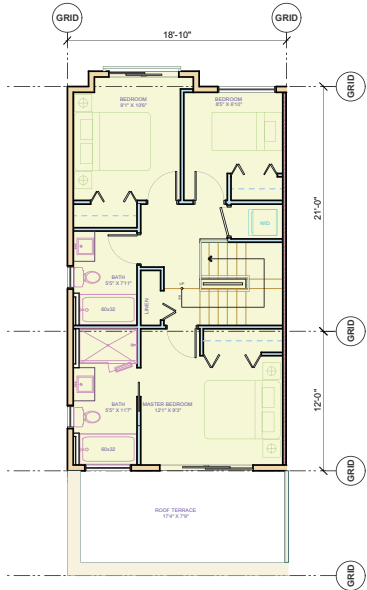
UNIT TYPE A5 - TOWNHOUSE STUDIO (END)



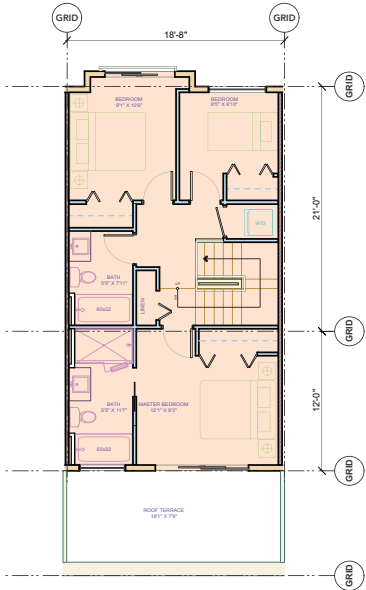
UNIT TYPE A6 - APARTMENT STUDIO (END)

# UNIT PLANS - 3 BEDROOM TOWNHOUSES

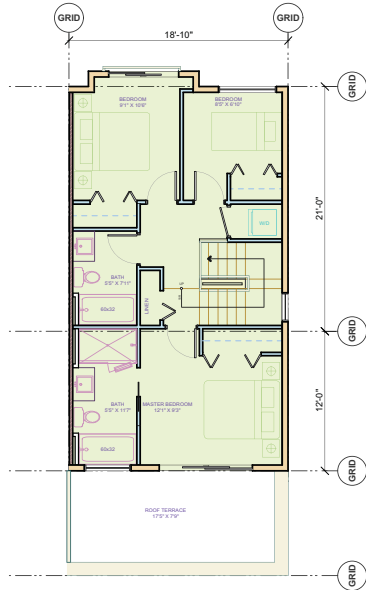
SCALE: 3/32" = 1'-0"



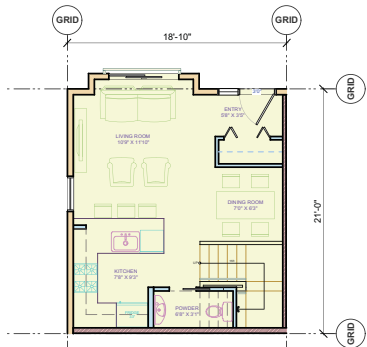
UNIT TYPE **B5** - 2 STOREY TOWNHOUSE (UPPER)



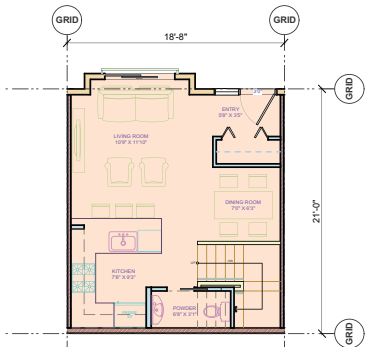
UNIT TYPE **B4** - 2 STOREY TOWNHOUSE (UPPER)



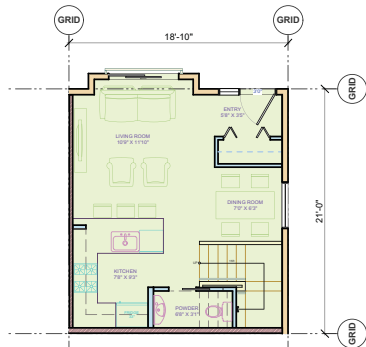
UNIT TYPE **B6** - 2 STOREY TOWNHOUSE (UPPER)



UNIT TYPE **B5** - 2 STOREY TOWNHOUSE (LOWER)



UNIT TYPE **B4** - 2 STOREY TOWNHOUSE (LOWER)

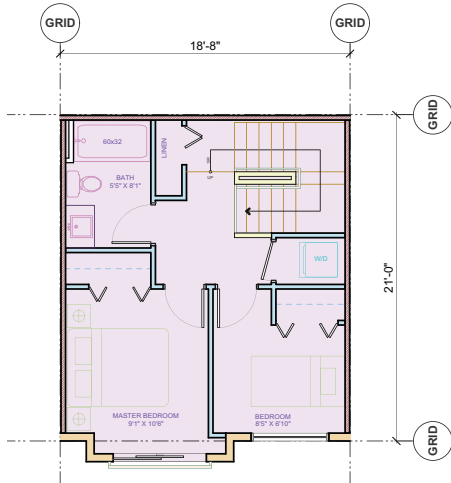


UNIT TYPE **B6** - 2 STOREY TOWNHOUSE (LOWER)

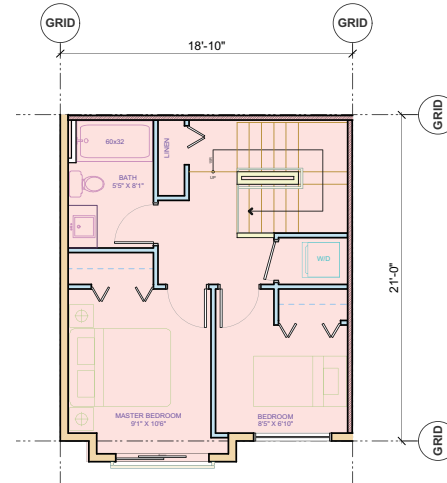


# UNIT PLANS - 3 BEDROOM TOWNHOUSES

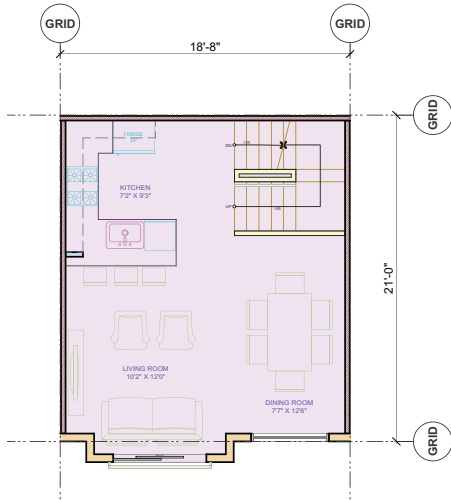
SCALE: 1/8" = 1'-0"



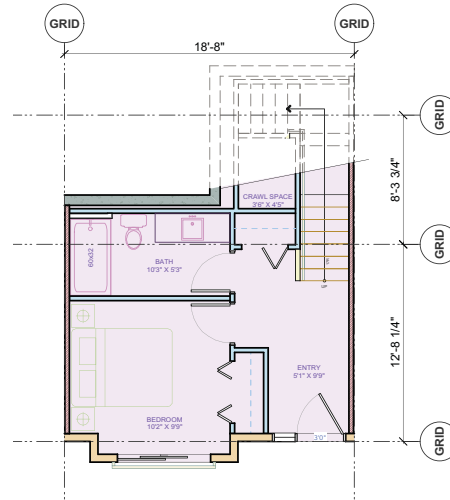
UNIT TYPE **C1** - 3 STOREY TOWNHOUSE (UPPER)



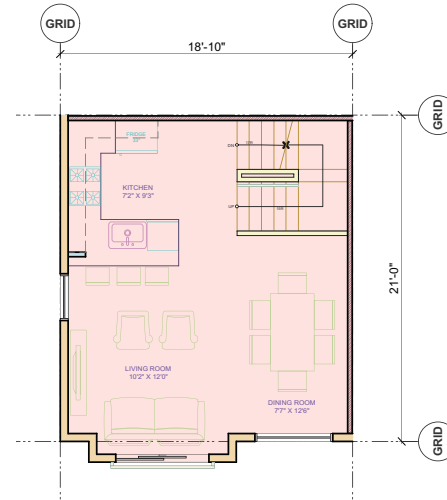
UNIT TYPE **C2** - 3 STOREY TOWNHOUSE (UPPER)



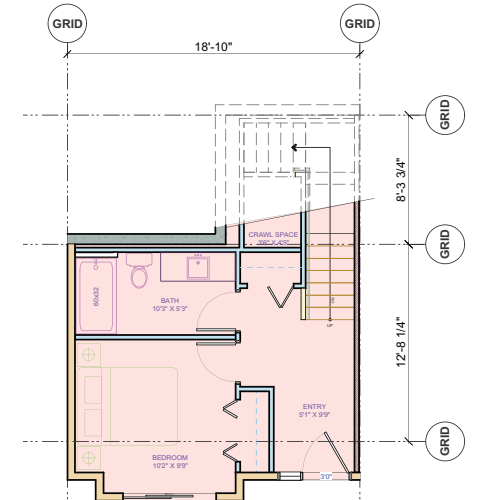
UNIT TYPE **C1** - 3 STOREY TOWNHOUSE (MID)



UNIT TYPE **C1** - 3 STOREY TOWNHOUSE (LOWER)



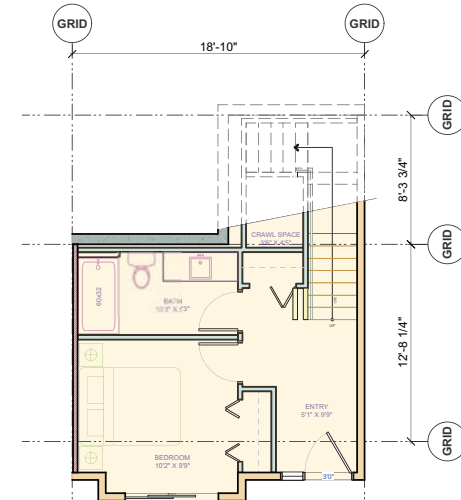
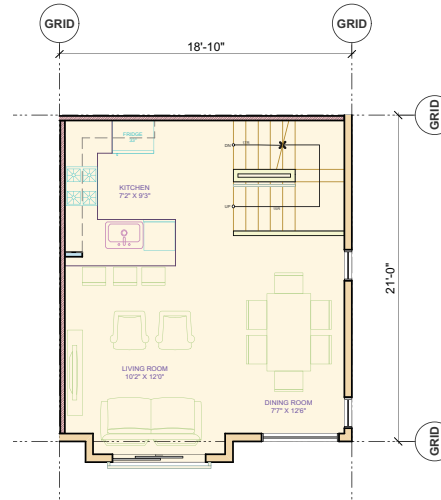
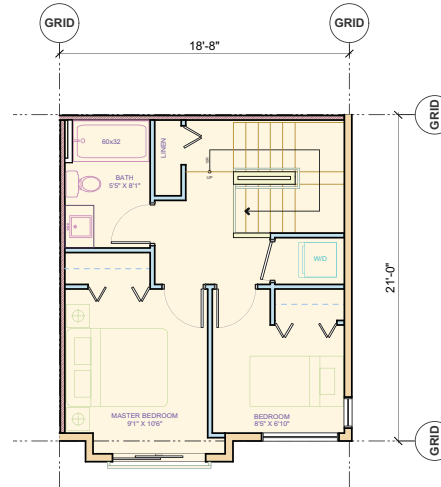
UNIT TYPE **C2** - 3 STOREY TOWNHOUSE (MID)



UNIT TYPE **C2** - 3 STOREY TOWNHOUSE (LOWER)

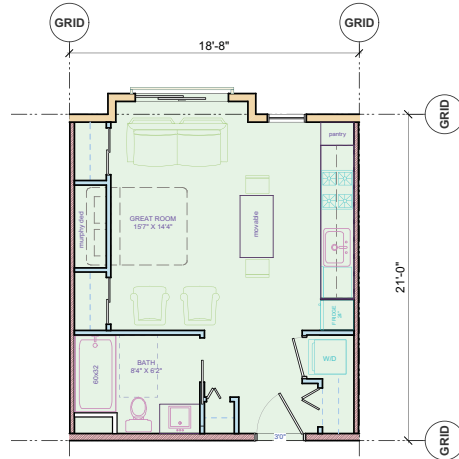
# UNIT PLANS - 3 BEDROOM TOWNHOUSES

SCALE: 1/8" = 1'-0"

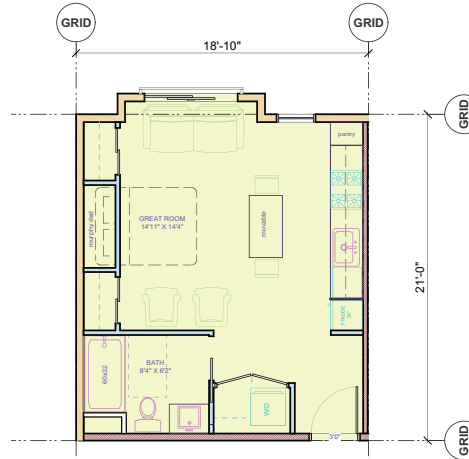


# UNIT PLANS - APARTMENT UNITS

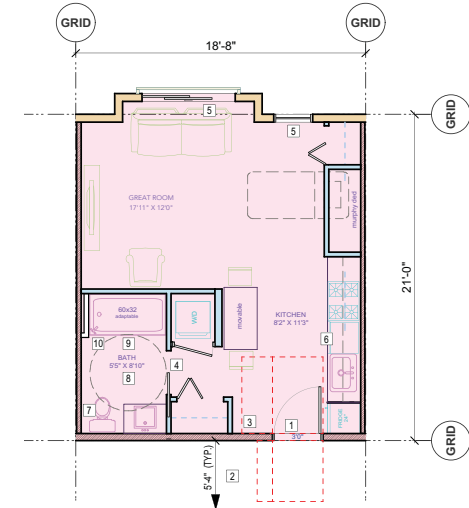
SCALE: 1/8" = 1'-0"



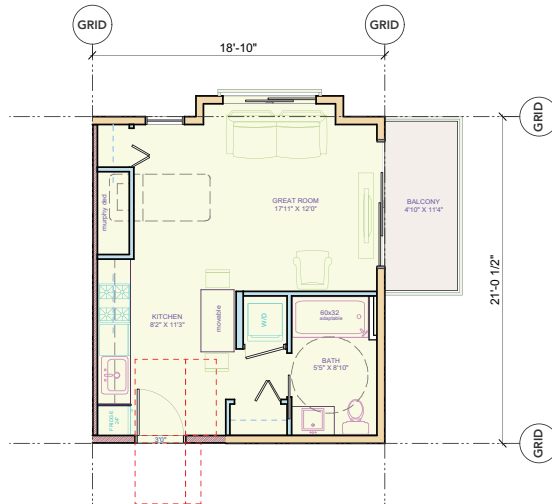
UNIT TYPE **D1** - STUDIO (INTERNAL) APARTMENT UNIT



UNIT TYPE **D2** - STUDIO (END) APARTMENT UNIT



UNIT TYPE **D3** - STUDIO (INTERNAL) (ADAPTABLE L2) APARTMENT UNIT



UNIT TYPE **D4** - STUDIO (END) (ADAPTABLE LEVEL 2)

## LEVEL 2 ADAPTABLE DESIGN ELEMENTS:

- 1 3'-0" Suite entry door c/w wiring for automatic door opener.
- 2 Corridors greater than 4' wide, typical.
- 3 2'-0" clear wall space adjacent to door latches where door swings toward user.
- 4 Bathroom door 2'-10" clear opening.
- 5 Window sills less than 2'-6" A.F.F.
- 6 Continuous counter between sink and stove.
- 7 Toilet located adjacent to wall.
- 8 Turning radius within bathroom provided.
- 9 Greater than 3' clearance along full length of tub.
- 10 Tub control valve placed at outer edge of tub, with tub spout remaining in central position.

3D STUDIES



EAST KEITH ROAD ELEVATION



EAST KEITH ROAD - LOOKING WEST



EAST KEITH ROAD - LOOKING EAST



## 3D STUDIES



EAST 6TH STREET ELEVATION



EAST 6TH STREET - LOOKING WEST



EAST 6TH STREET - LOOKING EAST

## 3D STUDIES



BIRDSEYE - LOOKING NORTHEAST



BIRDSEYE - LOOKING EAST



BIRDSEYE - LOOKING SOUTHEAST



BIRDSEYE - LOOKING SOUTHEAST



# SHADOW STUDIES

SUMMER / WINTER SOLSTICE & AUTUMN EQUINOX



June 21st - 10am



June 21st - 12pm



June 21st - 2pm



September 21st - 10am



September 21st - 12pm



September 21st - 2pm



December 21st - 10am



December 21st - 12pm



December 21st - 2pm



## EXTERIOR MATERIALS / COLOURS



### EXTERIOR MATERIAL/ COLOUR SCHEDULE

MAT NO	ELEMENT	FINISH	COLOUR
E01.1	CEMENTITIOUS PANEL SYSTEM (Hardie 2.0 Reveal)	PAINT	ARCTIC WHITE
E01.2	CEMENTITIOUS PANEL SYSTEM (Hardie 2.0 Reveal)	PAINT	LIGHT MIST
E01.3	CEMENTITIOUS PANEL SYSTEM (Hardie 2.0 Reveal)	PAINT	GRAY SLATE
E01.4	CEMENTITIOUS PANEL SYSTEM (Hardie 2.0 Reveal)	PAINT	IRON GRAY
E02.1	CEMENTITIOUS LAP SIDING (HardiePlank)	PREFINISHED	ARCTIC WHITE
E02.2	CEMENTITIOUS LAP SIDING (HardiePlank)	PREFINISHED	LIGHT MIST
E02.3	CEMENTITIOUS LAP SIDING (HardiePlank)	PREFINISHED	IRON GRAY
E03	WOOD COLOURED SOFFIT / SIDING	PREFINISHED	LIGHT ASH
E04	EXPOSED CONCRETE	PAINTED	CONCRETE - ELASTOMERIC PAINT
E05	ALUMINUM WINDOW WALL SYSTEM	PREFINISHED	CLEAR ANODIZED FINISH
E06	SPANDREL GLAZING	PREFINISHED	TO MATCH ADJACENT WINDOW WALL SYSTEM
E07.1	VINYL WINDOWS (Hardie Trim Casings at Lap Siding)	PREFINISHED	BLACK / CLEAR GLASS
E07.2	VINYL WINDOWS	PREFINISHED	BLACK / TRANSLUCENT GLASS
E08	VINYL SLIDING DOOR (Hardie Trim Casings)	PREFINISHED	IRON MOUNTAIN GREY
E09	VINYL SWING DOOR	PREFINISHED	IRON MOUNTAIN GREY / CLEAR GLASS
E10	WOOD GLAZED DOOR	STAINED	NATURAL STAIN / CLEAR GLASS
E11	HM DOOR	PAINT	TO MATCH ADJACENT MATERIAL
E12	METAL FLASHING	PREFINISHED	TO MATCH BM 2126-20 RACCOON FUR
E13	ALUMINUM GUARD RAILS WITH TEMPERED GLASS	PREFINISHED	GUARDS TO MATCH BM 2126-20 RACCOON FUR, CLEAR GLASS
E14	ALUMINUM PRIVACY SCREEN	PREFINISHED	CLEAR ANODIZED FINISH
E15	ALUMINUM FINS	PREFINISHED	CLEAR ANODIZED FINISH
E16	CEDAR TRELLIS / FENCE PRIVACY SCREENS	STAINED	NATURAL
E17	PREMANUFACTURED ALUMINUM PANEL	PREFINISHED	CLEAR ANODIZED FINISH

#### GENERAL MATERIAL NOTES:

MATERIAL INDICATORS ARE SHOWN FOR CLARITY OF TYPICAL AREAS OF THE ELEVATIONS. AREAS NOT NOTED ARE TO FOLLOW AS PER THE TYPICAL LOCATION CHANGES IN MATERIALS TO ALWAYS OCCUR AT INSIDE CORNERS UNLESS OTHERWISE NOTED OR SHOWN.

	<b>E01.1</b> Cementitious Panel System Hardie Reveal 2.0 Painted - Arctic White		<b>E07   E08   E09</b> Prefinished Vinyl Windows / Sliding Doors / Swing Doors w/ Hardie Trim Casings Iron Mountain Grey
	<b>E01.2</b> Cementitious Panel System Hardie Reveal 2.0 Painted - Light Mist		<b>E10</b> Stained Wood Glazed Door w/ Clear Glass Natural Finish
	<b>E01.3</b> Cementitious Panel System Hardie Reveal 2.0 Painted - Gray Slate		<b>E12   E13</b> Metal Flashing / Aluminum Guard Rails Raccoon Fur To match Benjamin Moore 2126-20
	<b>E01.4</b> Cementitious Panel System Hardie Reveal 2.0 Painted - Iron Gray		<b>E18</b> Aluminum Soffit Longboard Woodgrain Light Cherry
	<b>E02.1</b> Cementitious Lap Siding Prefinished - Arctic White		
	<b>E02.2</b> Cementitious Lap Siding Prefinished - Light Mist		
	<b>E02.3</b> Cementitious Lap Siding Prefinished - Iron Gray		
	<b>E03</b> Stained Wood Siding Natural Finish		
	<b>E05   E15   E17</b> Prefinished Aluminum Storefront System / Alum. Fins / Premanufactured Alum. Panel Clear Anodized Finish		



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OPERATIONS.

# PROPOSED RESIDENTIAL INFILL DEVELOPMENT

## LANDSCAPE SET: RE-ISSUED FOR REZONING

### FEBRUARY 4, 2019

#### PROJECT INFORMATION

**ARCHITECT:**  
BURROWES HUGGINS ARCHITECTS  
CONTACT NAME: MIKE HUGGINS  
205 - 1628 WEST 1ST AVENUE  
VANCOUVER, BRITISH COLUMBIA  
EMAIL: MIKEH@BHA.CA  
PH: 604.730.8100

**LANDSCAPE ARCHITECT:**  
CONNECT LANDSCAPE ARCHITECTURE INC.  
CONTACT NAME: DAVID STOYKO  
2305 HEMLOCK STREET,  
VANCOUVER, BRITISH COLUMBIA, V6H 2V1  
EMAIL: DAVID@CONNECTLA.CA  
PH: 604.681.3303

#### LANDSCAPE DRAWING INDEX

DRAWING INDEX	
SHEET No.	SHEET NAME
L 0.0	COVER SHEET AND DRAWING LIST
L 1.1	LANDSCAPE CONCEPT PLAN
L 1.2	LANDSCAPE CONCEPT LIGHTING PLAN
L 1.3	ROOF LEVEL PLAN
L 2.1	PLANT MATERIALS / LANDSCAPE MATERIALS
L 2.2	LANDSCAPE PLANTING PLAN
L 2.3	ROOF PLANTING PLAN
L 2.4	TREE PROTECTION AND REMOVAL

#### GENERAL NOTES

ALL LANDSCAPE ARCHITECTURAL DRAWINGS IN THIS PACKAGE SHALL BE READ IN CONJUNCTION WITH ALL OTHER LANDSCAPE ARCHITECTURAL DRAWINGS, DETAILS, SPECIFICATIONS, AND OTHER CORRESPONDANCE THAT MAY BE ISSUED DURING THE COURSE OF THE CONTRACT.

IF A DISCREPANCY OCCURS BETWEEN THE DRAWINGS AND THE SPECIFICATIONS OR ANY OTHER DOCUMENT ASSOCIATED WITH THE PROJECT, THE CONFLICT SHALL BE REPORTED IN WRITING TO THE LANDSCAPE ARCHITECT TO OBTAIN CLARIFICATION AND APPROVAL BEFORE PROCEEDING WITH WORKS.

THE CONTRACTOR SHALL VISIT THE SITE TO VERIFY THE TRUE EXISTING CONDITIONS. ANY UNCLEAR ISSUES SHALL BE CLARIFIED WITH THE LANDSCAPE ARCHITECT. NO CLAIM SHALL BE ALLOWED FOR EXTRAS WHICH MAY ARISE THROUGH NEGLIGENCE OF THIS ADVICE.

ALL EXISTING INFORMATION IS BASED ON AVAILABLE RECORDS AND SHALL NOT BE CONSTRUED TO BE COMPLETE OR ACCURATE.

LAYOUT OF HARDSCAPE, SITE FURNITURE, SOIL, PLANTING, AND ALL OTHER MATERIALS IS TO BE STAKED OUT AND APPROVED BY THE LANDSCAPE ARCHITECT PRIOR TO INSTALLATION.

ALL PLANTING SHALL BE IN ACCORDANCE WITH BC LANDSCAPE STANDARD, LATEST EDITION.

THE CONTRACTOR IS RESPONSIBLE FOR DETERMINING THE EXISTENCE, LOCATION, AND ELEVATION OF ALL UTILITIES AND CONCEALED STRUCTURES, AND IS RESPONSIBLE FOR NOTIFYING THE APPROPRIATE COMPANY, DEPARTMENT OR PERSON(S) OF ITS INTENTION TO CARRY OUT ITS OPERATIONS.

CONNECT LANDSCAPE ARCHITECTURE INC. DOES NOT GUARANTEE THE EXISTENCE, LOCATION, AND ELEVATION OF UTILITIES OR CONCEALED STRUCTURES AT THE PROJECT SITE.

FINAL SELECTION AND APPROVAL OF ALL STREET TREES TO BE DONE BY THE CITY OF NORTH VANCOUVER.

6	RE-ISSUED FOR REZONING	19-02-04
5	ISSUED FOR REVIEW	18-12-11
4	RE-ISSUED FOR REZONING	17-10-23
3	RE-ISSUED FOR REZONING	17-01-09
2	ISSUED FOR REZONING	16-11-22
1	ISSUED FOR REVIEW	16-10-20

#### REVISIONS



#### PROPOSED RESIDENTIAL INFILL 151 EAST KEITH ROAD

North Vancouver, BC

Scale:	n/a
Drawn:	KW
Reviewed:	DS
Project No.	06-516

#### COVER PAGE DRAWING INDEX

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#### REVISIONS



## PROPOSED RESIDENTIAL INFILL 151 EAST KEITH ROAD

North Vancouver, BC

Scale: 3/32" = 1'-0" (1:128)

Drawn: KW

Reviewed: DS

Project No. 06-516

## LANDSCAPE CONCEPT PLAN

L1.1



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#### REVISIONS



## PROPOSED RESIDENTIAL INFILL 151 EAST KEITH ROAD

North Vancouver, BC

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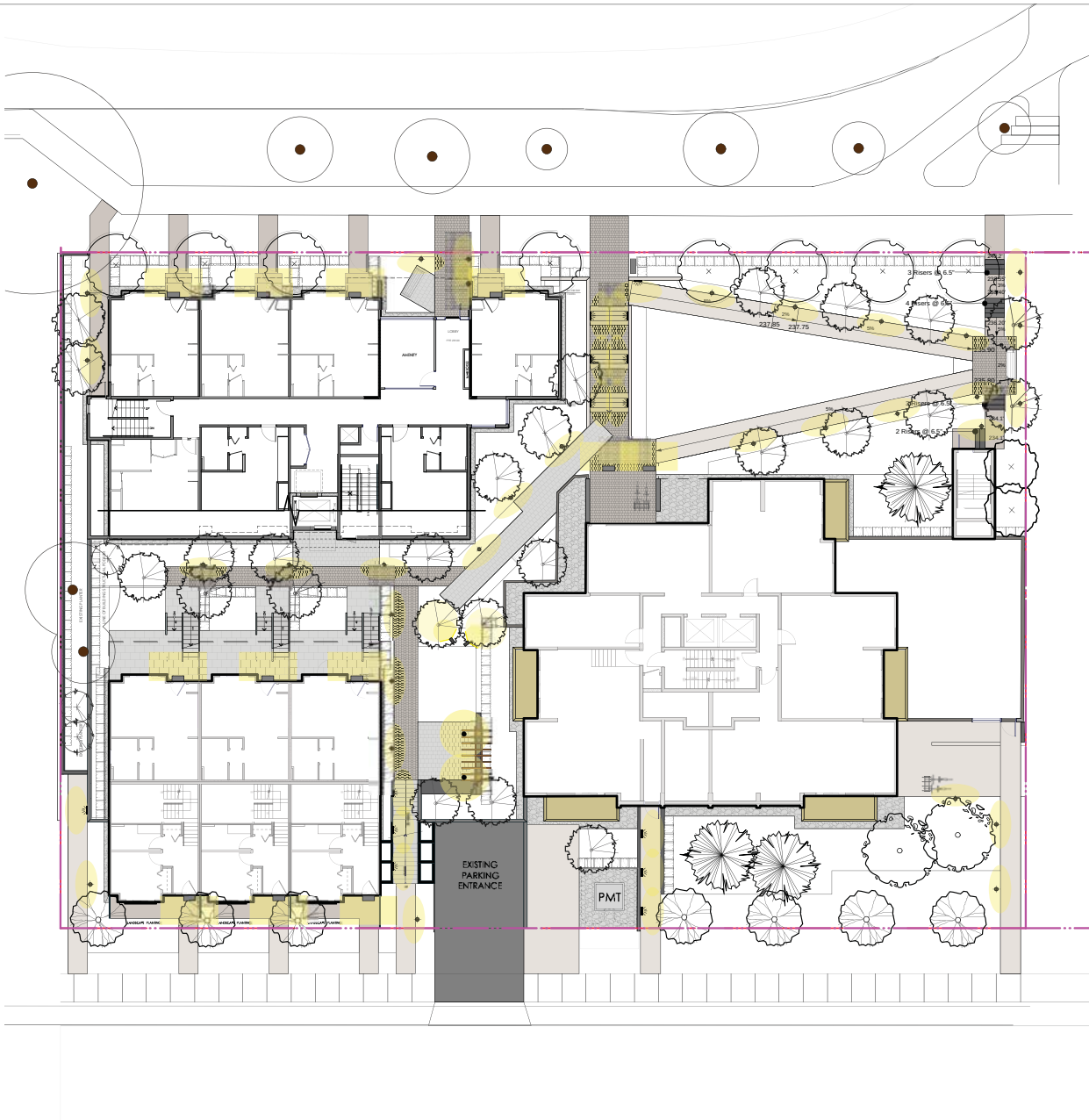
Drawn: KW

Reviewed: DS

Project No. 06-516

## LANDSCAPE CONCEPT LIGHTING PLAN

L1.2



#### SYMBOL



#### LEGEND LIGHTING

##### TYP. ENTRY DOOR LIGHT

REFER TO ARCHITECTURAL AND ELECTRICAL DWGS

##### WALL / STEP LIGHT

PEDESTRIAN IN-WALL LIGHTS FOR ILLUMINATING STAIRS AND PATHS  
SLV LIGHTING PEMA SQUARE  
30W CPL LAMP  
MODEL# 32303030U - BLACK COLOUR



##### BOLLARD LIGHTING

PEDESTRIAN LIGHT BOLLARD FOR PATH LIGHTING  
SLV LIGHTING F-POL BOLLARD  
30W LED LAMP  
MODEL #4231595U - ANTHRACITE COLOUR



##### UP-LIGHTING

AT SPECIMEN TREES AND MONUMENT LOCATIONS  
SLV LIGHTING SITRA 360 SPIKE (GX53 LED)  
5W LED 60 DEGREE LAMP  
MODEL# 5231525U - ANTHRACITE COLOUR



##### DOWN-LIGHTING

AT TRELLIS FEATURE  
SLV LIGHTING SITRA 360 WL (GX53 LED)  
5W LED 60 DEGREE LAMP  
MODEL# 5231515U - ANTHRACITE COLOUR



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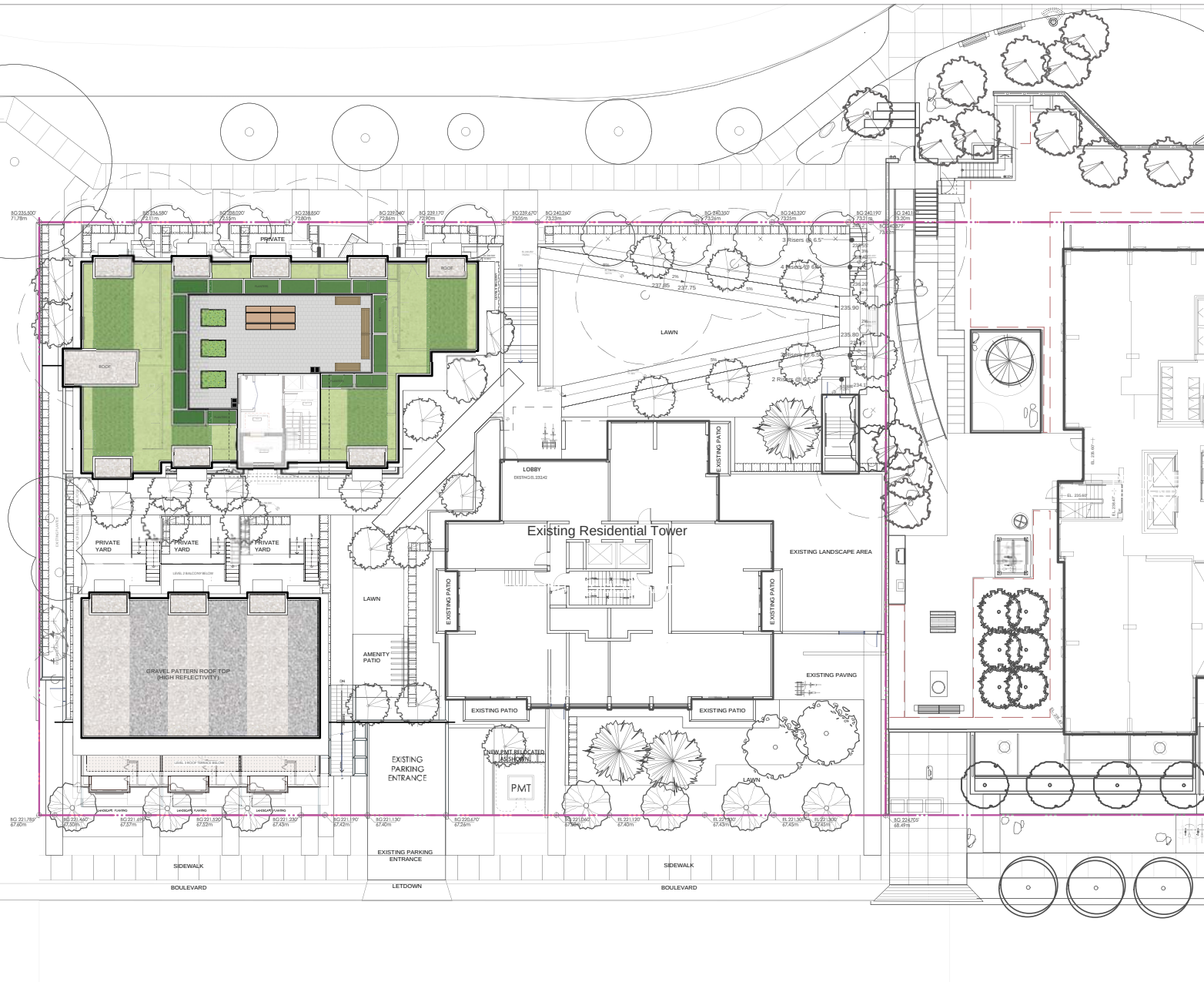
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Drawn: KW

Reviewed: DS

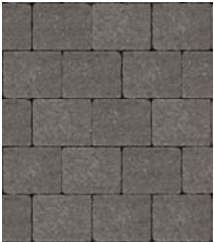
Project No. 06-516

### ROOF LEVEL PLAN





PAVING



FEATURE AREAS



WALKWAYS



PATIOS

PLANTING CHARACTER

EVERGREEN AND SHADE PLANTS

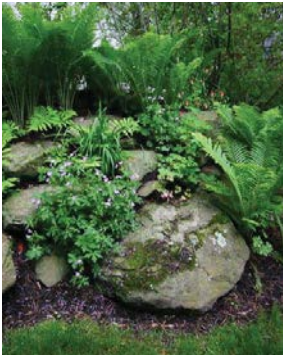


*Fragaria chiloensis*  
Beach Strawberry



*Rubus calycinoides*  
Emerald Carpet

PACIFIC NORTHWEST PLANTS



*Cryptogramma crispa*  
Parsley Fern



*Iris tenax*  
Oregon Iris

PLANT MATERIAL LIST

QTY.	BOTANICAL NAME	COMMON NAME	SIZE	SPACING
EXISTING TREES				
Existing Tree To Remain: install Tree Protection Fencing around dripline perimeter				
PROPOSED CONIFEROUS TREES				
3	<i>Picea omorika</i>	Serbian Spruce	3m HT. B&B	As shown
3	<i>Picea orientalis aurea</i>	Golden Oriental Spruce	3m HT. B&B	As shown
54	<i>Pinus parviflora 'Hillier'</i>	Hillier Japanese White Pine	1.5m HT. B&B	As shown
PROPOSED DECIDUOUS TREES				
17	<i>Acer circinatum</i>	Vine Maple	6cm cal., B&B	As shown
7	<i>Acer griseum</i>	Paperbark Maple	5cm cal., B&B	As shown
2	<i>Acer palmatum 'Osakazuki'</i>	Japanese Maple	5cm cal., B&B	As shown
2	<i>Cercidiphyllum japonicum</i>	Katsura Tree	6cm cal., B&B	As shown
8	<i>Fraxinus penn. 'Prairie Spire'</i>	Prairie Spire Ash	6cm cal., B&B	As shown
4	<i>Liquidambar styraciflua</i>	Slender Silhouette Sweetgum	5cm cal., B&B	As shown
4	<i>Syringa reticulata</i>	Japanese Tree Lilac	5cm cal., B&B	As shown

SHRUBS				
164	<i>Berberis thunbergii 'Rose Glow'</i>	Rose Glow Japanese Barberry	No. 2 Pot	600mm o.c.
60	<i>Cornus stolonifera</i>	Red Twig Dogwood	No. 3 Pot	900mm o.c.
13	<i>Forsythia intermedia</i>	Yellow Forsythia	No. 2 pot	600mm o.c.
122	<i>Lavandula angustifolium 'Hidcote'</i>	Hidcote Blue English Lavender	No. 2 Pot	450mm o.c.
83	<i>Pieris japonica</i>	Japanese Pieris	No. 3 Pot	900mm o.c.
68	<i>Polystichum munifolium</i>	Western Sword Fern	No. 1 pot	450mm o.c.
199	<i>Ilex crenata</i>	Japanese Holly	No. 2 Pot	600mm o.c.
24	<i>Rhododendron 'Ken Janeck'</i>	Pink Rhododendron	No. 3 Pot	450mm o.c.
72	<i>Rhododendron 'Unique'</i>	Pink Rhododendron	No. 3 pot	600mm o.c.
45	<i>Sarcococca hookerana 'Humilis'</i>	Himalayan Sweetbox	No. 2 Pot	600mm o.c.
105	<i>Skimmia japonica</i>	Japanese Skimmia	No. 2 Pot	600mm o.c.
82	<i>Spiraea x bumalda 'Goldflame'</i>	Goldflame Spirea	No. 2 Pot	600mm o.c.
73	<i>Symphoricarpos alba</i>	Snowberry	No. 3 pot	600mm o.c.
251	<i>Taxus x media 'Hilli'</i>	Hicks Yew (male form)	No. 5 Pot	450mm o.c.

GROUNDCOVERS				
148	<i>Ceanothus griseus horizontalis</i>	Creeping Ceanothus	No. 1 pot	380mm o.c.
127	<i>Fragaria chiloensis</i>	Beach Strawberry	No. 1 Pot	380mm o.c.
216	<i>Geranium cantabrigiense</i>	Cranesbill Geranium	4" (10cm) Pot	450mm o.c.
9	<i>Pachysandra terminalis</i>	Japanese Spurge	4" (10cm) Pot	380mm o.c.
66	<i>Rubus calycinoides</i>	Emerald Carpet	4" (10cm) Pot	380mm o.c.
37	<i>Thymus praecox 'Purple Carpet'</i>	Purple Carpet Creeping Thyme	4" (10cm) Pot	300mm o.c.

PERENNIALS				
22	<i>Dicentra formosa</i>	Western Bleeding Heart	No. 1 Pot	450mm o.c.
34	<i>Echinacea purpurea</i>	Purple Coneflower	No. 1 Pot	450mm o.c.
18	<i>Hosta 'Blue Angel'</i>	Blue Angel Hosta	No. 1 Pot	600mm o.c.
42	<i>Iris tenax</i>	Tough-Leaf Iris	4" (10cm) Pot	380mm o.c.

GREEN ROOF MIXES				
60m2	Species TBD	Dark Mix	Tray system	As shown
110m2	Species TBD	Light Mix	Tray system	As shown

6	RE-ISSUED FOR REZONING	19-02-04
5	ISSUED FOR REVIEW	18-12-11
4	RE-ISSUED FOR REZONING	17-10-23
3	RE-ISSUED FOR REZONING	17-01-09
2	ISSUED FOR REZONING	16-11-22
1	ISSUED FOR REVIEW	16-10-20

REVISIONS

PROPOSED  
RESIDENTIAL INFILL  
151 EAST KEITH ROAD

North Vancouver, BC

Scale:	n/a
Drawn:	KW
Reviewed:	DS
Project No.	06-516

PLANT MATERIALS  
LANDSCAPE MATERIALS

CONNECT LANDSCAPE ARCHITECTURE INC.  
DOES NOT GUARANTEE THE EXISTENCE,  
LOCATION, AND ELEVATION OF UTILITIES AND /  
OR CONCEALED STRUCTURES AT THE PROJECT  
SITE.

THE CONTRACTOR IS RESPONSIBLE FOR  
DETERMINING THE EXISTENCE, LOCATION, AND  
ELEVATION OF ALL UTILITIES AND / OR  
CONCEALED STRUCTURES, AND IS  
RESPONSIBLE FOR NOTIFYING THE  
APPROPRIATE COMPANY, DEPARTMENT OR  
PERSON(S) OF ITS INTENTION TO CARRY OUT ITS  
OPERATIONS.



6	RE-ISSUED FOR REZONING	19-02-04
5	ISSUED FOR REVIEW	18-12-11
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3	RE-ISSUED FOR REZONING	17-01-09
2	ISSUED FOR REZONING	16-11-22
1	ISSUED FOR REVIEW	16-10-20

#### REVISIONS



## PROPOSED RESIDENTIAL INFILL 151 EAST KEITH ROAD

North Vancouver, BC

Scale: 3/32" = 1'-0" (1:128)

Drawn: KW

Reviewed: DS

Project No. 06-516

## LANDSCAPE PLANTING PLAN

NOTE: SEE SHEET L2.1 FOR FULL PLANT LIST

CONNECT LANDSCAPE ARCHITECTURE INC.  
DOES NOT GUARANTEE THE EXISTENCE,  
LOCATION, AND ELEVATION OF UTILITIES AND /  
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3	RE-ISSUED FOR REZONING	17-01-09
2	ISSUED FOR REVIEW	16-11-22
1	ISSUED FOR REVIEW	16-10-20

#### REVISIONS



## PROPOSED RESIDENTIAL INFILL 151 EAST KEITH ROAD

North Vancouver, BC

Scale: 3/32" = 1'-0" (1:128)

Drawn: KW

Reviewed: DS

Project No. 06-516

## ROOF LEVEL PLANTING PLAN

NOTE: SEE SHEET L2.1 FOR FULL PLANT LIST

CONNECT LANDSCAPE ARCHITECTURE INC. DOES NOT GUARANTEE THE EXISTENCE, LOCATION, AND ELEVATION OF UTILITIES AND / OR CONCEALED STRUCTURES AT THE PROJECT SITE.

THE CONTRACTOR IS RESPONSIBLE FOR DETERMINING THE EXISTENCE, LOCATION, AND ELEVATION OF ALL UTILITIES AND / OR CONCEALED STRUCTURES, AND IS RESPONSIBLE FOR NOTIFYING THE APPROPRIATE COMPANY, DEPARTMENT OR PERSON(S) OF ITS INTENTION TO CARRY OUT ITS OPERATIONS.



6	RE-ISSUED FOR REZONING	19-02-04
5	ISSUED FOR REVIEW	18-12-11
4	RE-ISSUED FOR REZONING	17-10-23
3	RE-ISSUED FOR REZONING	17-01-09
2	ISSUED FOR REZONING	16-11-22
1	ISSUED FOR REVIEW	16-10-20

#### REVISIONS



## PROPOSED RESIDENTIAL INFILL 151 EAST KEITH ROAD

North Vancouver, BC

Scale: 3/32" = 1'-0" (1:128)

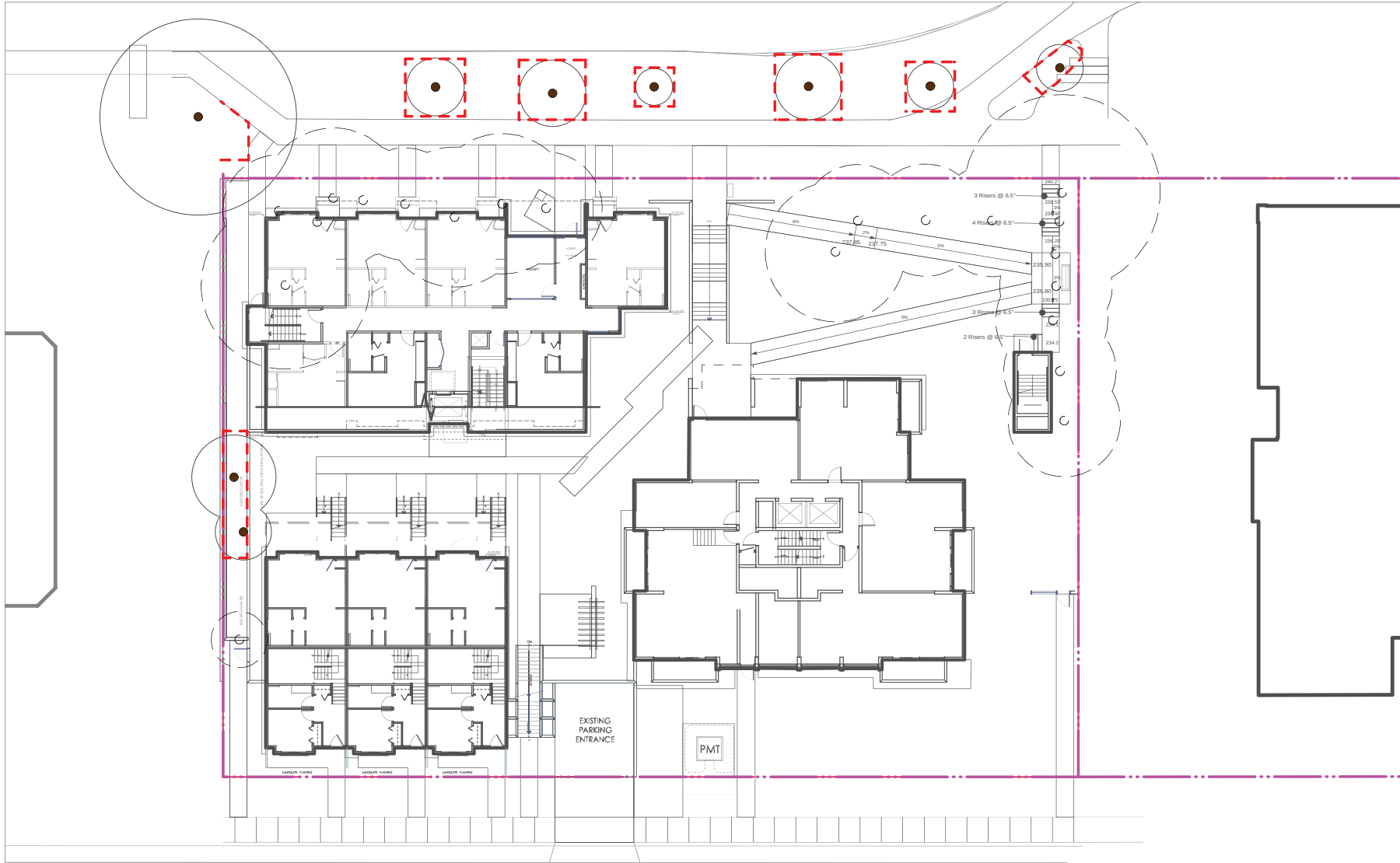
Drawn: KW

Reviewed: DS

Project No. 06-516

## TREE PROTECTION AND REMOVAL

L2.4



#### TREE REPLACEMENT STATISTICS DESCRIPTION

EXISTING TREES TO BE RETAINED ON-SITE	2
EXISTING TREES TO BE RETAINED OFF-SITE (BOULEVARDS AND ADJACENT EDGES)	7
EXISTING TREES TO BE REMOVED	21
NEW REPLACEMENT TREES (APPROXIMATELY 2.33:1 REPLACEMENT)	49

#### TREE RETENTION LEGEND DESCRIPTION

EXISTING TREE TO BE RETAINED
EXISTING TREE TO BE REMOVED

TREE PROTECTION FENCING  
TREE PROTECTION FENCING TO BE INSTALLED PER CITY OF NORTH VANCOUVER STANDARDS AND INSPECTED BY LANDSCAPE ARCHITECT PRIOR TO CONSTRUCTION ACTIVITY

#### SYMBOL



#### TREE RETENTION NOTES

1. REFER ALSO TO SURVEY DRAWINGS. ANY EXISTING TREES NOT IDENTIFIED ON DRAWINGS ARE TO BE PROTECTED UNTIL CONSULTANTS CAN REVIEW AND DETERMINE THE CIRCUMSTANCES.
2. WHERE REQUIRED INSTALL TREE PROTECTION BARRIER AROUND ALL TREES TO BE PRESERVED TO CITY OF NORTH VANCOUVER STANDARDS AND SPECIFICATIONS. INFORM LANDSCAPE ARCHITECT WHEN ALL TREE BARRIERS HAVE BEEN INSTALLED. ARBORIST TO PROVIDE WRITTEN INSPECTION AND APPROVAL OF ALL BARRIERS PRIOR TO CONSTRUCTION ACTIVITY.
3. DO NOT REMOVE OR RELOCATE ANY TREE. ALL RETAINED TREES ARE TO BE PRUNED AND PROTECTED BY CITY OF NORTH VANCOUVER TREE PROTECTION BY-LAWS. CONTRACTOR TO CONTACT CITY ARBORIST AND LANDSCAPE ARCHITECT 48 HOURS PRIOR TO ANY CONSTRUCTION WORK AFFECTING THE DRIP LINE OF TREES.
4. DO NOT ALTER EXISTING GRADE OR STORE MATERIALS UNDER THE DRIP LINE OR WITHIN TREE PROTECTION ZONE. EXCAVATION WITHIN DRIP LINES OF TREES IS NOT PERMISSABLE. TUNNEL UNDER AND AROUND ALL SIGNIFICANT ROOTS BY HAND DIGGING OR AIR SPADE. DO NOT CUT MAIN LATERAL ROOTS. FOR CARE AND PROTECTION OF ROOT GUARANTEE SYSTEM, SEE IRC SPECIFICATIONS. TRENCHING FOR UTILITY CONNECTIONS TO BE COORDINATED WITH ENGINEERING DEPARTMENT TO ENSURE SAFE ROOT ZONES OF RETAINED TREES.





## Community Open House Summary Report

151 East Keith Road  
Starlight Investments

March 27, 2019  
St. Andrew's Church  
1044 St Georges Avenue  
North Vancouver, B.C.

Final Version Date: April 11, 2019

## Summary

Starlight Investments, in collaboration with BHA Architecture, is proposing to increase the rental housing capacity at 151 East Keith Road in North Vancouver through modest, infill development. The proposal intends to respond to the well documented need for more rental housing in the City of North Vancouver.

The site currently features a 14-storey purpose-built rental tower (built in 1972) and a 2-storey underground parking garage. The proposal is strictly for infill housing, meaning the existing building on site will remain in place and that no existing tenants will be displaced as a result of this proposal.

The application seeks to rezone the property to allow for the development of 40 new rental homes, which will be housed in two 4-storey buildings (townhome building and apartment building), for an additional floor space ratio of 0.88. In alignment with the *Housing Action Plan (2016)*, 10% of the homes will be offered at below-market rates. These units will be rented at SAFER rates (30% below CMHC average) and secured in perpetuity, exceeding City requirements.

The site is designated as Level Six: High Density. The *Official Community Plan (2014)* notes that “Council may approve additional floor area if there is a commitment to provide market or non-market rental housing”. (*OCP 2014, Section 2.2*).

As part of the planning process, a voluntary, applicant-led Neighbourhood Open House was held on March 27, 2019 at St. Andrews Church to share the proposal with the community. The purpose of the Neighbourhood Open House was to share the revised scheme with the community and gather the public’s feedback. The notification and Neighbourhood Open House format adhered to the City of North Vancouver’s policy guidelines and requirements.

A turnout of 50 members of the community garnered a total of 26 comment sheets during the duration of the Neighbourhood Open House. The applicant team received 2 comments from local residents following the Neighbourhood Open House.

## Notification

An official letter of notification (Appendix A) was mailed to property owners within a 40 meter radius of the subject site. The notification radius provided by the City of North Vancouver included 2059 homes. The notification was received two weeks in advance of the Neighbourhood Open House, on March 13, 2019. In addition to the official notification, two signs were erected on the subject site (fronting East Keith Road and East 6<sup>th</sup> Street) on March 15<sup>th</sup>, 2019 and the Neighbourhood Open House was advertised in the North Shore News on March 20<sup>th</sup> and 22<sup>nd</sup>, 2019.

## Neighbourhood Open House Summary

Date: March 27<sup>th</sup>, 2019

Location: The Friendship Room - St. Andrews Church, 1044 St. Georges Avenue, North Vancouver, B.C.

Hours: 5:00pm-7:00pm

Registered Attendees: 50

Comment sheets submitted at Open House: 26

Comments received after the Open House: 2

Total comments received: 28

The Neighbourhood Open House followed an informal format with 14 project boards on display (Appendix B). Members of the applicant team were present to answer questions. No formal presentation was made.

Comment sheets were available for participants to privately record their feedback on the proposal. All comments received at the Open House have been transcribed exactly as they appear (no corrections of spelling or grammar have been made) and are included in this report. Scanned copies of the comment sheets and email feedback (Appendix C) and the sign-in sheets (Appendix D) are attached herein.

#### Neighbourhood Open House Sign-In Sheet:

Name	Mailing Address	Email	Telephone
W Daal	123 E Keith		
G Daal	123 E Keith		
P Montgomery	123 E Keith		
D. McDowall	123 E Keith		
R. Romano	138 Esplanade E		
Bob Bunting	655 E 9 <sup>th</sup>		
Cathy Lewis	655 E 9 <sup>th</sup>		
Don Petter	678 W Queen		
Linda Jones	123 E Keith		
Bente Nielsen	1468 St. Andrews Av		
Ken Megale	1601-151 Keith Rd		
Linda Heese	1501-140 Keith Rd		
Bill Heese	1501-140 Keith Rd		
Tanya Miller			
Joy Hayden	267 W. Esplanade		
Robert Van Veen	102-151 Keith E Rd		
Gabrielle Lonen	1004-175 Victory Ship Way		
Dean Rath	202-141 E 6 <sup>th</sup> St		
John Kennan	1401-123 E Keith Rd		
Gerry Fitzpatrick	123 E Keith		
David Fawley	1726 Rufus Dr		
H.Thomas	1001-123 E Keith		
T.Lehaullen	902-123 E Keith		
Ken Dueck	309-170 5 <sup>th</sup> St E		
S. Wilson	209-131 E 3 <sup>rd</sup> St		

Smith St N	150 E. Keith		
Schnieder S	140 E Keith		
I Gilbertson	141 E 6 <sup>th</sup> St		
B. Graves	140 E Keith		
Wayne Collett	123 E.Keith		
Emily DeGenova	155 2 <sup>nd</sup> St W		
Hudson Francesco	155 2 <sup>nd</sup> St W		
May Park	123 E.Keith		
B.Clark	123 E.Keith		
Robyn Newton	2104 Grand Blvd		
Kasin Lamers	141 E 6 <sup>th</sup> Ave		
Viire Daniels	802 - 123 E. Keith Rd		
Mitchell Hugh	801-123 E. Keith Rd		
Elonna Hugh	801-123 E. Keith Rd		
Kim Collett	1501 123 E. Keith Rd		
B. Kellington	1001-140 E Keith		
Bill Herman	123 E. Keith Rd		
Anne Herman	123 E. Keith Rd		
Chuck Handy	123 E. Keith Rd		
Helen Patuck	123 E. Keith Rd		
Catriona Remockez	66-728 14 <sup>th</sup> St		

#### Comment Sheet Results and Quantitative Analysis:

Of the 28 comments received, we have determined that:

- **43% (12 respondents) voiced support** for the project.
- **39% (11 respondents) voiced concern** for the project.
- **18% (5 respondents) voiced neutral** comments toward the project, with suggestions.

#### Key Areas of Support:

- **39% (11 respondents)** expressed support for increased rental, affordable and/or family-oriented housing supply in the area.
- **32% (9 respondents)** voiced support for the design (architecture, landscaping and density) of the proposal.
- **25% (7 respondents)** liked that the proposal is for infill development (ie. no displacement associated with this application).



## Key Areas of Concern:

- **39% (11 respondents)** indicated that they wanted to see the setbacks increased and/or had concerns about view corridors.
- **18% (5 respondents)** raised concerns about increased street-side parking and/or traffic in the area.
- **18% (5 respondents)** raised concerns about construction impacts (noise, debris, etc).
- **14% (4 respondents)** did not want to see housing developed at this location.

Of the 26 comment cards received at the Neighbourhood Open House, 7 comment cards were submitted anonymously (no name or contact information). Below provides an overview of the comments received. Please note that comments are transcribed verbatim and have not been corrected, altered, or censored.

### 1. Tell us a little about yourself. Check those applicable to you?

Transcription of Comments:

- **93% (25 respondents)** live in the City of North Vancouver
- **19% (5 respondents)** work in the City of North Vancouver
- **56% (15 respondents)** own their home
- **37% (10 respondents)** rent their home
- **7% (2 respondents)** did not provide a response

### 2. Are there elements of the proposed plan that you like? If so, what are they?

Transcription of Comments:

The below responses to this question were submitted to the applicant team during the Neighbourhood Open House:

1.	<i>Larger family units, starter home units (all in one!), Modest infill, fits with character of neighbourhood and rooftop patios</i>
2.	<i>-The design, how it fits in the community -Proximity to green space – great for seniors and small families -Pleased how it fits into the existing neighbourhood -Pleased to see family size units – need some children in the neighbourhood -Love the housing available to low-income seniors</i>
3.	<i>All of them!</i>
4.	<i>I like that the plan is supporting the development of more rental housing much needed, especially in the form of studio and 3-bedroom units, which we are particularly short on. I also don't think this development will be overly disruptive to existing residents or detract from their neighbourhood</i>
5.	<i>-Support for seniors -3-Bedroom rentals are needed in North Van! (Affordable) -Thrilled to see a progressive plan -Better amenities are needed in rental buildings. I am happy to see that there will be upgrades</i>

6.	<ul style="list-style-type: none"> <li>-Increase to rental stock</li> <li>-Seniors below market rental</li> <li>-3-bedroom units for families</li> <li>-Adaptable units</li> <li>-No displacement of existing residents</li> </ul>
7.	<ul style="list-style-type: none"> <li>-Rental stock will increase without displacing current units. This is a true increase in available rentals, not just replacing a building</li> <li>-Affordability. Affordability is a real concern in the City of NV. Rental prices seem to increase every month and there doesn't seem to be an end in sight. I want to live in a vibrant and diverse city, not a city that is only accessible to the highest income residents</li> <li>-I really can't emphasize enough how much I support this project</li> </ul>
8.	<ul style="list-style-type: none"> <li>-All! More affordable housing is needed in this city especially projects that do not displace current tenants</li> <li>-Makes the whole street look better and more modern</li> </ul>
9.	<ul style="list-style-type: none"> <li>-Like the aesthetics</li> <li>-Integrates well with the neighbouring buildings</li> </ul>
10.	<ul style="list-style-type: none"> <li>-Improves access</li> <li>-Revised layout (building 3 removed)</li> <li>-More landscapes and trees (Greenery)</li> </ul>
11.	<ul style="list-style-type: none"> <li>What are you planning to do with the trees on the west side of these buildings?</li> <li>How are ya going to accommodate the people with cars?</li> </ul>
12.	NONE
13.	-Not really – I do not wish this development to proceed
14.	<ul style="list-style-type: none"> <li>I do not like any of the plans</li> <li>Unnecessary devel</li> <li>Construction fatigue – due to 16 storey bld. Next door</li> <li>Obstruction to view corridor</li> <li>Parking problems</li> <li>Noise</li> </ul>
15.	NONE
16.	<ul style="list-style-type: none"> <li>No</li> <li>We have no need for unaffordable rental housing</li> </ul>
17.	<ul style="list-style-type: none"> <li>Like that the proponent is willing to buy their plans to the neighbourhood for early views</li> <li>I also like the fact that the unsightly piece of land will be addressed</li> </ul>
18.	<ul style="list-style-type: none"> <li>Too close to Victoria Park &amp; Victoria Place</li> <li>No sight lines from Park</li> <li>Stairwell lighting impacting wildlife &amp; Area Residents</li> <li>Affordable suites – I don't think so</li> </ul>
19.	<ul style="list-style-type: none"> <li>That it is 4-storey is good-but I still think this development is misplaced</li> <li>That tenants are not displaced-since nobody there!!</li> <li>That it would remain "RENTAL" is perpetuity</li> </ul>
20.	<ul style="list-style-type: none"> <li>I am supportive of an infill project for the site but it must respect neighbourhood values, particularly setbacks</li> </ul>
21.	The walkway is most considerate to all
22.	<ul style="list-style-type: none"> <li>We need more rental housing in NV that is affordable</li> <li>Proposed building &amp; landscaping looks attractive in renderings</li> </ul>

3. Are there elements of the proposed plan that you believe could be improved? If so, do you have suggestions on how they might be improved?

Transcription of Comments:

The below responses to this question were submitted to the applicant team during the Neighbourhood Open House:

1.	<i>Unsure</i>
2.	<i>Building 1 is too close to the property line to the west and to Keith road. The setbacks are insulting to the community need for open, uncluttered visual space especially as people live in smaller + smaller apartments studios Building 2 is too close to the property line to the west. 6<sup>th</sup> street set back is fine it is taking advantage of the "air space" left on the east side of the property to the west (123)</i>
3.	<i>Build sooner</i>
4.	<i>DO NOT BUILD</i>
5.	<i>DO NOT BUILD The Corridor is going to disappear. Most of the grocery &amp; park will be obscured by this building specially for those that live on East 6<sup>th</sup>. We are fed up with construction, trucks, no parking, excessive noise – ground tremors, smokers, foul language. THE TRAFFIC IS OUTRAGEOUS and our patience has reached its limit.</i>
6.	<i>Setback from E Keith Road, to be improved. The building encroaches on the sidewalk. As a pedestrian the building has an overbearing presence. It is totally out of character with the neighbourhood</i>
7.	<i>Yes, atm we the plan we don't want any more skyscrapers in around Victoria Park</i>
8.	<i>Excellent response to comments &amp; concerns raised at the public hearing last year</i>
9.	<i>No view corridors have been maximized already while not over bearing the site</i>
10.	<i>The plan needs to address the west side variance issue. I live at the 123 E Keith and will certainly feel the impact of the solid wall massing in this design. The protruding stairwell structure in that wall makes the impact even worse. When the adjacent buildings has an open airy side yard it is not fair to the neighbourhood to build so close to the property line and expect the adjacent property to provide the ambiance this area projects. Please increase the west side setback and redesign your buildings to reflect that. We are well aware that not having to build new parking allotments will save this developer a lot of money, but the neighbourhood will suffer for it. To allow the project to also destroy our light and openness corridors is not acceptable</i>
11.	<i>Increase setbacks at E Keith</i>
12.	<i>It is not the right place – looks awkward in between the high-rises, and like the previous proposal it blocks the park even now that the one building is removed. I don't think we should put anything there just because there aren't any buildings right now. It seems a bit desperate!</i>
13.	<i>The setback is....</i>
14.	<i>No I think it's great!</i>

15.	<i>Nope, needs to move forward</i>
16.	<i>Always curious about Highrise vs 4 storey integrations but can't think of any changes.</i>
17.	<i>Yes – abandon this project – not necessary- we don't need a wall of corridor facing us</i>
18.	<i>No I just want it to be build</i>
19.	<i>Victoria Park is probably the highest residential 2 block area in the city despite this density. Victoria Park is a very desperate area because of its ambiance. This ambiance is the result of the view corridors this project reduces the west view corridor to a minimal width. The setbacks should be 25 feet which will be consistent with the eastern setback</i>

4. Do you have any other comments you'd like to share with us?

Transcription of Comments:

The below responses to this question were submitted to the applicant team during the Neighbourhood Open House:

1.	<i>Respect the design principles which have made this area beautiful to live despite the density. By eliminating this view corridor, you will set a principle for many other Victoria Park properties</i>
2.	<i>People dislike change, but that shouldn't impede progress. This development is part of a solution to a longer problem. I hope it proceeds</i>
3.	<i>Pleased to see a well-designed smaller building rather than another 17-storey high rise. Also like the changes to the design, greater set back. It 'softens' the concrete jungle surrounding the area yet it fits in. What a fabulous opportunity for small families with a beautiful park across the street</i>
4.	<i>Great company, great building, great management</i>
5.	<i>No-DO NOT BUILD-It is enough. Our green corridor will disappear-we will have constant noise trucks, tremors, dust garbage-no parking smokers-screaming. Residents on E 6<sup>th</sup> street are worn out. We want a 5-year construction break. I have nothing good to say.</i>
6.	<i>This is an inappropriate project in an inappropriate place. The feel of the street will be completely changed with the units sitting in areas not similar to the rest of the buildings surrounding the park on the east &amp; west Keith road. The construct parking problem will be exasperated – the underutilized parking is because tenants don't want to rent spaces out because they don't have cars. The community finds this imposition, and should've listened too</i>
7.	<i>I look forward to seeing this project being built! We need more rental apartments!</i>
8.	<i>Much needed! Thank you!</i>
9.	<i>This development seems to me to be greed! We already have a concrete wall on our street!</i>
10.	<i>We have already been through 2.5 years of construction already on this block. Parking on street us a real problem.</i>
11.	<i>I find it very disturbing that having seen this project defeated by a previous city council, it is now back in an even more disturbing design. "What" makes the proponent think he has a better chance to pass it this time. Do you know something this neighbourhood is not being told??</i>



	<i>I since really believe something need to be built on this site and badly needed “social/ housing should be built here. This is exactly the kind of site where the proponent has not had factor in acquiring land costs or providing any additional parking that they could provide badly needed “social housing” and still make a reasonable profit by building within the OCP allowed limits.</i>
12.	<i>Its so important to recognize progress and growth the city is growing, and people need a place to live. I’ve heard so many people against development because they are against change and that is unacceptable. We have an enviable lifestyle here in the City of NV and that lifestyle is open to everyone. We all need a place to live and a community to call home and I for one, love this city. But the nay side is the real problem here. Nimbyism and thinly veiled classism hurt this community. Let’s be real about the projects that bring out hardcore opponents – its not the luxury towers that seems to bring out the worst. We all need a place to live, so build homes.</i>
13.	<i>“Below Market” – only 4 of 40 units will be “below market” and starlight properties are known for charging high rents, so affordability will be an issue (still). Starlight properties is North Van make a mocking of your first placard “project team”, which says how “excellent” the company is. Example – property at 150 E Keith has been under renovation since last November, and is still not finished. (Masking tape for door numbers, cardboard still or floor in lobby..etc etc) Most residents see the “upgrade” as a downgrade. (ugly gray doors, cement filing) – Not would be okay with that if it could just be finished. Lack of attention shows lack of respect for tenants.</i>
14.	<i>I am very concerned about the construction noise and length of time to build – 1.5 years I am also concerned about the additional vehicle traffic on 6<sup>th</sup> street to the underground parking and extra demand for street parking, which is already in very high demand.</i>
15.	<i>The 4-storey + building needs to be set back further from Keith Rd E. The bylaw regulation states 25 feet setback. Too much of this building is not the setback! This building is too high to be this close to the sidewalk + road -Parking- there are inefficient spaces for parking, the surrounding streets are full day and night! This is unacceptable to force more people to park on the street Too much potential car parking area is planned for bicycle lockers – too much space compound to car parking situation in this area.</i>

## Comments Received Post Open House

The applicant team received 2 comments from local residents following the Neighbourhood Open House. The following correspondence was submitted to the applicant team:

**From:** Jessie Brock  
**Date:** April 10, 2019 at 7:23:53 AM  
**To:** Virginia Bird

**Subject:** Rental Housing on Keith

To whom it may concern:

I recently received a flyer with an invitation to attend an Open House to view the plans for a new rental housing project on East Keith Road, less than a block from where I live (rent). I wasn't able to attend the Open House as I have a very young son but wanted to register my support by email.

Although the project is small in scale, this infill development will add 40 new units of much needed rental housing supply to the City of North Vancouver, and is a much better use of land than what is currently on site today. When we moved back to the North Shore from downtown, my family and I struggled to find appropriate rental accommodation in this neighbourhood given the lack of larger 2 & three bedroom homes.

The location of the proposed development off Lonsdale, opposite Victoria Park, is an ideal location to add additional density due to its proximity to services and amenities, outdoor recreation space, and transit. Thank you for your consideration.

Jessie Brock

**From:** Jody Rechenmacher  
**Sent:** Thursday, April 4, 2019 8:31 PM  
**To:** Kirsten Avison  
**Subject:** Keith Road - Infill Development

Hello,

I was unable to attend the open house last week but I am submitting these formal comments **in support** of the application for infill development on Keith Road.

My husband and I own a townhouse on 4<sup>th</sup> Street West. We moved to the City of North Vancouver in September of 2017 from Vancouver after having our first child. We chose the north shore because of proximity to the mountains, but we chose the City specifically because of the type of development that has been happening over the recent years. We value living in a walkable community with a vibrant streetscape on Lonsdale and the proximity to the seabus. We also value the City's many parks, including Victoria Park.

I support the proposed development for two reasons:

1. I love the mixed use space on Lonsdale and see new businesses starting at other places in the City. However, there is still some vacant commercial development and at times it seems some businesses struggle with having the population base to support their viability. I welcome additional density to certain areas in the City to ensure that we can continue to have access to vibrant streetscapes and a diversity of businesses.
2. I'm aware that the City needs to continue to increase housing supply – both market and affordable housing. I welcome additional density of the right type and in certain areas to meet this need. The proposed development is a perfect example of what I see to be the right type of infill density – leveraging existing buildings without evicting existing residents. Having Victoria Park across the street from the site maintains a desirable openness to the site without compromising the experience of being in the park. Infill development of this type is important because it doesn't sacrifice our City's green spaces. Approving this development helps the City to increase supply of rental stock in a way that protects the character and assets of the City, without evicting current residents.

Sincerely,

Jody Rechenmacher

#### **Appendices**

- **Appendix A:** Neighbourhood Open House Notification
- **Appendix B:** Project Display Boards
- **Appendix C:** Copy of Submitted Comment Cards
- **Appendix D:** Copy of Open House Registration
- **Appendix E:** Site Signage

# PLEASE JOIN US

BHA Architecture, together with Starlight Investments, invite you to a Neighbourhood Open House to view their proposal to increase rental housing capacity at 151 East Keith Road through modest, infill development.

BHA Architecture, together with Starlight Investments, are pleased to present a proposal to increase rental housing capacity at 151 East Keith Road. The proposal aligns with the current allowable density under the City's *Official Community Plan (2014)* and seeks to respond to the well documented need for rental and family oriented housing in the City. In alignment with the City's *Housing Action Plan (2016)*, 10% of the units will be offered at below market rates. The application seeks to rezone the property at 151 East Keith Road to allow for the development of 40 units of purpose-built rental housing, which will be housed in two buildings: a 4-storey apartment building and a 4-storey townhome building, for a floor space ratio of 0.88. Existing tenants of the property will not be displaced as a result of this proposal.

The applicant team welcomes your input and invites you to join us to learn about the proposal.

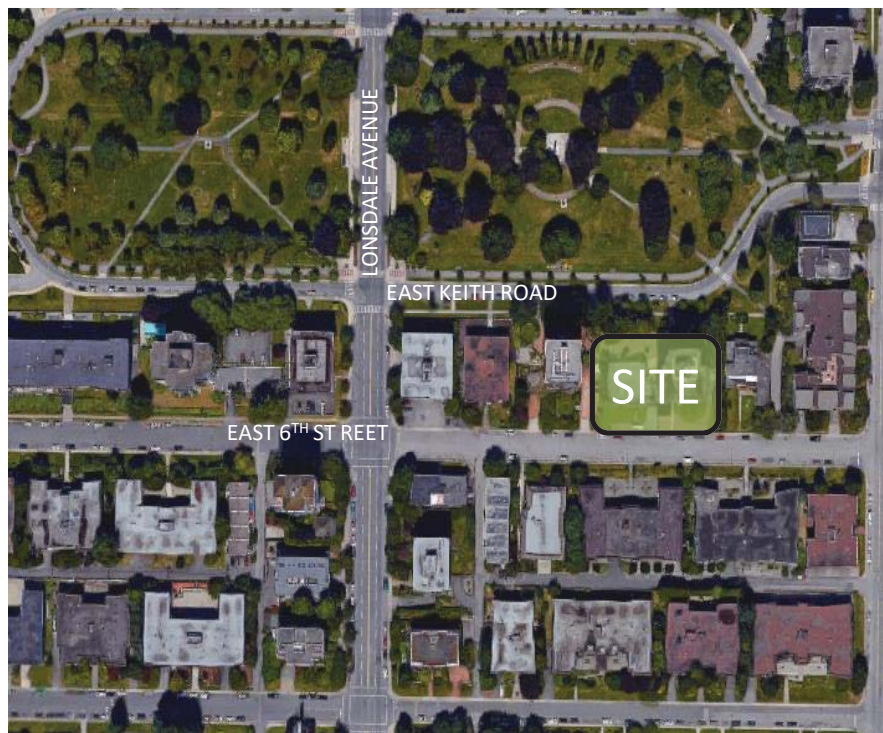
## Neighbourhood Open House Details

**Date:** Wednesday, March 27, 2019

**Time:** 5:00pm – 7:00pm (*drop in*)

**Place:** The Friendship Room, St. Andrews Church

**Address:** 1044 St. Georges Avenue, North Vancouver



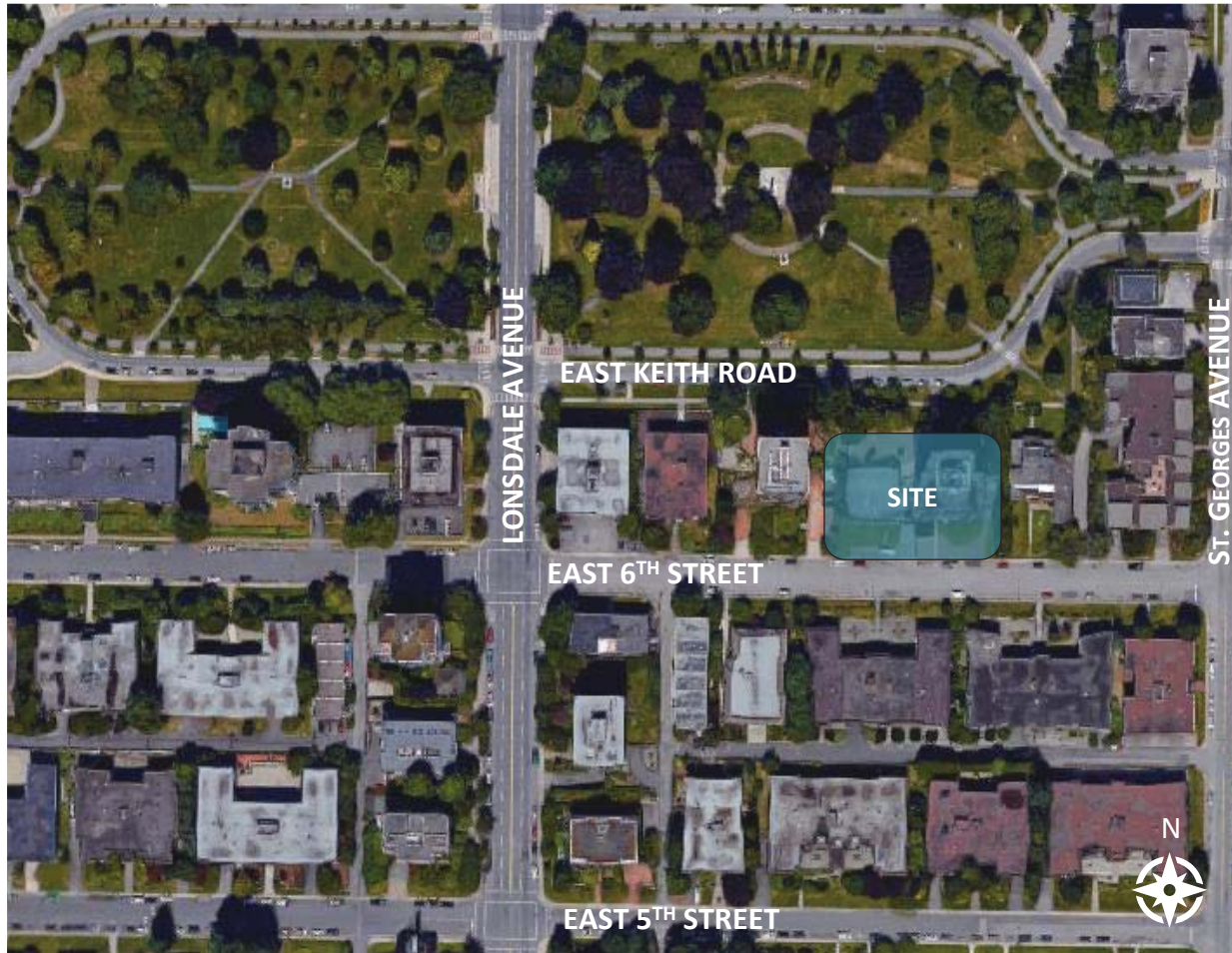
This is not a City of North Vancouver event. This Open House is being hosted voluntarily by the applicant team.

For further information please contact:

**Virginia Bird** at 604-619-0837



# Welcome



**Site Aerial (Above):** The site is located at 151 East Keith Road in North Vancouver, between Lonsdale Avenue and St. Georges Avenue

Starlight Investments, in collaboration with BHA Architecture, are proposing to increase rental housing capacity at 151 East Keith Road through modest, low-rise infill development.

The purpose of tonight's Neighbourhood Open House is to share our vision for the site, provide information about the planning process and local development policies and to gain valuable feedback from our neighbours and the community.

This Open House is being held voluntarily by the applicant team.

# Project Team



Starlight Investments

**Starlight Investments Ltd.** is a real estate asset management firm with a primary focus on multifamily rental properties across Canada and the Southern United States. Starlight's innovative approach to the properties under its management is reflected in its commitment to excellence in quality design and construction for both existing assets and infill development projects alike.



BHA Architecture

**BHA Architecture** was formed in 1991, and has developed expertise in a variety of construction forms and municipal processes. The firm has executed public and private sector projects across Western Canada and has developed a track record of producing imaginative and functional design solutions.



Urban Systems

**Urban Systems** is a multi-disciplinary planning firm founded in 1975 whose innovative approach to land development has resulted in some of the most celebrated development projects in Western Canada. Urban Systems harnesses the expertise of professional engineers, community planners, policy analysts, landscape architects, environmental scientists, communication experts, technologists, legal surveyors, and administrative professionals.



Connect Landscape Architecture

**Connect Landscape Architecture** is recognized as a leader in the design and transformation of healthy vibrant communities. Connect draws inspiration from the natural landscape while embracing built form and infrastructure.

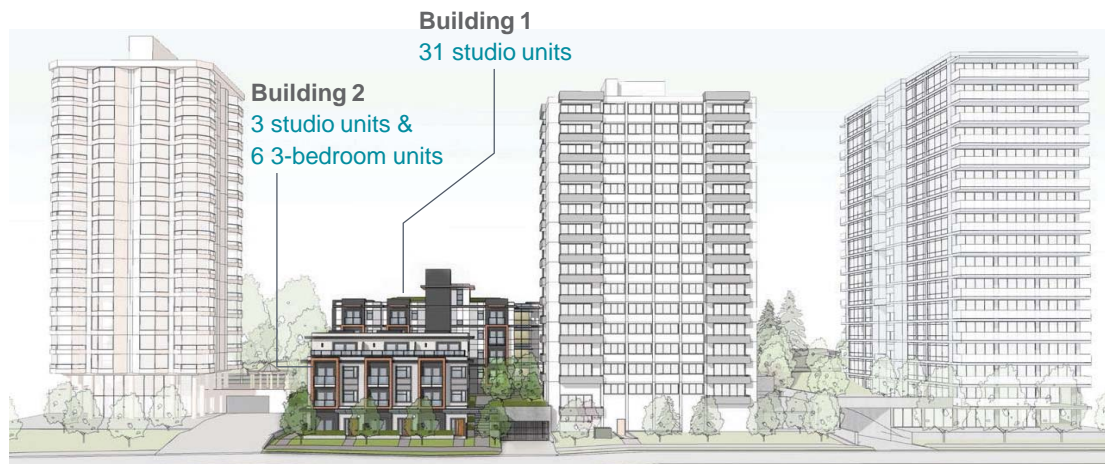




# Site & Neighbourhood Context



**Site Elevations (Above):** View of the proposed infill development and neighbouring buildings. View shown from East Keith Road.



**Site Elevations (Above):** View of the proposed infill development and neighbouring buildings. View shown from East 6<sup>th</sup> Street.



**Site Aerial (Above):** The site is located at 151 East Keith Road in North Vancouver, between Lonsdale Avenue and St. Georges Avenue.

## Site Context

The site is located on the south side of Keith Road, two blocks east of Lonsdale Avenue. Adjacent to the north, is Victoria Park, while the existing tower is flanked by a 14 storey condominium to the west and a 16 storey condominium to the east. The immediate area is predominantly multi-family dwellings ranging between 2-14 storeys in height. The site is located within close walking distance to shopping, community amenities and transit, including the Seabus.

# Policy Context

## Policy Context

Our proposal intends to respond to the well documented need for more rental housing in the City of North Vancouver. The City has continued to see a tightening rental market, with the documented vacancy rate down to 0.8% in 2017 from 1.3% the previous year. The vacancy rate for family oriented, 3 bedroom rental units in 2017 was even lower at 0% (Source: *CMHC 2018 Rental Housing Market Report*).

## Official Community Plan (OCP)

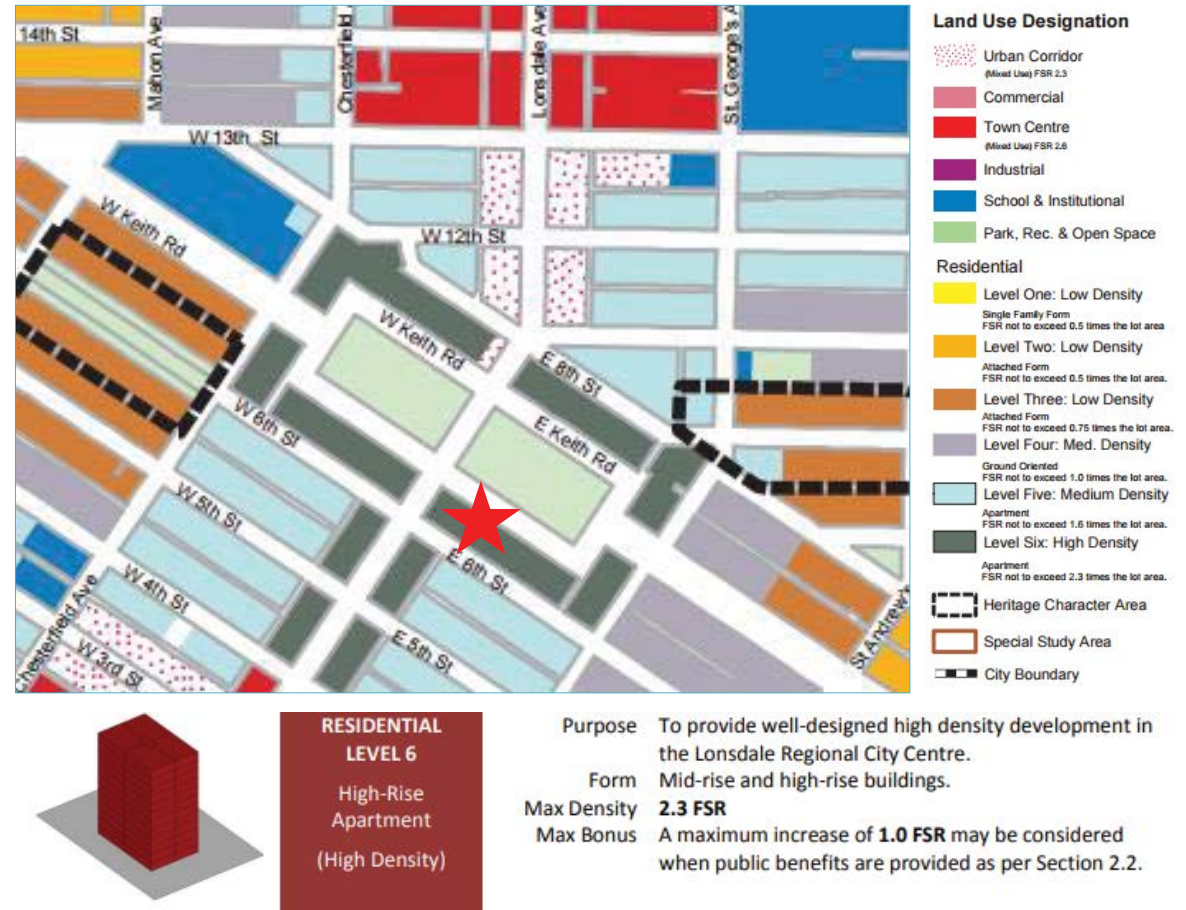
The OCP was adopted in 2014 after a 3-year public consultation process. The project aligns with the current allowable density bonus as per the *City's Official Community Plan (OCP 2014)*.

## Current Zoning

The site is zoned as Level Six: High Density. The OCP notes that "Council may approve additional floor area if there is a commitment to provide market or non-market rental housing". (*OCP 2014, Section 2.2*).

## Housing Action Plan (HAP)

In alignment with the *Housing Action Plan (HAP 2016)*, 10% of the homes will be offered at below-market rates. These units will be rented at SAFER rates (30% below CMHC average) and secured in perpetuity, exceeding City requirements. The proposal also exceeds the HAP's family housing requirement (10% of total units) with 15% of the proposed units as 3-bedroom, family oriented homes.



**Above:** City of North Vancouver Land Use Designation Map (2014). The site is zoned as Level Six: High Density.



# Community Benefits



## Increase in Rental Housing Stock

The proposal seeks to add 40 new purpose built rental homes on the site. 100% of the new homes will be secured as rental homes.



## No Tenant Displacement

There will be no loss of existing rental homes on the site. None of the existing tenants in the 89 suite building will be displaced.



## Support for Seniors

Starlight has established a partnership with a local service provider to provide greater below market housing to seniors.



## Design Excellence

The project received unanimous support from the City's Advisory Design Panel in February 2017.



## Maximization of Existing Site

The development will maximize the use of the existing site, including the currently underutilized underground parkade, resulting in minimal disruption to current tenants and neighbours.



## New Pedestrian Crossing

New stairs and landscaped pathways will allow pedestrian travel from East 6<sup>th</sup> Street to East Keith Road and Victoria Park.



## Affordable Housing

In alignment with the Housing Action Plan, 10% of the homes will be offered at below-market rates. These units will be rented at SAFER rates (30% below CMHC average) and secured in perpetuity, exceeding City requirements.



## Housing for All

The development will include 34 studio units and six 3-bedroom units. 14 of the units will be 'adaptable units'.



## Alignment with City Vision

The project aligns with the City's Official Community Plan (OCP).



## Commitment to Sustainability

A new bike repair station and 68 new bicycle parking spaces, including some with e-charging stations, will be provided on-site. A \$25,000 contribution will help finance new EV charging stations in the City.

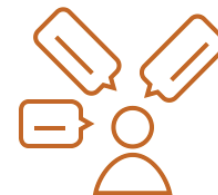


## On-Site Amenities

Communal amenities will be provided for all tenants including an upgraded gym and rooftop deck. A new accessible pathway and landscaping will provide enhanced access to the existing building.

# Community Feedback

In response to feedback from the Developers Information Session (April 26, 2017), discussions with community stakeholders and the comments at the Public Hearing (April 23, 2018), the design team has made a number of changes to the proposal for 151 East Keith Road.

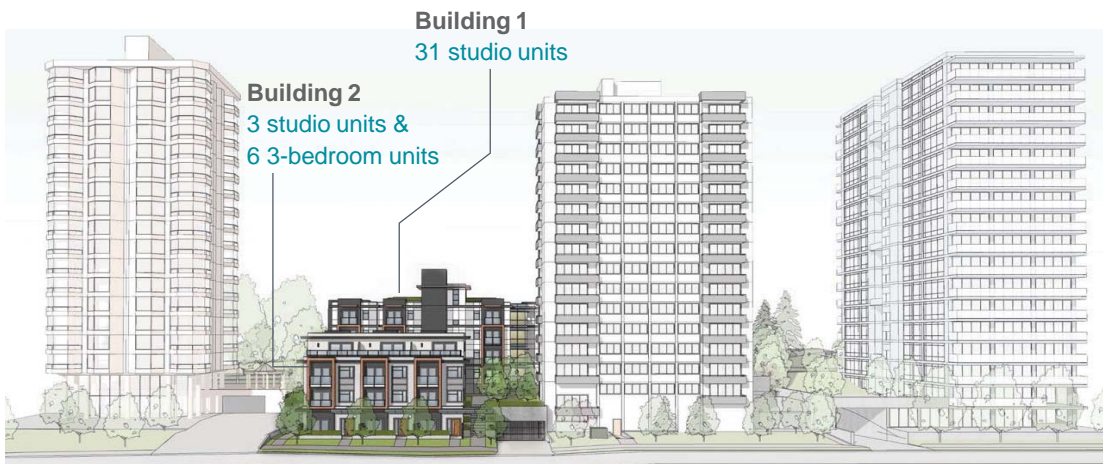


What We Heard		How We Responded
<b>Key Areas of Support</b> <ul style="list-style-type: none"> <li>Increased rental and family oriented housing supply</li> <li>Inclusion of affordable housing for seniors</li> <li>The design (architecture, height and density)</li> <li>Better utilization of the site with no tenant displacement</li> <li>Strategic location (close proximity to transit, community amenities and retail)</li> </ul>	<b>Key Areas of Concern</b> <ul style="list-style-type: none"> <li>Increased density</li> <li>Proposed setbacks (proximity to Victoria Park and loss of privacy of neighbouring buildings)</li> <li>Loss of trees and open space on the site</li> <li>The site is currently is not wheelchair accessible</li> </ul>	<ul style="list-style-type: none"> <li><b>Access:</b> A new ramp is proposed to provide barrier-free access from East Keith Road to the existing tower, proposed apartment building and underground parkade (via new elevator)</li> <li><b>Green Space:</b> New landscaping and trees will be added along the ramp to create a garden path experience</li> <li><b>Building Form:</b> The proposed townhouse building fronting East Keith Road (and Victoria Park) from previous rezoning application has been removed (reduction in density from 0.91 to 0.88 FSR and increased setback)</li> <li><b>Connectivity:</b> New exterior stairs and landscaped, accessible pathways will allow pedestrian travel from East 6<sup>th</sup> Street through the site to East Keith Road and Victoria Park</li> </ul>

# The Proposal



**Site Elevations (Above):** View of the proposed infill development and neighbouring buildings. View shown from East Keith Road.



**Site Elevations (Above):** View of the proposed infill development and neighbouring buildings. View shown from East 6<sup>th</sup> Street.

## Project Summary

The application seeks to rezone the property to allow for the development of 40 new rental homes, which will be housed in two 4-storey buildings (townhome building and apartment building), for an additional floor space ratio of 0.88. In alignment with the Housing Action Plan, 10% of the homes will be offered at below-market rates. These units will be rented at SAFER rates (30% below CMHC average) and secured in perpetuity, exceeding City requirements.

The site currently features a 14-storey purpose-built rental tower (built in 1972) and a 2-storey underground parking garage. The proposal is strictly for infill housing, meaning the existing building on site will remain in place and that no existing tenants will be displaced as a result of this proposal.

## Project Statistics

Project Summary		
Height	4 storeys	
Density	Proposed Infill: 0.88 FSR Existing: 2.29 FSR Total: 3.17 FSR	
Site Area	27997 sq. ft.	
Unit Breakdown	Building 1 (Apartments)	Building 2 (Townhomes)
Total Units	31	9
Studio	31	3
3 Bedroom	-	6
Vehicle Parking	77 stalls	
Bicycle Parking	68 new spaces	



# Rendering



View from Victoria Park  
looking southwest



[illegible]

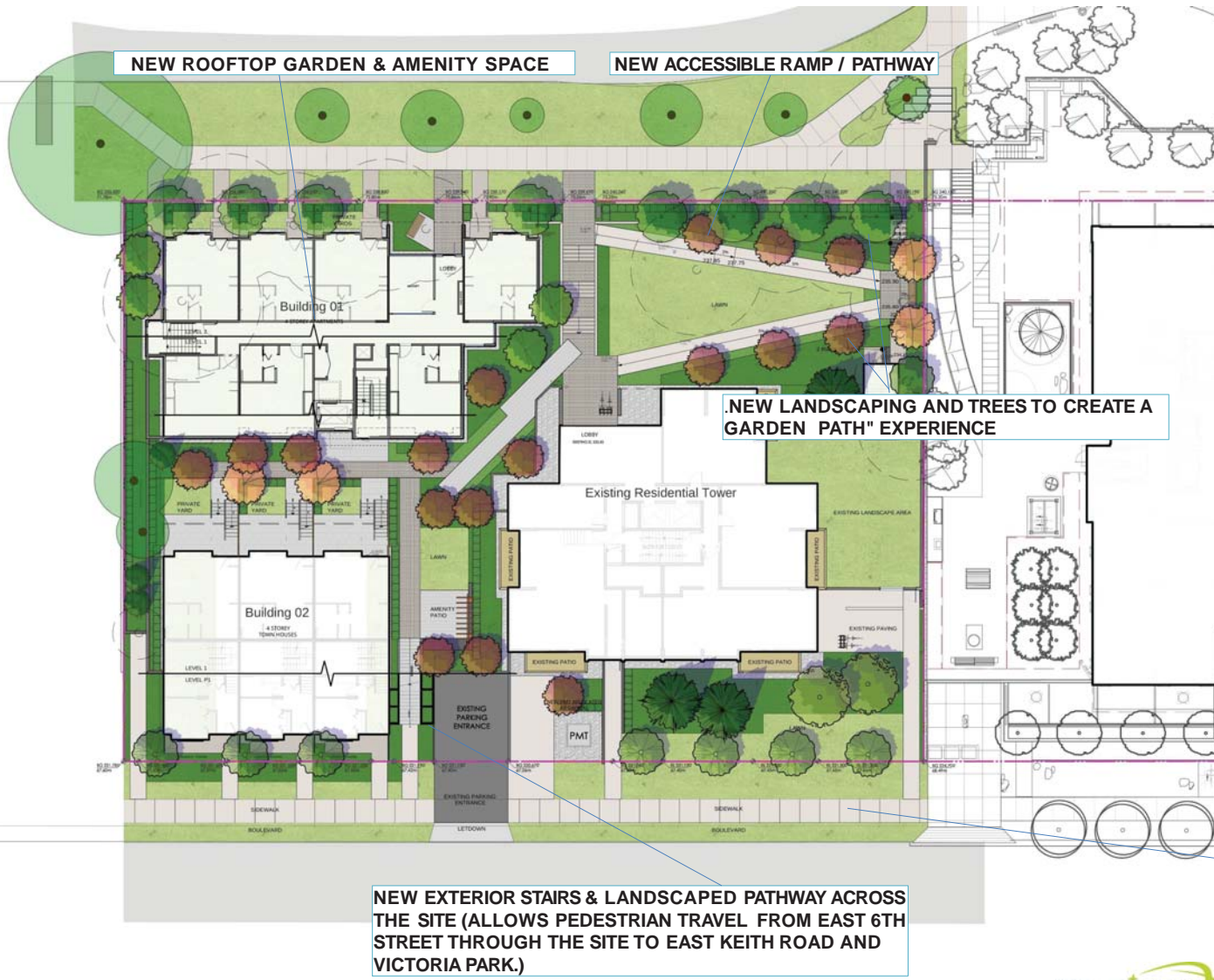
EXISTING  
RESIDENTIAL  
TOWER  
(161 E KEITH RD)

EAST 6TH STREET

- A 1.5m LANDSCAPED BOULEVARD AND A WIDENED SIDEWALK WILL BE ADDED ALONG EAST 6TH ST TO ENHANCE THE STREETSCAPE.

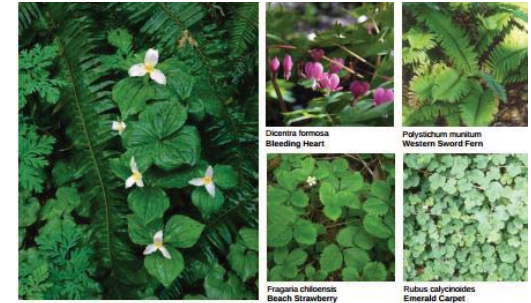


# Landscape Plan



## Planting Character

### Evergreen & Shade Plants



### Pacific Northwest Plants



NEW LANDSCAPED BOULEVARD AND A WIDENED SIDEWALK WILL BE ADDED ALONG EAST 6TH STREET TO ENHANCE THE STREETSCAPE.



# 3D Studies



EAST KEITH ROAD ELEVATION



EAST KEITH ROAD - LOOKING WEST



EAST KEITH ROAD - LOOKING EAST

# 3D Studies



EAST 6TH STREET ELEVATION



EAST 6TH STREET - LOOKING WEST



EAST 6TH STREET - LOOKING EAST



# Process Timeline

The following diagram provides an overview of the steps ahead and opportunities for public input on the infill rental housing proposal for 151 East Keith Road.

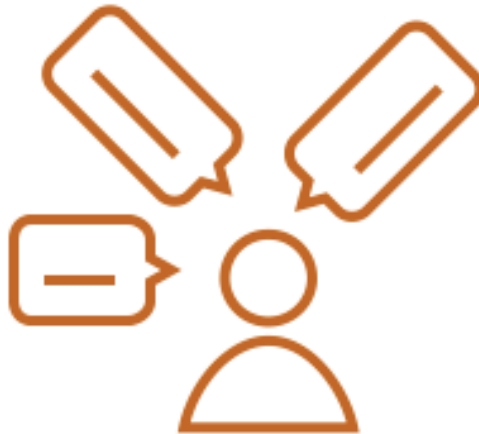


# Thank You

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Thank you for attending our Neighbourhood Open House.

Your feedback, insights and ideas are important to us. Please take a moment to fill in a comment card to share your feedback with the project team.



# COMMENT CARD

Neighbourhood Open House  
Infill Rental Housing Proposal  
151 East Keith Road, North Vancouver

March 27, 2019

1. Tell us a little about yourself. Check those applicable to you:

☒ I live in the City of North Vancouver

☐ I work in the City of North Vancouver

☒ I own my home

☐ I rent my home

2. Are there elements of the proposed plan that you like? If so, what are they?

Larger family units

Starter home units

Modest infill

Fits with character of neighbourhood.

Roof top patios

3. Are there elements of the proposed plan that you believe could be improved? If so, do you have suggestions on how they might be improved?

No, view corridors have been maximized already while not overbearing the site

# COMMENT CARD

Neighbourhood Open House  
Infill Rental Housing Proposal  
151 East Keith Road, North Vancouver

March 27, 2019

1. Tell us a little about yourself. Check those applicable to you:

☐ I live in the City of North Vancouver

☒ I work in the City of North Vancouver

☒ I own my home

☐ I rent my home

2. Are there elements of the proposed plan that you like? If so, what are they?

The design  
• How it fits in the community  
• Proximity to green space - great for seniors & small families  
• How it fits into the existing neighbourhood  
• Pleased to see units family size units - need some children in that neighbourhood

⊕ Love the housing available to low-income ~~families~~ seniors

3. Are there elements of the proposed plan that you believe could be improved? If so, do you have suggestions on how they might be improved?



# COMMENT CARD

Neighbourhood Open House  
Infill Rental Housing Proposal  
151 East Keith Road, North Vancouver

March 27, 2019

4. Do you have any other comments you'd like to share with us?

Pleased to see a well designed smaller building  
rather than another 17 story high rise

also like the changes to the design - greater  
set back

It 'softens' the concrete jungle surrounding the  
area yet it fits in

What a fabulous opportunity for small families  
with a beautiful park across the street.

Contact Information Please Print:

Name: Joey Hayden

Address: 104 - 267 W. Esplanade

Phone: [REDACTED]

Email: [REDACTED]

Would you like to be contacted for future updates? (please leave an email)

☒ Yes / ☐ No (circle)

Please return your comment sheet to the registration table or via email to [kirsten@pottingerbird.com](mailto:kirsten@pottingerbird.com) by April 10, 2019.

Thank you for your feedback!

Good luck !!

# COMMENT CARD

Neighbourhood Open House  
Infill Rental Housing Proposal  
151 East Keith Road, North Vancouver

March 27, 2019

1. Tell us a little about yourself. Check those applicable to you:

☒ I live in the City of North Vancouver

☒ I work in the City of North Vancouver

☐ I own my home

☒ I rent my home

2. Are there elements of the proposed plan that you like? If so, what are they?

All of them!

3. Are there elements of the proposed plan that you believe could be improved? If so, do you have suggestions on how they might be improved?

Build sooner

# COMMENT CARD

Neighbourhood Open House  
Infill Rental Housing Proposal  
151 East Keith Road, North Vancouver

March 27, 2019

4. Do you have any other comments you'd like to share with us?

Great company!  
Great building!  
Great Manageress.

Contact Information Please Print:

Name:

Ken Megale

Address:

1601-151 Keith Rd E

Phone:

[REDACTED]

Email:

[REDACTED]

Would you like to be contacted for future updates? (please leave an email)

☒ Yes / ☐ No (circle)

Please return your comment sheet to the registration table or via email to [kirsten@pottingerbird.com](mailto:kirsten@pottingerbird.com) by April 10, 2019.

Thank you for your feedback!

# COMMENT CARD

Neighbourhood Open House  
Infill Rental Housing Proposal  
151 East Keith Road, North Vancouver

March 27, 2019

1. Tell us a little about yourself. Check those applicable to you:

☒ I live in the City of North Vancouver

☒ I work in the City of North Vancouver

☒ I own my home

☐ I rent my home

2. Are there elements of the proposed plan that you like? If so, what are they?

I like that the plan is supporting the development of more rental housing (much needed), especially in the form of studio and 3-bedroom units, which we are particularly short on. I also don't think this development will be overly disruptive to existing residents or detract from their neighbourhood.

3. Are there elements of the proposed plan that you believe could be improved? If so, do you have suggestions on how they might be improved?

None.



# COMMENT CARD

Neighbourhood Open House  
Infill Rental Housing Proposal  
151 East Keith Road, North Vancouver

March 27, 2019

4. Do you have any other comments you'd like to share with us?

People dislike change, but that shouldn't impede progress. This development is part of a solution to a larger problem. I hope it proceeds.

Contact Information Please Print:

Name: Catrina Remickson

Address: 66-728 14<sup>th</sup> St. W. North Van

Phone: [REDACTED]

Email: \_\_\_\_\_

Would you like to be contacted for future updates? (please leave an email)

Yes / No (circle)

Please return your comment sheet to the registration table or via email to [kirsten@pottingerbird.com](mailto:kirsten@pottingerbird.com) by April 10, 2019.

Thank you for your feedback!

# COMMENT CARD

Neighbourhood Open House  
Infill Rental Housing Proposal  
151 East Keith Road, North Vancouver

March 27, 2019

1. Tell us a little about yourself. Check those applicable to you:

☒ I live in the City of North Vancouver

☐ I work in the City of North Vancouver

☒ I own my home

☐ I rent my home

2. Are there elements of the proposed plan that you like? If so, what are they?

- ★ SUPPORT FOR SENIORS !!
- 3 bedroom rentals are needed in North Van! (AFFORDABLE)
- THRILLED TO SEE A PROGRESSIVE PLAN.
- BETTER AMENITIES ARE NEEDED IN RENTAL BUILDINGS. I am happy to see that there will be upgrades.

3. Are there elements of the proposed plan that you believe could be improved? If so, do you have suggestions on how they might be improved?

# COMMENT CARD

Neighbourhood Open House  
Infill Rental Housing Proposal  
151 East Keith Road, North Vancouver

March 27, 2019

4. Do you have any other comments you'd like to share with us?

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Contact Information Please Print:

Name: Emily D

Address: 155 2nd St. West

Phone: 

Email: \_\_\_\_\_

Would you like to be contacted for future updates? *(please leave an email)*

☒ Yes / ☐ No (circle)

Please return your comment sheet to the registration table or via email to [kirsten@pottingerbird.com](mailto:kirsten@pottingerbird.com) by April 10, 2019.

Thank you for your feedback!

# COMMENT CARD

12

Neighbourhood Open House  
Infill Rental Housing Proposal  
151 East Keith Road, North Vancouver

March 27, 2019

1. Tell us a little about yourself. Check those applicable to you:

☒ I live in the City of North Vancouver

☐ I work in the City of North Vancouver

☒ I own my home

☐ I rent my home

2. Are there elements of the proposed plan that you like? If so, what are they?

Adapt - Increase to rental stock  
- seniors below market rental  
- 3 bedroom units for families  
- adaptable units  
- no displacement of existing residents.

3. Are there elements of the proposed plan that you believe could be improved? If so, do you have suggestions on how they might be improved?



Neighbourhood Open House  
Infill Rental Housing Proposal  
151 East Keith Road, North Vancouver

4. Do you have any other comments you'd like to share with us?

This image shows a single sheet of white paper with horizontal blue or grey ruling lines. The lines are evenly spaced and run across the width of the page. There is no handwriting or other markings on the paper.

Name: Robyn Newton

Address: 2104 Grand Blvd

Phone: 6

Email: [REDACTED]

Yes / No (circle)

Thank you for your feedback!

# COMMENT CARD

Neighbourhood Open House  
Infill Rental Housing Proposal  
151 East Keith Road, North Vancouver

March 27, 2019

1. Tell us a little about yourself. Check those applicable to you:

- ☒ I live in the City of North Vancouver ☐ I work in the City of North Vancouver
- ☐ I own my home ☒ I rent my home

2. Are there elements of the proposed plan that you like? If so, what are they?

- Rental stock will increase without displacing current units. This is a true increase in available rentals, not just replacing a building.
- Affordability. Affordability is a real concern in the CNV. Rental prices seem to increase every month, and there doesn't seem to be an end in sight. I want to live in a vibrant and diverse city, not a city that is only accessible to the highest income earners.
- I REALLY CAN'T EMPHASIZE ENOUGH HOW MUCH I SUPPORT THIS PROJECT.

3. Are there elements of the proposed plan that you believe could be improved? If so, do you have suggestions on how they might be improved?

No, I think it's great!

# COMMENT CARD

Neighbourhood Open House  
Infill Rental Housing Proposal  
151 East Keith Road, North Vancouver

March 27, 2019

## 4. Do you have any other comments you'd like to share with us?

It's so important to recognize progress and growth.  
The city is growing and people need a place to live.  
I've heard so many people against development because  
they are against change and that is unacceptable.  
We have an enviable lifestyle here in the city of North  
Vancouver and that lifestyle is open to everyone. We all  
need a place to live and a community to call home, and  
I, for one, love this city. But the nay side is the real problem  
here. Wimpyism and thinly veiled classism hurt this  
community. Let's be real about the projects that bring out  
hardcore opponents - it's not the luxury towers that seem  
to bring out the worst. We all need a place to live, so  
let's build homes.

## Contact Information Please Print:

Name: Stephanie Wilson

Address: 209-131 E. 3rd St. North Vancouver V7L 1E5

Phone: 

Email: 

Would you like to be contacted for future updates? (please leave an email)

☒ Yes / ☐ No (circle)

Please return your comment sheet to the registration table or via email to [kirsten@pottingerbird.com](mailto:kirsten@pottingerbird.com) by April 10, 2019.

Thank you for your feedback!



# COMMENT CARD

Neighbourhood Open House  
Infill Rental Housing Proposal  
151 East Keith Road, North Vancouver

March 27, 2019

1. Tell us a little about yourself. Check those applicable to you:

- ☒ I live in the City of North Vancouver ☐ I work in the City of North Vancouver
- ☐ I own my home ☒ I rent my home

2. Are there elements of the proposed plan that you like? If so, what are they?

all! → more affordable housing is needed in this city  
especially projects that ~~don't~~<sup>doesn't</sup> displace current tenants.  
→ makes the whole street look better and more modern

3. Are there elements of the proposed plan that you believe could be improved? If so, do you have suggestions on how they might be improved?

nope. needs to move forward



Neighbourhood Open House  
Infill Rental Housing Proposal  
151 East Keith Road, North Vancouver

4. Do you have any other comments you'd like to share with us?

[illegible]

Name: RHIA ROMANO

Address: 138 E. ESPLANADE

Phone: \_\_\_\_\_

Email: \_\_\_\_\_

Would you like to be contacted for future updates? *(please leave an email)*

Yes / No (circle)

Please return your comment sheet to the registration table or via email to [kirsten@pottingerbird.com](mailto:kirsten@pottingerbird.com) by April 10, 2019.

Page 2 of 2

# COMMENT CARD

Neighbourhood Open House  
Infill Rental Housing Proposal  
151 East Keith Road, North Vancouver

March 27, 2019

1. Tell us a little about yourself. Check those applicable to you:

☒ I live in the City of North Vancouver

☐ I work in the City of North Vancouver

☒ I own my home

☐ I rent my home

2. Are there elements of the proposed plan that you like? If so, what are they?

(1) IMPROVED ACCESS  
(2) REVISED LAYOUT (BUILDING 3 REMOVED)  
(3) MORE LANDSCAPING AND TREES

3. Are there elements of the proposed plan that you believe could be improved? If so, do you have suggestions on how they might be improved?

EXCELLENT RESPONSE TO COMMENTS & CONCERNS  
RAISED AT THE PUBLIC HEARING LAST YEAR.

# COMMENT CARD

Neighbourhood Open House  
Infill Rental Housing Proposal  
151 East Keith Road, North Vancouver

March 27, 2019

4. Do you have any other comments you'd like to share with us?

I look forward to seeing this  
project being built! We need more  
rental apartments!

Contact Information Please Print:

Name: DAVID FAULSTY

Address: 1726 RUFUS DR, NORTH VAN

Phone:

Email:

Would you like to be contacted for future updates? (please leave an email)

☒ Yes / ☐ No (circle)

Please return your comment sheet to the registration table or via email to [kirsten@pottingerbird.com](mailto:kirsten@pottingerbird.com) by April 10, 2019.

Thank you for your feedback!

# COMMENT CARD

Neighbourhood Open House  
Infill Rental Housing Proposal  
151 East Keith Road, North Vancouver

March 27, 2019

1. Tell us a little about yourself. Check those applicable to you:

☒ I live in the City of North Vancouver

☐ I work in the City of North Vancouver

☐ I own my home

☒ I rent my home

2. Are there elements of the proposed plan that you like? If so, what are they?

- Like the aesthetics
- Integrates well with neighbouring building

3. Are there elements of the proposed plan that you believe could be improved? If so, do you have suggestions on how they might be improved?

Always curious about high rise vs 4 storey integrations  
but can't think of any changes.



# COMMENT CARD

Neighbourhood Open House  
Infill Rental Housing Proposal  
151 East Keith Road, North Vancouver

March 27, 2019

4. Do you have any other comments you'd like to share with us?

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\_\_\_\_\_

Contact Information Please Print:

Name: \_\_\_\_\_

Address: \_\_\_\_\_

Phone: \_\_\_\_\_

Email: \_\_\_\_\_

Would you like to be contacted for future updates? *(please leave an email)* Yes / No (circle)

Please return your comment sheet to the registration table or via email to [kirsten@pottingerbird.com](mailto:kirsten@pottingerbird.com) by April 10, 2019.

Thank you for your feedback!

# COMMENT CARD

Neighbourhood Open House  
Infill Rental Housing Proposal  
151 East Keith Road, North Vancouver

March 27, 2019

1. Tell us a little about yourself. Check those applicable to you:

☒ I live in the City of North Vancouver

☒ I work in the City of North Vancouver

☐ I own my home

☒ I rent my home

2. Are there elements of the proposed plan that you like? If so, what are they?

None

3. Are there elements of the proposed plan that you believe could be improved? If so, do you have suggestions on how they might be improved?

DO NOT BUILD.

# COMMENT CARD

Neighbourhood Open House  
Infill Rental Housing Proposal  
151 East Keith Road, North Vancouver

March 27, 2019

4. Do you have any other comments you'd like to share with us?

NO - Do NOT BUILD - It is enough. Our green  
corridor will disappear - We will have constant noise  
trucks, tremors, dust, garbage - no parking  
smokers - screaming. Residents on East  
6<sup>th</sup> Street are worn-out -  
WE WANT A 5 YEAR CONSTRUCTION  
Break

Contact Information Please Print:

Name:

Ad de Wildt

Address:

141 East 6<sup>th</sup> Street

Phone:

Email:

I HAVE NOTHING GOOD TO SAY

Would you like to be contacted for future updates? (please leave an email)

Yes / No (circle)

Please return your comment sheet to the registration table or via email to [kirsten@pottingerbird.com](mailto:kirsten@pottingerbird.com) by April 10, 2019.

Thank you for your feedback!

# COMMENT CARD

Neighbourhood Open House  
Infill Rental Housing Proposal  
151 East Keith Road, North Vancouver

March 27, 2019

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☒ I live in the City of North Vancouver

☐ I work in the City of North Vancouver

☐ I own my home

☒ I rent my home

2. Are there elements of the proposed plan that you like? If so, what are they?

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3. Are there elements of the proposed plan that you believe could be improved? If so, do you have suggestions on how they might be improved?

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# COMMENT CARD

Neighbourhood Open House  
Infill Rental Housing Proposal  
151 East Keith Road, North Vancouver

March 27, 2019

4. Do you have any other comments you'd like to share with us?

I am very concerned about the construction noise & length of time to build - 1 1/2 years. I am also concerned about the additional vehicle traffic on 6th St to the underground parking and extra demand for street parking, which is already in very high demand.

Contact Information Please Print:

Name: Dean Rath

Address: #202 - 141 E 6th St.

Phone: [REDACTED]

Email: [REDACTED]

Would you like to be contacted for future updates? (please leave an email)

Yes / No (circle)

Please return your comment sheet to the registration table or via email to [kirsten@pottingerbird.com](mailto:kirsten@pottingerbird.com) by April 10, 2019.

Thank you for your feedback!

# COMMENT CARD

Neighbourhood Open House  
Infill Rental Housing Proposal  
151 East Keith Road, North Vancouver

March 27, 2019

1. Tell us a little about yourself. Check those applicable to you:

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☐ I work in the City of North Vancouver

☐ I own my home

☒ I rent my home

2. Are there elements of the proposed plan that you like? If so, what are they?

Not really - I do not wish this  
development to proceed.

3. Are there elements of the proposed plan that you believe could be improved? If so, do you have suggestions on how they might be improved?

yes - abandon this project -  
Not necessary - we don't need a wall  
of condos facing us.

# COMMENT CARD

Neighbourhood Open House  
Infill Rental Housing Proposal  
151 East Keith Road, North Vancouver

March 27, 2019

4. Do you have any other comments you'd like to share with us?

Much needed - Thank you!

Contact Information Please Print:

Name: Gabrielle Loren

Address: 1004-175 Victory Ship Way Nv

Phone: [REDACTED]

Email: [REDACTED]

Would you like to be contacted for future updates? (please leave an email)

☒ Yes / ☐ No (circle)

Please return your comment sheet to the registration table or via email to [kirsten@pottingerbird.com](mailto:kirsten@pottingerbird.com) by April 10, 2019.

Thank you for your feedback!

# COMMENT CARD

Neighbourhood Open House  
Infill Rental Housing Proposal  
151 East Keith Road, North Vancouver

March 27, 2019

4. Do you have any other comments you'd like to share with us?

We have been through 2½ YEARS of  
construction already on this block.  
parking on street is a real problem.

Contact Information Please Print:

Name: CHRISTINA SILBERTSON

Address: # 203 - 141 E. 6TH ST.

Phone: [REDACTED]

Email: \_\_\_\_\_

Would you like to be contacted for future updates? (please leave an email)

Yes ☒ No ☐ (circle)

Please return your comment sheet to the registration table or via email to [kirsten@pottingerbird.com](mailto:kirsten@pottingerbird.com) by April 10, 2019.

Thank you for your feedback!



# COMMENT CARD

Neighbourhood Open House  
Infill Rental Housing Proposal  
151 East Keith Road, North Vancouver

March 27, 2019

1. Tell us a little about yourself. Check those applicable to you:

☒ I live in the City of North Vancouver

☐ I work in the City of North Vancouver

☐ I own my home

☒ I rent my home

2. Are there elements of the proposed plan that you like? If so, what are they?

- I DO NOT LIKE ANY OF THE PLANS.
- UNNECESSARY DEVEL.
- CONSTRUCTION FATIGUE - DUE TO 16 STOREY BLD. NEXT DOOR
- OBSTRUCTION TO VIEW CORRIDOR.
- PARKING PROBLEMS
- NOISE

3. Are there elements of the proposed plan that you believe could be improved? If so, do you have suggestions on how they might be improved?

NO - I JUST WANT IT TO BE BUILT.

# COMMENT CARD

Neighbourhood Open House  
Infill Rental Housing Proposal  
151 East Keith Road, North Vancouver

March 27, 2019

4. Do you have any other comments you'd like to share with us?

THIS DEVELOPMENT SEEMS TO ME TO BE  
GREED!

WE ALREADY HAVE A CONCRETE WALK ON  
OUR STREET.

Contact Information Please Print:

Name: LARA GILBERTSON

Address: #203-141 E. 6TH ST.

Phone:

Email:

Would you like to be contacted for future updates? (please leave an email)

Yes / No (circle)

Please return your comment sheet to the registration table or via email to [kirsten@pottingerbird.com](mailto:kirsten@pottingerbird.com) by April 10, 2019.

Thank you for your feedback!

# COMMENT CARD

Neighbourhood Open House  
Infill Rental Housing Proposal  
151 East Keith Road, North Vancouver

March 27, 2019

1. Tell us a little about yourself. Check those applicable to you:

☒ I live in the City of North Vancouver

☐ I work in the City of North Vancouver

☒ I own my home

☐ I rent my home

2. Are there elements of the proposed plan that you like? If so, what are they?

None.

3. Are there elements of the proposed plan that you believe could be improved? If so, do you have suggestions on how they might be improved?

↑ to be improved.  
Setback from E. Keith Road. The building encroaches on the sidewalk. As a pedestrian the building has an overbearing presence. It is totally out of character with the neighbourhood.

Neighbourhood Open House  
Infill Rental Housing Proposal  
151 East Keith Road, North Vancouver

4. Do you have any other comments you'd like to share with us?

[illegible]

Name: Shirley Schneider

Address: 901-140 Keith Rd E.

Phone: [REDACTED]

Email: \_\_\_\_\_

Would you like to be contacted for future updates? *(please leave an email)*

Yes / No (circle)

Please return your comment sheet to the registration table or via email to [kirsten@pottingerbird.com](mailto:kirsten@pottingerbird.com) by April 10, 2019.

Thank you for your feedback!



# COMMENT CARD

Neighbourhood Open House  
Infill Rental Housing Proposal  
151 East Keith Road, North Vancouver

March 27, 2019

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☒ I work in the City of North Vancouver

☐ I own my home

☒ I rent my home

2. Are there elements of the proposed plan that you like? If so, what are they?

No

We have no need for unaffordable rental housing.

3. Are there elements of the proposed plan that you believe could be improved? If so, do you have suggestions on how they might be improved? DO NOT BUILD.

The corridor is going to disappear. Most of the greenery + park will be obscured by this building specially for those that live on East 6<sup>th</sup>. We are fed up with construction, trucks no parking, Excessive noise, garbage noise - ground tremors smokers, foul language. THE TRAFFIC IS OUTRAGEOUS and our patience has reached its limit.

Neighbourhood Open House  
Infill Rental Housing Proposal  
151 East Keith Road, North Vancouver

4. Do you have any other comments you'd like to share with us?

[illegible]

Name:

# Xwin Lamers

Address:

Phone:

Email:

\_\_\_\_\_

Yes / No (circle)

Thank you for your feedback!

# COMMENT CARD

Neighbourhood Open House  
Infill Rental Housing Proposal  
151 East Keith Road, North Vancouver

March 27, 2019

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☐ I work in the City of North Vancouver

☒ I own my home

☐ I rent my home

2. Are there elements of the proposed plan that you like? If so, what are they?

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3. Are there elements of the proposed plan that you believe could be improved? If so, do you have suggestions on how they might be improved?

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# COMMENT CARD

Neighbourhood Open House  
Infill Rental Housing Proposal  
151 East Keith Road, North Vancouver

March 27, 2019

4. Do you have any other comments you'd like to share with us?

This is an inappropriate project in an inappropriate place. The feel of the street will be completely changed with the units sitting in areas not similar to the rest of the buildings surrounding the park on East + West Keith Road. The onstreet parking problem will be exacerbated - the 'underutilized parking' is because tenants don't want to rent spaces, not because they don't have cars. The community finds this an imposition, and should be listened to.

Contact Information Please Print:

Name: WILLIAM HEISE

Address: 1501 - 140 KEITH RD. E., N. VAN.

Phone: [REDACTED]

Email: [REDACTED]

Would you like to be contacted for future updates? (please leave an email)

☒ Yes ☐ No (circle)

Please return your comment sheet to the registration table or via email to [kirsten@pottingerbird.com](mailto:kirsten@pottingerbird.com) by April 10, 2019.

Thank you for your feedback!



# COMMENT CARD

Neighbourhood Open House  
Infill Rental Housing Proposal  
151 East Keith Road, North Vancouver

March 27, 2019

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☐ I work in the City of North Vancouver

☒ I own my home

☐ I rent my home

2. Are there elements of the proposed plan that you like? If so, what are they?

Like that the proponent is willing to bring their plans to the neighbourhood for early viewing.

I also like the fact that the unsightly piece of land will be addressed.

3. Are there elements of the proposed plan that you believe could be improved? If so, do you have suggestions on how they might be improved?

The plan needs to address the west side variance issue. I live at 123 E. Keith and will certainly feel the impact of the solid wall massing in this design. The protruding stairwell structure in that wall makes the impact even worse. When the adjacent building has an open airy side yard it is not fair to the neighbourhood to build so close to the property line and expect the adjacent property to provide the ambience this area projects.

Please increase the ~~west~~ side setback and redesign your buildings to reflect that. We are well aware that not having to build new parking allotments will save this developer a lot of money but the neighbourhood will suffer for it. To allow the project to also destroy our light and openness corridors.

# COMMENT CARD

Neighbourhood Open House  
Infill Rental Housing Proposal  
151 East Keith Road, North Vancouver

March 27, 2019

4. Do you have any other comments you'd like to share with us?

I find it very disturbing that having seen this project detracted by a previous City council, it is now back in an even more disturbing design. "What" makes the proponent think he has a better chance to pass it this time. Do you know something this neighbourhood is not being told ???

I sincerely believe something needs to be built on this site and badly needed "social" housing should be built here. This is exactly the kind of site where the proponent has not had to factor in ~~the~~ acquiring land costs or providing any additional parking that they could provide badly needed "social" housing and still make a reasonable profit by building within the OC & allowed limits.

Contact Information Please Print:

Name: V. Daniels

Address: #802- 123 E. Keith Road, N. Van.

Phone: [REDACTED]

Email: \_\_\_\_\_

Would you like to be contacted for future updates? (please leave an email)

Yes / No (circle)

Please return your comment sheet to the registration table or via email to [kirsten@pottingerbird.com](mailto:kirsten@pottingerbird.com) by April 10, 2019.

Thank you for your feedback!

# COMMENT CARD

Neighbourhood Open House  
Infill Rental Housing Proposal  
151 East Keith Road, North Vancouver

March 27, 2019

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☐ I work in the City of North Vancouver

☒ I own my home

☐ I rent my home

2. Are there elements of the proposed plan that you like? If so, what are they?

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3. Are there elements of the proposed plan that you believe could be improved? If so, do you have suggestions on how they might be improved?

*- increase setbacks on E Keith*

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# COMMENT CARD

Neighbourhood Open House  
Infill Rental Housing Proposal  
151 East Keith Road, North Vancouver

March 27, 2019

## 4. Do you have any other comments you'd like to share with us?

- The 4-story<sup>+</sup> building needs to be set back further from Keith Rd E. The by-law requires 25 foot setback.
- Too much of this building is on the setback!
- This building is too high to be this close to the sidewalk & road
- parking - there are insufficient spaces for parking
  - the surrounding streets are full day and night
  - This is unacceptable to force more people to park on the street
  - too much potential car parking area is planned for bicycle lockers - too much space compared to car parking situation in this area

## Contact Information Please Print:

Name: LINDA HEESE

Address: 1501-140 KEITH RD E.

Phone: [REDACTED]

Email: \_\_\_\_\_

Would you like to be contacted for future updates? (please leave an email)

Yes / No (circle)

Please return your comment sheet to the registration table or via email to [kirsten@pottingerbird.com](mailto:kirsten@pottingerbird.com) by April 10, 2019.

Thank you for your feedback!



# COMMENT CARD

Neighbourhood Open House  
Infill Rental Housing Proposal  
151 East Keith Road, North Vancouver

March 27, 2019

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☒ I live in the City of North Vancouver

☐ I work in the City of North Vancouver

☒ I own my home

☐ I rent my home

2. Are there elements of the proposed plan that you like? If so, what are they?

Too close to Victoria Park + Victoria Place

No sight lines from Park.

- Streetwell lighting impacting  
wildlife + area residents

Affordable suites - I don't think so.

3. Are there elements of the proposed plan that you believe could be improved? If so, do you have suggestions on how they might be improved?

~~Don't Renters know~~

# COMMENT CARD

Neighbourhood Open House  
Infill Rental Housing Proposal  
151 East Keith Road, North Vancouver

March 27, 2019

4. Do you have any other comments you'd like to share with us?

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Contact Information Please Print:

Name: \_\_\_\_\_

Address: \_\_\_\_\_

Phone: \_\_\_\_\_

Email: \_\_\_\_\_

Would you like to be contacted for future updates? *(please leave an email)*

Yes / No (circle)

Please return your comment sheet to the registration table or via email to [kirsten@pottingerbird.com](mailto:kirsten@pottingerbird.com) by April 10, 2019.

Thank you for your feedback!

# COMMENT CARD

Neighbourhood Open House  
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151 East Keith Road, North Vancouver

March 27, 2019

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☒ I live in the City of North Vancouver

☐ I work in the City of North Vancouver

☒ I own my home

☐ I rent my home

2. Are there elements of the proposed plan that you like? If so, what are they?

What are you planning to do with the trees  
on the west side of these buildings?

How are you going to accommodate the people who  
live there?

3. Are there elements of the proposed plan that you believe could be improved? If so, do you have suggestions on how they might be improved?

Yes, other than the plan we don't want any more  
skyscrapers in central business district.

## COMMENT CARD

Neighbourhood Open House  
Infill Rental Housing Proposal  
151 East Keith Road, North Vancouver

March 27, 2019

4. Do you have any other comments you'd like to share with us?

This image shows a single sheet of white paper with horizontal blue or grey ruling lines. The lines are evenly spaced and run across the width of the page. There are approximately 20 lines visible. The paper appears to be a standard notebook page or a sheet of stationery. There is no handwriting or other markings on the page.

Contact Information Please Print:

Name: \_\_\_\_\_

Address: \_\_\_\_\_

Phone: \_\_\_\_\_

Email: \_\_\_\_\_

Would you like to be contacted for future updates? *(please leave an email)*

Yes / No (circle)

Please return your comment sheet to the registration table or via email to [kirsten@pottingerbird.com](mailto:kirsten@pottingerbird.com) by April 10, 2019.

Thank you for your feedback!



# COMMENT CARD

Neighbourhood Open House  
Infill Rental Housing Proposal  
151 East Keith Road, North Vancouver

March 27, 2019

1. Tell us a little about yourself. Check those applicable to you:

☒ I live in the City of North Vancouver

☒ I work in the City of North Vancouver

(retired)

(I did)

☒ I own my home

☐ I rent my home

2. Are there elements of the proposed plan that you like? If so, what are they?

The useway is most considerate ~~of~~ to all.

3. Are there elements of the proposed plan that you believe could be improved? If so, do you have suggestions on how they might be improved?

Building 1 is too close to the property line to the West AND to Keith Road. The setbacks are insulting to <sup>the</sup> community's need for open, uncluttered <sup>visual</sup> space, especially as people live in smaller & smaller apartments/studios.

Building 2 is too close to the property line to the west. 6th street set back is fine. It is taking advantage of the "air space" left on the east side of the property to the West (23).

# COMMENT CARD

Neighbourhood Open House  
Infill Rental Housing Proposal  
151 East Keith Road, North Vancouver

March 27, 2019

## 4. Do you have any other comments you'd like to share with us?

- (1) 10 of "below market" - only 4 of 40 units will be "below market" - and Starlight Properties are known for ~~charging~~ <sup>charging</sup> high rents, so ~~even~~ <sup>even</sup> those so affordability will be an issue (still).
- (2) Starlight Properties in North Van make a mockery of your first placard ("A Project Team"), which says how "excellent" the company is. Example - property at 150 East Keith has been under renovation since last November, and is still not finished (Masking tape for door numbers, cardboard still on floor in lobby, etc. etc.) Most residents see the "upgrade" as a downgrade (vulgar gray doors, cement tiling) - but would be okay with that if it would just be finished. Lack of attention shows lack of respect for tenants.

## Contact Information Please Print:

Name: STEPHEN SMITH

Address: \_\_\_\_\_

Phone: \_\_\_\_\_

Email: [REDACTED]

Would you like to be contacted for future updates? (please leave an email)

Yes / No (circle)

Please return your comment sheet to the registration table or via email to [kirsten@pottingerbird.com](mailto:kirsten@pottingerbird.com) by April 10, 2019.

Thank you for your feedback!

# COMMENT CARD

Neighbourhood Open House  
Infill Rental Housing Proposal  
151 East Keith Road, North Vancouver

March 27, 2019

1. Tell us a little about yourself. Check those applicable to you:

☒ I live in the City of North Vancouver

☐ I work in the City of North Vancouver

☐ I own my home

☒ I rent my home

2. Are there elements of the proposed plan that you like? If so, what are they?

① We need more rental housing in N.V. that is affordable.

② Proposed building & landscaping looks attractive in renderings.

3. Are there elements of the proposed plan that you believe could be improved? If so, do you have suggestions on how they might be improved?

## COMMENT CARD

Neighbourhood Open House  
Infill Rental Housing Proposal  
151 East Keith Road, North Vancouver

March 27, 2019

4. Do you have any other comments you'd like to share with us?

[illegible]

**Contact Information Please Print:**

Name: \_\_\_\_\_

Address: \_\_\_\_\_

Phone: \_\_\_\_\_

Email: \_\_\_\_\_

Would you like to be contacted for future updates? *(please leave an email)*

Yes / No (circle)

Please return your comment sheet to the registration table or via email to [kirsten@pottingerbird.com](mailto:kirsten@pottingerbird.com) by April 10, 2019.

Thank you for your feedback!



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Infill Rental Housing Proposal  
151 East Keith Road, North Vancouver

March 27, 2019

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☒ I live in the City of North Vancouver

☐ I work in the City of North Vancouver

☒ I own my home

☐ I rent my home

2. Are there elements of the proposed plan that you like? If so, what are they?

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3. Are there elements of the proposed plan that you believe could be improved? If so, do you have suggestions on how they might be improved?

*The setback is*

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# COMMENT CARD

Neighbourhood Open House  
Infill Rental Housing Proposal  
151 East Keith Road, North Vancouver

March 27, 2019

4. Do you have any other comments you'd like to share with us?

*- ~~But~~ Respect the design principles which have made this  
area beautiful to live <sup>in</sup> despite the density.  
By eliminating this view corridor you will set a  
precedent for many other Victoria Park properties*

Contact Information Please Print:

Name: *Wagne Collett -*

Address: *#1501 123 Keith Road East*

Phone: 

Email: 

Would you like to be contacted for future updates? (please leave an email)

☒ Yes / No (circle)

Please return your comment sheet to the registration table or via email to [kirsten@pottingerbird.com](mailto:kirsten@pottingerbird.com) by April 10, 2019.

Thank you for your feedback!

# COMMENT CARD

Neighbourhood Open House  
Infill Rental Housing Proposal  
151 East Keith Road, North Vancouver

March 27, 2019

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- ☒ I live in the City of North Vancouver ☐ I work in the City of North Vancouver
- ☐ I own my home ☐ I rent my home

2. Are there elements of the proposed plan that you like? If so, what are they?

I am supportive of ~~an~~ an infill project for this site  
but it must respect ~~the~~ neighbourhood values,  
particularly setbacks.

3. Are there elements of the proposed plan that you believe could be improved? If so, do you have suggestions on how they might be improved?

Victoria Park is probably the highest residential 2 block area in the city.  
Despite this density, Victoria Park is a very desirable area because  
of its ambience.  
This ambience is the result of the view corridors.  
This project reduces the western view corridor to a nominal width.  
The setback should 25 feet which will be consistent with the  
eastern setback.

# COMMENT CARD

Neighbourhood Open House  
Infill Rental Housing Proposal  
151 East Keith Road, North Vancouver

March 27, 2019

4. Do you have any other comments you'd like to share with us?

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Contact Information Please Print:

Name: \_\_\_\_\_

Address: \_\_\_\_\_

Phone: \_\_\_\_\_

Email: \_\_\_\_\_

Would you like to be contacted for future updates? *(please leave an email)*

Yes / No (circle)

Please return your comment sheet to the registration table or via email to [kirsten@pottingerbird.com](mailto:kirsten@pottingerbird.com) by April 10, 2019.

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March 27, 2019

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☒ I own my home

☐ I rent my home

2. Are there elements of the proposed plan that you like? If so, what are they?

- That it is 4-story is good - but I still think  
this development is misplaced

- That tenants are not displaced - since nobody there !!

- That it would remain "RENTAL" is perpetually

3. Are there elements of the proposed plan that you believe could be improved? If so, do you have suggestions on how they might be improved?

It is not the right place - looks awkward in between the  
highrises. And - like in the previous proposal - it boxes in the  
park even now that the one building is removed.

I don't think we should put ~~it~~ anything there just because  
there aren't any buildings right now.

It seems a bit desperate !!

# SIGN IN SHEET,

151 E Keith North Vancouver

March 27, 2019 (5:00 – 7:00 pm)

Name	Address	Email	Phone	Can we contact you with updates about this proposal (Y/N)?
Robert van Veen	102-151 Keith Rd. E., N. Van.			Y
Gudrun Loeven	1004-175 Victory Ship Way			Y
Dean Rath	141 E 6th St #202			✓
Jean Kennedy	123 East Keith #1401			Y
Georg Fitzpatrick	123 East Keith Rd			Y
Dan Finner	1726 Rufus Dr.			Y
L. Thomas	1001 123 E Keith			
T. Lehoullier	#902 123 E. Keith			
Kent Dueck	#309 170 5th St east			Y.
S. Wilson	201-131 E. 3rd St.			Y.

# SIGN IN SHEET,

151 E Keith North Vancouver

March 27, 2019 (5:00 – 7:00 pm)

Name	Address	Email	Phone	Can we contact you with updates about this proposal (Y/N)?
SMITH ST N.	150 Keith Rd E.			
Schneider, S.	140 Keith Rd E.			
GILBERTSON, I	141 E. 6TH ST.			
B GRAVES	140 E Keith Rd			
Wayne Collett	123 Keith Rd East			
EMILY/DEGENWA	155 2nd St. WEST NORTH VAN ↑			YES
Hudson allan Francesco De aeng				YES
May Barr	123 E Keith			
B Clark	123 E. Keith			
Robyn Newton	2104 Grand Blvd.			

# SIGN IN SHEET,

151 E Keith North Vancouver

March 27, 2019 (5:00 – 7:00 pm)

Name	Address	Email	Phone	Can we contact you with updates about this proposal (Y/N)?
KWIN Lamers	141 East 6 <sup>N</sup>			Y
Viire Daniels	#802-123 E. Keith Rd.			Y
Mitchell Hugh & ELOUNNA	#801-123 E. Keith Rd.			Y
Kim Collett	#1501-123 E. Keith Rd.			Y
B. Kellington	1001-140 E. KEITH			N
BILL HERMAN	E. Keith Rd.			
Anne Herman	E. KEITH RD.			
CHUCK HANDY	123 KEITH RD			Y
HELEN PASTUCK	123 E. KEITH RD			
Catrina Remack	66-728 HUNTS W.			



# SIGN IN SHEET,

151 E Keith North Vancouver

March 27, 2019 (5:00 – 7:00 pm)

Name	Address	Email	Phone	Can we contact you with updates about this proposal (Y/N)?
BOB BUNTING	655 E 9TH			
CATHY LEWIS	11 11			
DON PETER	6016 W. QUEEN	—	—	Yes
Hinda Jones	123 Keith Rd E			
Bent Nielsen	1468 St. Andrews St			
Ken Megak	1601 151 Keith Rd E			Y
Hinda Heese	1501 - 140 KEITH RD E			X
Bill Heese	11			
Tanya Miller				
Joy Hayden	267 W. Esplanade			X

## SIGN IN SHEET,

151 E Keith North Vancouver

March 27, 2019 (5:00 – 7:00 pm)

[illegible]







# MEMORANDUM

Date: February 6, 2019  
To: City of North Vancouver, Community Development Department  
cc: Howard Paskowitz, Starlight Investments  
From: Matthew Steyer  
File: 3734.0005.01  
Subject: Statement of Affordability (Updated) – 151 E. Keith Rd., North Vancouver

## Project Overview

IMH 151 E. Keith Rd. Apartments Ltd., in partnership with Starlight Investments, is proposing to build a 40-unit, purpose-built rental infill development at 151 E. Keith Rd. This development, located in the vibrant Lower Lonsdale area, will include a mix of studio and three-bedroom units catering to seniors, young professionals, and young families. Target market rents, unit types, and unit sizes are as follows:

**Table 1 – Total Unit Summary**

Unit Type	Building Type	Number of Units	Square Footage	Target Market Rent <sup>1</sup>
Studio	Townhouse and Apartment	34 (14 Adaptable)	400 sf – 410 sf	\$1,550*
3-Bedroom	Townhouse	6	1033 sf – 1115 sf	\$3,200
<i>*4 studio units will be rented at SAFER subsidy rates (30% below CMHC averages)</i>				

To deepen affordability and support vulnerable seniors, 10% of the new units (4 studios) will be rented at 30% below CMHC averages (at SAFER subsidy rates)<sup>2</sup> in perpetuity, greatly exceeding the City's requirement of 10/10/10 (see Table 2).<sup>3</sup> Due to the central location of the project, 30% below CMHC city-wide averages is equal to approximately 50% below market rents in Lower Lonsdale. These below-market units will be administered through a partnership with Hollyburn Family Services Society, a local non-profit.

**Table 2 – Below-Market Unit Summary**

Below-Market Units	Percentage of Total	Rates	Period
Required	10% (4 units)	10% below CMHC averages	10 years
Proposed	10% (4 units)	30% below CMHC averages	Perpetuity

These 40 new units will complement Starlight's existing 89-unit market rental apartment building, located on the site since 1975. The new units will be constructed on an underutilized portion of the site, on-and-around the existing underground parking structure. This will be made possible by utilizing the City's density bonus program (under the OCP's R-6 designation). In total, the site will contain 129 multi-family rental units. As an infill development proposal, no existing tenants or residents will be displaced as part of this project.

<sup>1</sup> Subject to change; to be aligned with market rents for comparable units in area at time of completion.

<sup>2</sup> The 2018-2019 Shelter Aid for Elderly Renters (SAFER) subsidy rate for singles in Zone 1 is \$803 (BC Housing). In comparison, 2018 CMHC average rents for bachelor units in the City of North Vancouver is \$1,143 (CMHC / City of North Vancouver).

<sup>3</sup> For applications in-stream prior to January 1, 2019.



Date: February 6, 2019  
 File: 3734.0005.01  
 Subject: Statement of Affordability (Updated) – 151 E. Keith Rd., North Vancouver  
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### Alignment with City Goals & Policies

There is an acute need for purpose-built rental housing in North Vancouver, a city with a private apartment vacancy rate of 0.8% in 2018 (down from 1.3% in 2017).<sup>4</sup> To address this situation, the City's recently endorsed Housing Action Plan (HAP) identified six goals, three of which are:<sup>5</sup>

- Increase the diversity of housing to meet the needs of various household types and income levels;
- Address the current local rental housing shortfall and meet anticipated rental demand in the future; and
- Build partnerships with and capacity of non-profit organizations, community partners, the development sector, and housing stakeholders to collectively respond to housing issues in the City of North Vancouver.

Key to meeting these goals will be adding new (and varied) supply to the market, while also retaining existing (older and more affordable) supply. The proposed development at 151 E. Keith Rd. will help the City achieve its goals by:

- Adding 40 new purpose-built rental units to the City's housing supply without displacing existing tenants.
- Providing 10% of new units at 30% below CMHC averages in perpetuity, beyond the 10/10/10 requirement for projects in-stream prior to January 1, 2019.
- Partnering with a local non-profit to support vulnerable seniors, one of the "groups with the greatest challenge" as identified in the HAP.
- Providing 6 new 3-bedroom units (15% of new units), exceeding the HAP recommendation of 10% family-friendly units in all new rental residential developments.
- Retaining 89 older purpose-built rental units (1 studio, 28 2-bedroom, and 60 1-bedroom units) and preserving existing rental supply.

In combination, the 129 units will cater to a range of incomes and family types in a vital, core area of the City, while also helping to fill in two key links along the Housing Continuum:

**Figure 1 – Housing Continuum**



*Source: City of North Vancouver, Housing Action Plan (2016)*

Beyond adding and preserving vital purpose-built market rental housing and partnering with a local non-profit to support vulnerable seniors, the proposed development offers additional affordability benefits to both residents and the City:

- 151 E. Keith Rd. is located in a highly accessible transit area, within close proximity of bus routes (and Future Rapid Transit Service) on Lonsdale Avenue and a variety of car-share options within the neighbourhood and within walking distance of the core commercial areas of Lower and Central Lonsdale.

<sup>4</sup> CMHC, Rental Market Report: Vancouver CMA, 2017 and 2018.

<sup>5</sup> City of North Vancouver, Housing Action Plan, Endorsed October 17, 2016.

MEMORANDUM

Date: February 6, 2019  
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This will significantly reduce yearly costs for future residents who choose to forgo car ownership and utilize active, public, and shared transportation.

- The infill development will better utilize existing residential land and City services while having minimal impact on existing infrastructure due to its modest scale.
- Upgrades to the site's common areas, including new rooftop amenity, will be accessible to existing tenants at no additional cost.
- Starlight has recently installed a new gym and upgraded the communal laundry facilities in the existing building. These new facilities will be accessible to tenants of the new development at no additional cost.
- Starlight will be upgrading the entrance to the existing building so that its now fully accessible and has relocated the elevator in the new building to allow barrier-free access to the parking garage for all tenants, further enhancing the site to ensure long-term livability for current and future residents.

We feel that the proposed development meets many of the City's rental housing and affordability goals and look forward to the opportunity to add much-needed purpose-built rental housing to support this vibrant North Shore community.

Sincerely,

URBAN SYSTEMS LTD.

A handwritten signature in black ink, appearing to read "Matthew Steyer".

Matthew Steyer, M. Pl.  
Community Planner



# SUSTAINABLE DEVELOPMENT GUIDELINES FOR REZONING & DEVELOPMENT PERMIT APPLICATIONS

Staff Use

Case Number \_\_\_\_\_

**CIVIC ADDRESS** 151 East Keith Road, City of North Vancouver Lot:B DL:274 Plan:15049 PID:007-897-481

**APPLICANT NAME** Starlight Investments (Owner) / Urban Systems Ltd. (Applicant)

## I. INTRODUCTION

These Guidelines have been developed to help applicants prepare a successful Development Application submission. All Development Applications must include a response to the Sustainable Development Guidelines, which will be reviewed by Advisory Bodies, staff and, ultimately, City Council who will give serious consideration to the sustainability achievements of a project.

Applicants must demonstrate how their development will contribute to the current and future needs of the community by highlighting sustainability achievements of an application.

The Guidelines challenge applicants to advance the sustainability objectives of the City, as outlined in the 2014 Official Community Plan (OCP), which guides community development in our city with the following Vision:

**In 2031, the City of North Vancouver will be a vibrant, diverse, and highly livable community that is resilient to climate or other changes, and sustainable in its ability to prosper without sacrifice to future generations.**

One of the key ways that the community vision will be realized is through property development. Buildings house us, provide employment centres and frame our streets. They remain with us for many decades with significant ongoing impacts, including generating approximately 50% of our community greenhouse gas emissions. Buildings now need to adapt to the impacts of climate change to help the City become resilient to that new reality. Building forms and densities also have significant effects on housing cost and diversity, transportation choices, and the liveability of our community.



**Sustainability** in the City means balancing the natural, physical (human-made), human, social, cultural and local economic implications of our activities in order to meet the needs of people today without compromising the ability of future generations to meet their own needs.



## II. PREPARING THE SUSTAINABLE DEVELOPMENT GUIDELINES

Applicants are required to submit a response to the Guidelines as a key part of their development application package. Projects are not expected to incorporate all measures in the Guidelines.

For information on underlying City goals and objectives, it is recommended that applicants refer to other relevant City policies such as the OCP, Social Plan, Economic Development Strategy, Transportation Plan, Community Energy and Emissions Plan, Food Strategy and Food Action Plan, as well as others.

The Guidelines address the six capacities that comprise the OCP's Sustainable City Framework, including Natural Systems, Physical Structures / Infrastructure, Local Economy, Human Potential, Social Connections, and Cultural Diversity.

**1. Natural Systems:** The ability of natural systems, both global and local, to support life. Parks and green spaces help regulate the climate, clean and filter water and air, and provide recreational and aesthetic benefits. Maintaining healthy natural systems will reduce strain on municipal infrastructure, support local wildlife and enhance quality of life for community members.



	Y	N	N/A	Please Provide Comments:
<b>LANDSCAPE</b>				
Private Trees Retained or Added in proposal (indicate number of each)	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	2 on-site trees retained, 7 boulevard / offsite trees protected, 50 new trees to be added.
Green Roof / Wall	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Apartment building to have combination of extensive green roof and rooftop amenity space with some urban agriculture.
Majority Native Species Landscaping	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	Combination of native and near native plants.
Habitat Restoration (butterfly, bird-friendly, naturalized areas)	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Plants providing food and shelter for wildlife are included.
Community Gardens*	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	
50% or More Edible Landscaping for Common Space	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	Some urban agriculture planters on apartment building roof.
Water Efficient Irrigation System (drip hose, low-flow nozzles)	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Any irrigation will be high efficiency.
Rainwater Collection (rain barrel)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	
Reuse of Wastewater	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	
<b>HARDSCAPE</b>				
Permeable Paving for Hardscape	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	
40%+ Open Site Space (see Zoning Bylaw definition)	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Over 59% of the full site is open site space.
<b>Other Sustainability Achievements:</b>	Proposed Infill project to be built on top of existing parkade structure, which means no excavation and no greenfield development.			

\*See City of North Vancouver Active Design Guidelines for recommended compliance paths.



**2. Physical Structures/Infrastructure:** The ability to effectively deliver basic services, shelter and physical amenities required to sustain the health and well-being of the community. This includes water supply, sanitary sewer, stormwater drainage, solid waste management, roads, telecommunications, and energy efficiency and conservation including district energy. As well, this category includes attractive streetscapes, durable buildings, provision of a range of housing types and adequate community amenities.



	Y	N	N/A	Please Provide Comments:
<b>HIGH PERFORMANCE CONSTRUCTION</b>				
Durable Building (modular / deconstructable)	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	
Building Reuse / Recycled Content / Use of Repurposed Materials	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Will be using ready available materials with recycling content.
Majority Use of Environmentally Friendly Materials (non-toxic, wood)	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Non-toxic materials for millwork and water-based paints.
Certified by a Third Party Green Building Rating System	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	
<b>ENERGY EFFICIENCY AND HEALTHY BUILDINGS</b>				
<u>Energy Performance (per building type)</u>	Min.	Exceeding	(if so, specify Step)	
Part 3 Commercial (Step 2 min.)	<input type="checkbox"/>	<input type="checkbox"/>		
Part 3 Residential (Step 3 min.)	<input checked="" type="checkbox"/>	<input type="checkbox"/>		
Part 9 Commercial (BCBC min.)	<input type="checkbox"/>	<input type="checkbox"/>		
Part 9 Residential (Step 3 min.)	<input type="checkbox"/>	<input type="checkbox"/>		
Part 9 Residential < 1,200 ft <sup>2</sup> (Step 1 min.)	<input type="checkbox"/>	<input type="checkbox"/>		
Superior Insulation (thick wall exclusion in Zoning Bylaw sought for insulation above BC Building Code)	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	
Airtightness (1.5+ blower door test and appropriate ventilation strategy)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	To be determined.
High-performance Windows e.g. Energy-Star, Passive House Certified (whole project)	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	Double-glazing with argon fill.
Heat Recovery Ventilator (75% or better recovery)	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	TBD by mechanical engineer at building permit stage.
LED Lighting (whole building)	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	Some LED will be used.
Energy-Star Appliances (whole building)	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
Renewable Energy Fixtures Installed	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	TBD by mechanical engineer.
Water Efficient Fixtures (whole building)	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
Greywater Reuse	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	

## TRANSPORTATION

End of Trip Bicycle Infrastructure (beyond Zoning Bylaw requirements)	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/> 62 proposed long-term bicycle parking stalls (60 required). 6 proposed short-term bicycle parking stalls (6 required).
Car-Share Program	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<b>Electric Vehicle Supply Equipment:</b> 20% of all residential parking spaces include an electrical outlet, a receptacle or electric vehicle supply equipment, and are supplied by a branch circuit rated not less than 40A at the nominal voltage of 208 V or 240V as applicable.	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/> Parking garage is existing.
<b>Electric Vehicle Supply Equipment:</b> Adequate space in the electrical room or electrical vault to support future electric vehicle charging for the remaining 80% of parking spaces.	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/> Parking garage is existing.
<b>Other Sustainability Achievements:</b>			

**3. Local Economy:** The ability to maintain and grow a healthy local economy. A strong economy brings employment and a solid tax base to support services without compromising other areas of capacity. A stronger economy has been shown to support healthier lifestyles for community members and greater opportunities for personal fulfillment and overall quality of life.



	Y	N	N/A	Please Provide Comments:
Net New Jobs Generated (long term, full time)	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	Jobs to construct only.
Commercial floor space (net increase, indicate area)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	
Neighbourhood-Scale Commercial (unit frontages ≤6m (20ft))	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	
Non-Market / Lower-End of Market Commercial	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	
Commercial Relocation Strategy	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	
<b>Other Sustainability Achievements:</b>				





**4. Human Potential:** The ability of our local community to support our residents in their pursuit of individual livelihood objectives including access to education, healthy food, active transportation and affordable housing. Meeting these basic needs is essential for the maintenance and growth of human capacity.

	Y	N	N/A	Please provide comments:
Market Rental Housing (net increase, indicate number of units)	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	The project is 100% purpose built rental housing with a total of 40 units.
Non-Market / Lower-End of Market Rental Housing	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	10% (4 units) will be rented at 30% below mid-market rates (SAFER) in perpetuity in partnership with local non-profit.
10%+ Three+ Bedroom Units (in multi-unit residential buildings)	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	15% 3-bedroom units provided.
Micro-units ~37.16m <sup>2</sup> (~400 ft <sup>2</sup> )	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
Childcare Facilities	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	
Community Space for Food Preparation, Storage and Processing	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	
Green Building Educational / Interpretive Features	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	
Primary and Secondary Stair Design*	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
Outdoor Circulation*	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Includes new barrier-free ramp and courtyard spaces.
Storage space for residents in units and storage rooms (multi-unit residential buildings)	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	
<b>Other Sustainability Achievements:</b> Proposed Infill development includes retention of all existing units on-site (89 units, 100% rental) and no displacement of existing tenants.				

\*See City of North Vancouver Active Design Guidelines for recommended compliance paths.



**5. Social Connections:** The ability of our community to foster communication, interaction and networks to respond effectively to community issues. These may include supporting community members with low incomes, lone-parent families, and matters specific to children, youth, seniors and people with disabilities.

	Y	N	N/A	Please provide comments:
Design Features for People with Disabilities (beyond Zoning Bylaw requirement)	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	A new ramp/pathway has been added to provide barrier-free access from Keith Road to the existing tower as well as a new indoor/outdoor elevator which provides accessibility to all parkade levels. Also, 14 Level Two adaptable units (35%) are being proposed.
Communal Cooking Amenities	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	
Indoor Amenity*	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Amenity room provided in new apartment building, and tenants from new rental buildings will have access to amenity spaces in existing tower.
Outdoor Recreation*	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Outdoor garden space provided on roof terrace of Building 1. New ramp/pathway connecting internal courtyard and garden spaces on site.
Amenities for Senior Users	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	
Crime Prevention Through Environmental Design	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
<b>Other Sustainability Achievements:</b> New central garden space and communal terrace space will foster interaction between tenants from the new and existing building on site.				

\*See City of North Vancouver Active Design Guidelines for recommended compliance paths.



**6. Cultural Diversity:** The ability of our community to support and celebrate a diversity of cultural backgrounds. This includes recognition of the traditions of the Squamish Nation and the many cultures of residents who make the City their home. With both tangible and intangible elements, cultural capacity has economic implications and is strongly connected to social traditions. Manifestations of cultural practices can range from spiritual practices to heritage buildings.



	Y	N	N/A	Please provide comments:
Formal and Informal Gathering Spaces	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Courtyard design, landscape terraces and patios.
Retention of Heritage Building	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	
Public Art Reflecting Local Culture	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	
Streetscape Improvements (benches, planters, lighting)	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	New 1.5m grass boulevard, updated site lighting.
<b>Other Sustainability Achievements:</b>				




### III. SUMMARY

The Sustainable Development Guidelines are important in both shaping and processing development applications. Applicants are advised to consider these issues at the outset of a project and to contact planning staff for more information on sustainable design strategies.



Endorsed by Council October 5, 2015



 Division Manager	 Director	 CAO
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The Corporation of **THE CITY OF NORTH VANCOUVER**  
**PLANNING DEPARTMENT**

**REPORT**

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To: Mayor Darrell R. Mussatto and Members of Council

From: David Johnson, Development Planner

SUBJECT: REZONING APPLICATION: 151 EAST KEITH ROAD (URBAN SYSTEMS / IMH 151 E KEITH APARTMENTS LTD)

Date: March 7, 2018 File No: 08-3360-20-0403/1

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*The following is a suggested recommendation only. Please refer to Council Minutes for adopted resolution.*

**RECOMMENDATION:**

**PURSUANT** to the report of the Development Planner, dated March 7, 2018, entitled "Rezoning Application: 151 East Keith Road (Urban Systems / IMH 151 E Keith Apartments LTD)":

**THAT** Zoning Bylaw 1995, No. 6700, Amendment Bylaw, 2017 No. 8618 (Urban Systems / IMH 151 E Keith Apartments LTD, 151 East Keith Road, CD-702), be considered and referred to a Public Hearing;

**THAT** "Housing Agreement Bylaw, 2018, No. 8622" (Urban Systems / IMH 151 E Keith Apartments LTD, 151 East Keith Road, CD-702, Rental Housing Commitments) be considered and referred to a Public Hearing;

**THAT** additional offsite works and community amenities listed in the March 7, 2018 report in the section "Community Amenities and Infrastructure Improvements" be secured, through agreements at the applicant's expense and to the satisfaction of staff, as conditions of rezoning;

**AND THAT** the Mayor and City Clerk be authorized to execute all necessary covenants and legal agreements required to secure the commitments outlined in the report.



## ATTACHMENTS:

1. Context Map (#1623276)
2. Project Fact Sheet (#1623391)
3. Reduced Development Plans (#1625895)
4. Public Consultation Summary (#1626569)
5. Sustainable Development Guideline Checklist (#1627810)
6. Zoning Amendment Bylaw 8618 (#1628354)
7. Housing Agreement Bylaw 8622 (#1626592)
8. Affordability Statement (#1626726)

## PURPOSE:

The purpose of this report is to present for Council's consideration, a Development Application to rezone 151 East Keith Road (Attachment #1) from High Density Apartment Residential 1 (RH-1) to Comprehensive Development 702 (CD-702) to support adding three infill buildings to the existing residential tower. The proposed three infill buildings are to be built on top of the existing two level parkade structure.

## BACKGROUND:

<i>Applicant:</i>	Matthew Steyer – Urban Systems
<i>Architect:</i>	Burrowes Huggins Architects
<i>Official Community Plan Designation:</i>	<b>Residential Level 6 (R6)</b>
<i>Existing Zoning:</i>	<b>High Density Apartment Residential (RH-1)</b>
<i>Applicable Guidelines:</i>	<b>None</b>

## DISCUSSION:

### Site Context and Surrounding Use

The subject site is located in the Lower Lonsdale area along the south side of East Keith Road, across from Victoria Park, between Lonsdale Avenue to the west and St. George's Avenue to the east. It is also a double frontage lot with East 6<sup>th</sup> Street directly to the south (Attachment #1).

The site currently contains a 15 storey residential tower with 89 rental apartment units, which was completed in 1975. The current parking structure surrounding the tower serves to support resident parking. It is mostly covered by landscaping.

The buildings adjacent to the subject site are high density residential buildings, ranging from mid-rise to towers. Across East 6<sup>th</sup> Street to the south are predominately three storey apartment buildings.

A description of the buildings surrounding the subject site is outlined in Table 1 below.

**Table 1. Surrounding Uses**

Direction	Address	Zoning	OCP	Description
North (across the south arm of East Keith Road)	Victoria Park	Public Use and Assembly 1 (P-1)	Parks, Recreation and Open Space	Park Space
South (across East 6 <sup>th</sup> Street)	141 and 165 East 6 <sup>th</sup> Street	Medium Density Apartment Residential 1 (RM-1)	Residential Level 5. Max. 1.6 FSR with 1.0 FSR bonus density.	Two three-storey rental apartment buildings of 54 units each.
East	161-165 East Keith Road, and 160 E 6 <sup>th</sup> Street	Comprehensive Development 651 (CD-651)	Residential Level 6. Max. 2.3 FSR with 1.0 FSR bonus density.	16 storey 93 unit apartment building (currently under construction)
West	123 East Keith Road	Comprehensive Development 155 (CD-155)	Residential Level 6. Max. 2.3 FSR with 1.0 FSR bonus density.	17 storey, 32 unit apartment building.

## Proposal

The applicant is proposing to add, in addition to the existing 89 unit tower, three separate infill buildings to house 40 new rental residential units. These new buildings are to be constructed on top of the existing parking structure. The applicant is not proposing replacing or undertaking any major renovations to the tower, other than upgrades to individual units when they become vacant. As a result, the applicant has stated that no evictions to the existing tower are necessary during construction.

The density of the current tower is 2.29 times the lot area (FSR), just under the Official Community Plan (OCP) maximum of 2.30. With the OCP allowing Council to consider an additional 1.0 Density Bonus to the site, the applicant is proposing to use that additional density to present the 40 unit addition. The three separate buildings are outlined in the submitted drawings shown in Attachment #3.

Building Number One is located at the North West corner of the property and is a four storey rental apartment building containing 31 studio units ranging in size from 37.1 square metres (400 square feet) to 38.2 square metres (412 square feet) in size. The orientation of the building toward East Keith Road provides access to the main lobby. The ground floor units fronting Keith Road will have direct street access from their units. The elevator within this building provides direct access to the underground parkade.



To encourage social interaction amongst the residents as well as encouraging physical activity, this building proposes a number of elements that are outlined in the City's Active Design guidelines, including:

1. A proposed indoor amenity room of 9.7 square metres (104 square feet) in size, is located adjacent to the lobby;
2. To encourage stairwell use, the main stairway is highly visible upon entering the lobby as well as from the exterior of the building from the glazing along the south and east side of the stairway;
3. A secondary stairway also is in a prominent location at the end of the hallway and is glazed for easy visibility; and
4. A rooftop terrace to promote social interaction for all residents of the property.

Building Number Two is a four storey walk-up townhouse building located at the south west corner of the subject site and contains six units. Two of these units are 38 square metre (409 square feet) studio units with direct outdoor access to the interior courtyard between this and Building Number One. Two other units are two level, two bedroom units approximately 96.8 square metres (1,042 square feet) in size, and stacked directly above these studio suites with their own individual access to the same interior courtyard. The final two units are three level, three bedroom units approximately 103.6 square metres (1,115 square feet) in size and directly accessed from the East 6<sup>th</sup> Street side. No rooftop decks are proposed with this building.

Building Number Three proposes three two level, two bedroom units approximately 73.2 square metres (788 square feet) in size and is located at the northeast corner of the property. Each unit is individually accessed from East Keith Road.

### Public Realm

The application seeks to improve the interface between the site and one of the City's most prominent civic spaces and streets. Presently the parking structure and landscaping are directly adjacent to the public realm. This is an uncomfortable interface between the public and private realms. By proposing infill buildings that both are ground related and orient their principal unit entrances to the public realm, the new buildings will significantly improve the experience and views into and out of the public realm.

### Proposed Parking and Access

The application is proposing to use the current parking structure to accommodate the off-street parking for both the proposed 40 units as well as the existing tower. The proposed 82 off-street stalls meets the minimum Zoning Bylaw requirement of 77.

The parkade will also house the required number of bicycle parking for the proposed units as well as the recycling and garbage enclosure. The enclosures for the current tower are contained within that building.

## PLANNING ANALYSIS:

### Supporting Documents

#### *2014 Official Community Plan*

The Official Community Plan identifies the subject site as Residential Level 6 (R6), which permits a residential development of 2.3 times the size of the site (FSR) with the potential of a 1.0 FSR density bonus to be granted at the discretion of Council for an overall density of 3.30 FSR. The OCP also limits the maximum height of a building to be 46 metres (151 feet) tall.

The combined density of the existing tower with the proposed infill buildings would result in a FSR of 3.21 and will not exceed the OCP maximum.

By proposing the potential density bonus for rental housing, this application does meet a key objective of the OCP. Objective 1.5.4 states:

*Prioritize the development and revitalization of affordable rental housing and use density bonusing and density transfers to incentivise the retention, renewal and/or replacement of rental units as a public benefit.*

Other objectives being addressed are listed in the Corporate Plan and/or Policy Implication section at the end of this report.

The proposal as presented will not require an amendment to the OCP.

#### *Zoning Bylaw 1995, No. 6700*

The property is currently zoned High Density Apartment Residential 1 (RH-1). An amendment to the Zoning Bylaw is required to permit the proposal and is reflected in Zoning Amendment Bylaw 8618 (Attachment #6). Additionally, current zoning requirements are in the Project Fact Sheet (Attachment #2).

Using the RH-1 Zone as a base, the proposal is in conformance with the Zoning Bylaw with the exceptions to permit a density of 3.21 FSR, to allow a minimum parking stall width of 2.44 metres (8.0 feet), and setbacks to the proposed buildings as outlined in the following table:



**Table 2. Proposed Building Setbacks**

Building	Setback	Distance
Northeast Building	North setback	2.45 m (8.0 ft.)
	West setback	3.04 m (10.0 ft.)
		1.65 m (5.41 ft.) for stairwell
Southwest Building	South setback	1.63 m (5.33 ft.)
	West setback	7.06 m (23.17 ft.)
Northeast Building	North setback	2.40 m (7.83 ft.)
	East setback	1.97 m (6.5 ft.)

The proposed setback variances are to support the proposed building locations within a property whose current zoning supports higher building setback distances for high-rise buildings. The current setback requirement of 7.62 metres (25 feet) from all property lines would not support infill structures in addition to an existing tower.

The proposed parking variance is due to the applicant proposing to use the existing parking structure. At the time it was built, the required parking stall width was 2.44 metres (8 feet). This would affect the spacing of the structural columns and the structural design of the parkade. In the past two years, the Zoning Bylaw was amended to increase the width to 2.5 metres (8.2 feet). For the applicant to achieve their proposed number of parking stalls, the variance is necessary.

The applicant has always presented this proposal, along with the existing tower, as a long-term investment for them to provide rental dwelling units. To secure this initiative, the proposed Comprehensive Development Zone in Amendment Bylaw 8618 limits the use to Rental Apartment Residential.

### *Housing Action Plan*

To achieve a 0.91 FSR Density Bonus for this proposal, the City's Density Bonus and Community Benefits Policy as well as the City's Housing Action Plan would require that 10% of the new units be rented at 10% below market rates for a period of 10 years (Mid-Market units). As the existing tower is not being replaced or undergoing a major renovation, the 89 current units are not included with the Mid-Market unit count.

To meet the requirement, the applicant is partnering with Hollyburn Family Services to provide four studio units (10% of the total number of new units) at rates less than what the City's Mid-Market units could achieve by being able to tap into the SAFER subsidy to rent to vulnerable seniors. The applicant has updated their Affordable Housing statement (Attachment #8) to outline this proposal, and is reflected in the Housing Agreement (Attachment #7), which will secure these rates for a period of 10 years. It should be noted that rents would revert to mid-market rental rates if the contract with Hollyburn Family Services terminates within the 10 year period.

The updated Affordability Statement outlines the rate difference between the City's Mid-Market units and what the SAFER subsidy will be able to provide using 2018 numbers. The chart below outlines the difference between the two rates as well as market rates.

	July 2017 market rents*	2017 mid- market rents	Percentage difference	SAFER subsidized rates	Percentage difference from market
Studio	\$1,500	\$916	39% below	\$765	49% below

\* Current market rents were obtained from a new market rental development in Central Lonsdale, advertised on Craigslist, in July 2017

To address the Family-Friendly Housing Section of the Housing Action Plan, the applicant is proposing two of the 40 new units be three bedroom units as well as five, two bedroom units to give families unit type options. The existing tower contains primarily one and two bedroom units.

### **Community Amenities/Infrastructure**

The City's *Density Bonus and Community Benefits Policy* (Policy), in conjunction with the Official Community Plan, allows Council to consider a density bonus beyond the OCP limit of 2.30 FSR, up to a maximum of 3.30 FSR in the Residential Level 6 area.

The policy provides a number of community benefit options for projects seeking additional density and seeks to ensure the City receives value for additional density granted. The value could be in the form of a cash contribution or some form of amenity, such as an all rental apartment building to be secured in perpetuity plus a form of affordability in accordance with the City's Housing Action Plan. In this case, to achieve the desired 0.91 FSR bonus density for the new infill buildings, the applicant has agreed to secure all the proposed 40 unit addition as rental as well as partnering with Hollyburn Family Services to secure four units using the SAFER rental rate for vulnerable seniors, for a period of 10 years. As this would exceed what the City would achieve through acquiring the same number of units through the Housing Action Plan, staff is satisfied that the intent of the Policy has been met and that no community amenity cash contribution is required for this development proposal.

For comparison, a cash contribution in accordance with the Policy or this density bonus would be valued at approximately \$3.69 million, as outlined in Table 3 below. The City would receive rental housing in lieu of a cash contribution.

**Table 3. Estimated Value of Community Benefits through Density Bonusing**

Density Value Calculation	Value
Density Addition from 2.29 FSR to 2.30 FSR / OCP Density (@ \$20 / sq. ft.)	\$5,600
Density Bonus to 0.91 FSR / OCP Density Bonus (@ \$140 / sq. ft.)	\$3,684,660
<b>Total Value of Community Benefits</b>	<b>\$3,690,260</b>



Staff also seeks Council's endorsement for the item listed below. This item is not specifically required under the authority of City bylaws, but could be made a requirement of rezoning under the authority of Council.

- A financial contribution of \$25,000 from the applicant to help finance a public Electric Vehicle Charging Station within the City. This contribution is payable prior to adoption of rezoning.

## **CONSULTATION:**

### *Advisory Design Panel*

The application appeared before the Advisory Design Panel on February 15, 2017 where the Panel supported the application subject to the applicant addressing the following items to the satisfaction of the Development Planner:

- Wall articulation, especially the west side of Building 1;
- Consideration of extending Building 4 for additional bike storage and plug-ins for electric bikes and scooters;
- Review of the east end of Building 1 to improve visual permeability through the site;
- Review adjusting the north south pathway to increase the separation along Building 2; and
- Roof top treatments to all infill buildings.

Staff worked with the applicant to resolve the appearance of the west side of the building as well as removing Building 4 that was to be a storage building for bicycles and garbage and recycling on the south end of the site fronting the existing tower. Its removal has been addressed using the existing parkade. The pathway and sightlines through the site have also been improved.

### *Public Consultation*

The applicant held a Developer Information Session (DIS) on April 26, 2017 where 32 members of the community attended. At the meeting, a total of eight sign-in comment sheets were submitted. Many more submissions were received by the City after the DIS. The applicant did provide a report of the meeting that includes comments provided on and after the meeting.

The feedback received (Attachment #4) showed that most were opposed to the application for the following reasons:

- The building setbacks from the property line should be maintained and not be varied;
- Lack of off-street parking;
- Increased traffic in the neighbourhood; and

- Low to mid-rise building is not in character with the neighbourhood or the streetscape.

Those who supported the application liked that there would be more rental units, and the overall appearance of the buildings.

The applicant has reached out to the community to address the concerns and did reduce the scale of the proposal by removing three units in Building 2 to open up the south western part of the site and to protect views.

Should Council grant first and second reading to proposed Bylaws 8618 and 8622, a Public Hearing will be scheduled.

### **SUSTAINABILITY COMMENTS:**

The subject site is one half-block from Lonsdale Avenue and has good access to public transit. Victoria Park (which contains a portion of the Green Necklace) is across the street. The proposed development would be using unused portions of the site on top of an existing parkade while maintaining enough space for landscaping.

The applicant will also be:

- Connecting the infill buildings to LEC and providing the infrastructure for the future connection of the existing residential tower to LEC;
- Contribution towards an Electric Vehicle Charging Station in the City;
- Using high-performance windows in the building envelope;
- Using energy saving certified appliances; and
- Installing low consumption water fixtures.

The applicant has filled out the Sustainable Development Guideline Checklist as part of their application to identify their intent on achieving the City's sustainability goals. These are shown in Attachment #5.

### **FINANCIAL IMPLICATIONS:**

A financial contribution of \$25,000 towards the City's Community Amenity Reserve Fund towards the installation of a public Electric Charging Station in the City.

A cash contribution for additional density are not required for rental property as noted in the Community Amenities/Infrastructure section above.

### **INTER-DEPARTMENTAL IMPLICATIONS:**

The Civic Projects Team and Directors Team reviewed and endorsed this report at their joint meeting on March 6, 2018.



## CORPORATE PLAN AND/OR POLICY IMPLICATIONS:

The City's Official Community Plan is centered on a set of guiding principles and a sustainable city framework. Proceeding with the proposed zoning and redevelopment of the subject site will advance many of these goals and objective particularly the following:

- 1.1.1 Plan for growth in the City's population, dwelling units and employment in keeping with the projections in Metro Vancouver's regional Growth Strategy;
- 1.3.1 Ensure that new development is compatible with the established urban form of the City, reflecting the primacy of the Lonsdale Regional City Centre and the transition through mid- and low rise buildings to lower density neighbourhoods;
- 1.3.5 Encourage design excellence in developments through carefully considered, high quality architecture and landscaping, with varied designs which are interesting, sensitive and reflective of their surroundings;
- 1.3.10 Encourage active, healthy lifestyles and the opportunity for more social connections through planning and active design principals that encourage physical activity and contribute to enhanced walkability and active streets, sidewalks and public spaces;
- 1.5.4 Prioritize the development and revitalization of affordable rental housing and use density bonusing and use density bonusing and density transfers to incentivise the retention, renewal and/or replacement of rental units as a public benefit;

RESPECTFULLY SUBMITTED:

  
David Johnson  
Development Planner

Attachments

DJ:eb



# PROPOSED RESIDENTIAL INFILL DEVELOPMENT

151 EAST KEITH ROAD  
NORTH VANCOUVER B.C.



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REISSUED FOR REZONING APPLICATION - FEBRUARY 27th, 2018 (Original Submission October 25th, 2017)

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Burrowes Huggins Architects

**SUPERSEDED**

## PROJECT TEAM

## DEVELOPER - STARLIGHT INVESTMENTS

1400-3280 BLOOR STREET WEST, CENTRE TOWER  
TORONTO ONTARIO M8X 2X3

Founded in 2011, Starlight Investment Ltd. is a real estate asset management firm with a primary focus on multifamily rental properties in over 400 properties across Canada and the Southern United States. Starlight's innovative approach to the properties under its management is reflected in its commitment to excellence in quality design and construction for both existing assets and new infill development projects alike.

# ARCHITECT

BURROWES HUGGINS ARCHITECTS  
205-1628 WEST 1ST AVE.  
VANCOUVER BC V6J 1G1

Burrowes Huggins Architects was founded in 1991 and since that time has been actively involved in a variety of developments, and worked with many local and regional development companies. BHA completed work includes a number and variety of Whistler buildings including the Four Seasons Resort and Residences, the Athlete's Lodge in Cheakamus Village and a mix of resort condominiums in the Blackcomb Benchlands.

Burrowes Huggins Architects has longstanding experience in the Lower Mainland with mixed-use and ground-oriented housing projects including street-oriented townhouses, stacked townhouses, 4 to 6 storey mixed-use retail residential projects, purpose-built rental housing and not for profit housing projects.

# LANDSCAPE ARCHITECT

CONNECT LANDSCAPE ARCHITECTURE  
2305 HEMLOCK STREET  
VANCOUVER BC V6H 2V1

Connect Landscape Architecture (formerly Sharp & Diamond) is a recognized leader in the design and transformation of healthy vibrant communities. Our vision is simple - Connect people to their environment through meaningful design. Connect Landscape Architecture draws inspiration from the natural landscape while embracing built form and infrastructure.

## PLANNING CONSULTANT

URBAN SYSTEMS  
550-1090 HOMER STREET  
VANCOUVER BC V6B 2W9

Urban Systems is a multi-disciplinary consulting firm founded in 1975. At our 14 offices across 5 provinces, we bring spirit in service for vibrant communities and have been recognized as one of the Best Workplaces in Canada for 12 years running. With over forty years of land development experience, we assist clients in moving through the different stages of development as quickly and efficiently as possible. Urban Systems promotes sustainable development methods that add efficiency, improve performance, and provide marketing value to development projects. Our integrated approach to land development has resulted in some of the most celebrated development projects in Western Canada. We provide the full range of services required to take projects from start to finish, including land use planning and design, civil engineering, landscape architecture, legal survey, and construction services. We assemble a customized team of professionals for every new assignment to design and construct all aspects of each unique site.

[illegible]

issues:	
2016-11-22	Issued for Rezoning
2017-10-25	Reissued for Rezoning

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**Burrowes Huggins Architects**  
Suite 205 - 1628 West 1st Avenue, Vancouver B.C.  
Telephone: (604) 730-8100 | Web: [www.bha.ca](http://www.bha.ca)

**project:**

Proposed Residential Infill  
151 East Keith  
North Vancouver BC



consultant:

soak

drawn: MCH/PDH	date: September 2016
checked	
scale: as noted	project no.: 424

sheet title:  
PROJECT DESIGN TEAM

sheet no.:  
**A0.01**  
REV.:  
plotted: 17-10-24 11:30 14 AM

plotted: 17-10-24 11:30:14 AM

**SUPERSEDED**

PROJECT STATISTICS

Project Name:	RESIDENTIAL INFILL DEVELOPMENT										SF	SM	ACRES	HA	Units Per Acre	Units Per HA	Date:	9 Jan 17																																
Project Number:	P424										27997.90 sf	2401.09 sm	0.84 acres	0.260 ha	n/a	n/a	Revised:	22 Feb 18																																
Client:	STARLIGHT INVESTMENTS										Gross Site Area:				Amenity Provided:																																			
TOTAL FSR SUMMARY (ALL NEW BUILDINGS)											Permitted FSR				Permitted FSR				Actual SF				Actual SM				Actual FSR				Required				Site Area				27997.9 sf											
TOTAL GFA											37908 sf				1.00 ftr				25582 sf				2376.64 sm				25582 sf				166 sf				11.47 sm				Existing Footprint				4452 sf				15.90%			
Storage															0 sf				0.00 sm																New Footprint				7179 sf				25.64%							
Total FSR															25582 sf				2376.64 sm				0.91 ftr												Total Coverage				41.54%											
BUILDING ONE AREAS (APARTMENT BUILDING)																																																		
LEVEL				RES LOBBY		CIRCULATION		RESIDENTIAL		AMENITY		GFA		EFF		NO UNITS		STORAGE		FSR				GFA CHECK																										
1		(NOTE: PROJECT LEVEL 3 ON FLOOR PLANS)		104 sf		904 sf		2836 sf		166 sf		4000 sf		79.66 %		7		0 sf		3894 sf				3999 sf																										
2		(NOTE: PROJECT LEVEL 4 ON FLOOR PLANS)		0 sf		868 sf		3220 sf		0 sf		4088 sf		78.76 %		8		0 sf		4088 sf				4088 sf																										
3		(NOTE: PROJECT LEVEL 5 ON FLOOR PLANS)		0 sf		870 sf		3220 sf		0 sf		4090 sf		78.72 %		8		0 sf		4090 sf				4088 sf																										
4		(NOTE: PROJECT LEVEL 6 ON FLOOR PLANS)		0 sf		868 sf		3220 sf		0 sf		4088 sf		78.76 %		8		0 sf		4088 sf				4088 sf																										
5		(NOTE: PROJECT LEVEL 7 ON FLOOR PLANS)		0 sf		468 sf		0 sf		0 sf		468 sf		0.00 %		0		0 sf		468 sf				468 sf																										
TOTAL				104 sf		3978 sf		12485 sf		166 sf		16734 sf		74.61 %		31 units		0 sf		16588 sf		in amenities and storage		16731 sf																										
				9.66 sm		369.60 sm		1159.92 sm		15.42 sm		1554.41 sm						0.00 sm		1339.18 sm				1554.35 sm																										
BUILDING TWO AREAS (TOWNHOUSE BUILDING)																																																		
TOWNHOUSE UNIT TYPE		UNIT NO		UNIT TYPE		FLOOR		LOWER		MID		UPPER		TOTAL		NO UNITS		TOTAL GFA		STORAGE		TOTAL FSR																												
A3		T24		STUDIO TOWNHOUSE		409 sf		0 sf		0 sf		409 sf		37.99 sm		1 units		409 sf		37.99 sm		0 sf																												
A4		T28		STUDIO TOWNHOUSE		409 sf		0 sf		0 sf		409 sf		37.99 sm		1 units		409 sf		37.99 sm		0 sf																												
B4		T25		2 STOREY TOWNHOUSE (2 BR)		408 sf		0 sf		0 sf		408 sf		36.84 sm		1 units		408 sf		36.84 sm		0 sf																												
B5		T26		2 STOREY TOWNHOUSE (2 BR)		408 sf		0 sf		0 sf		408 sf		36.84 sm		1 units		408 sf		36.84 sm		0 sf																												
C1		T21		3 STOREY TOWNHOUSE (3 BR)		299 sf		408 sf		408 sf		1115 sf		103.59 sm		1 units		1115 sf		103.59 sm		0 sf																												
C2		T22		3 STOREY TOWNHOUSE (3 BR)		299 sf		408 sf		408 sf		1115 sf		103.59 sm		1 units		1115 sf		103.59 sm		0 sf																												
TOTAL																6 units		6657 sf				6657 sf																												
																		618.43 sm				618.43 sm																												
BUILDING THREE AREAS (TOWNHOUSE BUILDING)																																																		
TOWNHOUSE UNIT TYPE		UNIT NO		UNIT TYPE		FLOOR		LOWER		MID		UPPER		TOTAL		NO UNITS		TOTAL GFA		STORAGE		TOTAL FSR																												
B1		T28		2 STOREY TOWNHOUSE (2 BR)		393 sf		0 sf		0 sf		393 sf		73.24 sm		1 units		788 sf		73.24 sm		0 sf																												
B2		T27		2 STOREY TOWNHOUSE (2 BR)		394 sf		0 sf		0 sf		394 sf		73.24 sm		1 units		788 sf		73.24 sm		0 sf																												
B3		T29		2 STOREY TOWNHOUSE (2 BR)		394 sf		0 sf		0 sf		394 sf		73.20 sm		1 units		788 sf		73.20 sm		0 sf																												
TOTALS																3 units		2358 sf				2358 sf																												
																		219.02 sm				219.02 sm																												
PROJECT SUMMARY TOTALS																																																		
NEW INFILL PROJECT TOTALS															40 units								25582 sf FSR				2376.64 sm				0.91 ftr																			
EXISTING BUILDING PROJECT TOTALS															89 units								64185 sf FSR				5962.98 sm				2.29 ftr																			
TOTALS FOR NEW INFILL AND EXISTING															129 units								89767 sf FSR				8339.62 sm				3.21 ftr																			

UNIT SUMMARY BUILDING 1

BUILDING 1 STRATA LOT UNIT SUMMARY											
Floor	Unit #	Type	Area (sf)	Area (sm)	Storage (sf)	Storage (sm)	Net Area (sf)	Net Area (sm)	Balcony (sf)	Balcony (sm)	
1	101	A2	412 sf	38.33 sm	0 sf	0.00 sm	412 sf	38.33 sm	0 sf	0.00 sm	
1	102	A1	404 sf	37.53 sm	0 sf	0.00 sm	404 sf	37.53 sm	0 sf	0.00 sm	
1	103	A1	400 sf	37.21 sm	0 sf	0.00 sm	400 sf	37.21 sm	0 sf	0.00 sm	
1	104	A2	404 sf	37.53 sm	0 sf	0.00 sm	404 sf	37.53 sm	0 sf	0.00 sm	
1	105	D1	404 sf	37.53 sm	0 sf	0.00 sm	404 sf	37.53 sm	0 sf	0.00 sm	
1	106	D1 (ADAPTABLE)	400 sf	37.21 sm	0 sf	0.00 sm	400 sf	37.21 sm	0 sf	0.00 sm	
1	107	D1 (ADAPTABLE)	402 sf	37.33 sm	0 sf	0.00 sm	402 sf	37.33 sm	0 sf	0.00 sm	
Level 1 Totals:			2836 sf	262.68 sm	0 sf	0.00 sm	2836 sf	262.68 sm	0 sf	0.00 sm	
Floor	Unit #	Type	Area (sf)	Area (sm)	Storage (sf)	Storage (sm)	Net Area (sf)	Net Area (sm)	Balcony (sf)	Balcony (sm)	
2	201	D2	407 sf	37.78 sm	0 sf	0.00 sm	407 sf	37.78 sm	0 sf	0.00 sm	
2	202	D1 (ADAPTABLE)	400 sf	37.21 sm	0 sf	0.00 sm	400 sf	37.21 sm	0 sf	0.00 sm	
2	203	D1	400 sf	37.21 sm	0 sf	0.00 sm	400 sf	37.21 sm	0 sf	0.00 sm	
2	204	D1	400 sf	37.21 sm	0 sf	0.00 sm	400 sf	37.21 sm	0 sf	0.00 sm	
2	205	D2	404 sf	37.53 sm	0 sf	0.00 sm	404 sf	37.53 sm	0 sf	0.00 sm	
2	206	D2	404 sf	37.53 sm	0 sf	0.00 sm	404 sf	37.53 sm	0 sf	0.00 sm	
2	207	D1 (ADAPTABLE)	400 sf	37.21 sm	0 sf	0.00 sm	400 sf	37.21 sm	0 sf	0.00 sm	
2	208	D1 (ADAPTABLE)	404 sf	37.53 sm	0 sf	0.00 sm	404 sf	37.53 sm	0 sf	0.00 sm	
Level 2 Totals:			5220 sf	499.22 sm	0 sf	0.00 sm	5220 sf	499.22 sm	0 sf	0.00 sm	
Floor	Unit #	Type	Area (sf)	Area (sm)	Storage (sf)	Storage (sm)	Net Area (sf)	Net Area (sm)	Balcony (sf)	Balcony (sm)	
3	301	D2	407 sf	37.78 sm	0 sf	0.00 sm	407 sf	37.78 sm	0 sf	0.00 sm	
3	302	D1 (ADAPTABLE)	400 sf	37.21 sm	0 sf	0.00 sm	400 sf	37.21 sm	0 sf	0.00 sm	
3	303	D1	400 sf	37.21 sm	0 sf	0.00 sm	400 sf	37.21 sm	0 sf	0.00 sm	
3	304	D1	400 sf	37.21 sm	0 sf	0.00 sm	400 sf	37.21 sm	0 sf	0.00 sm	
3	305	D2	404 sf	37.53 sm	0 sf	0.00 sm	404 sf	37.53 sm	0 sf	0.00 sm	
3	306	D2	404 sf	37.53 sm	0 sf	0.00 sm	404 sf	37.53 sm	0 sf	0.00 sm	
3	307	D1 (ADAPTABLE)	400 sf	37.21 sm	0 sf	0.00 sm	400 sf	37.21 sm	0 sf	0.00 sm	
3	308	D1 (ADAPTABLE)	404 sf	37.53 sm	0 sf	0.00 sm	404 sf	37.53 sm	0 sf	0.00 sm	
Level 3 Totals:			3220 sf	299.22 sm	0 sf	0.00 sm	3220 sf	299.22 sm	0 sf	0.00 sm	
Floor	Unit #	Type	Area (sf)	Area (sm)	Storage (sf)	Storage (sm)	Net Area (sf)	Net Area (sm)	Balcony (sf)	Balcony (sm)	
4	401	D2	407 sf	37.78 sm	0 sf	0.00 sm	407 sf	37.78 sm	0 sf	0.00 sm	
4	402	D1 (ADAPTABLE)	400 sf	37.21 sm	0 sf	0.00 sm	400 sf	37.21 sm	0 sf	0.00 sm	
4	403	D1	400 sf	37.21 sm	0 sf	0.00 sm	400 sf	37.21 sm	0 sf	0.00 sm	
4	404	D1	400 sf	37.21 sm	0 sf	0.00 sm	400 sf	37.21 sm	0 sf	0.00 sm	
4	405	D2	404 sf	37.53 sm	0 sf	0.00 sm	404 sf	37.53 sm	0 sf	0.00 sm	
4	406	D2	404 sf	37.53 sm	0 sf	0.00 sm	404 sf	37.53 sm	0 sf	0.00 sm	
4	407	D1 (ADAPTABLE)	400 sf	37.21 sm	0 sf	0.00 sm	400 sf	37.21 sm	0 sf	0.00 sm	
4	408	D1 (ADAPTABLE)	404 sf	37.53 sm	0 sf	0.00 sm	404 sf	37.53 sm	0 sf	0.00 sm	
Level 4 Totals:			3220 sf	299.22 sm	0 sf	0.00 sm	3220 sf	299.22 sm	0 sf	0.00 sm	
Overall Totals:			12485 sf	1160.34 sm	0 sf	0.00 sm	12485 sf	1160.34 sm	0 sf	0.00 sm	

ZONING SUMMARY

ZONING SUMMARY - INFILL DEVELOPMENT				PROJECT		KEITH ROAD RESIDENTIAL INFILL DEVELOPMENT	
PROJECT NO.	P424	IMPERIAL	METRIC	VALUE	NOTES		
1 CIVIC ADDRESS:							
151 East Keith Road, City of North Vancouver		LOT: B	BLOCK: 114	DL: 274	PLAN: 15049		
PID: 007-497-481							
2 APPLICATION DESCRIPTION:							
To construct a residential infill development consisting of 3 buildings and 40 rental units.							
3 LOT AREA							
EXISTING		27997.90 sf		2601.09 sm			
PROPOSED		27997.90 sf		2601.09 sm			
4 ZONING							
EXISTING		RH-1					
PROPOSED		CD					
5 OFFICIAL COMMUNITY PLAN (OCP):							
As per the OCP, the site is Residential Level 6 (High Density) - refer to Schedule A Land Use Map.							
Existing OCP Density (FSR) = 2.3 with a Maximum Bonus (FSR) of up to 1.0. 2.3 + 1.0 = 3.3 Total Permitted FSR							
6 GROSS FLOOR AREA							
PERMITTED: 1.0 Bonus FSR		27998.00 sf		2601.09 sm			
PROPOSED: 0.91 FSR		25582.00 sf		2376.65 sm	= 0.91 FSR		
7 RESIDENTIAL UNIT MIX							
STUDIO APARTMENTS: 31 units (includes 11 Adaptable Level 2 Units)							
TOWNHOUSE UNITS: 9 units (includes 2 Studio Townhouses and 2 3-Storey Townhouses)							
TOTAL: 40 Units							
8 VEHICLE PARKING							
Refer to Parking Statistics							
9 BICYCLE PARKING							
		REQUIRED	PROPOSED				
LONG TERM		40 (40 units x 1.5)	42.00				
SHORT TERM		6.00	6.00				

PARKING STATISTICS

		DATE REVISED	27-Jul-18 24-Oct-17
TOTAL PARKING PROVIDED BY EXISTING BUILDING			
LEVEL 1	EXISTING REGULAR STALLS	H/C STALLS	TOTAL
LEVEL P1	51 stalls	0 stalls	51 stalls
	53 stalls	0 stalls	53 stalls
TOTAL	104 stalls	0 stalls	104 stalls
TOTAL PARKING PROVIDED BY MODIFICATIONS			
LEVEL 1	EXISTING REGULAR STALLS	H/C STALLS	SMALL STALLS
LEVEL P1	38 stalls	2 stalls	0 stalls
	40 stalls	2 stalls	0 stalls
TOTAL			82 stalls
<hr/>			
SUMMARY OF UNITS/PARKING			
<hr/>			
EXISTING BUILDING			
2 BEDROOM UNITS	28 units		
1 BEDROOM UNITS	60 units		
STUDIO	1 units		
TOTAL UNITS EXISTING BUILDING	89 units		
NEW BUILDING			
STUDIO APARTMENTS	34 units		
TOWNHOUSE UNITS	6 units		
TOTAL UNITS NEW BUILDING	40 units		
TOTAL UNITS EXISTING AND NEW	129 units		
TOTAL PARKING PROVIDED	82 stalls	0.64 stalls per unit	

SUPERSEDED

revisions:

no.	date	description

issues:

2016-11-22	Issued for Reasoning
2017-01-09	Reissued for Reasoning
2017-10-25	Reissued for Reasoning

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Suite 205 - 162







revisions:		
no.	date	description

issues:  
2016-11-22 Issued for Rezoning  
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
**project:**

Proposed Residential Infill  
151 East Keith  
North Vancouver BC



consultant:

seal:



**AREA PLAN**  
Scale: 1"=100'-0"



NORTH

drawn: MCH/PDH	date: June 2016
checked	
scale: as noted	project no.: 424
sheet title: AREA PLAN	

sheet no.:  
**A1.03**

rev.:

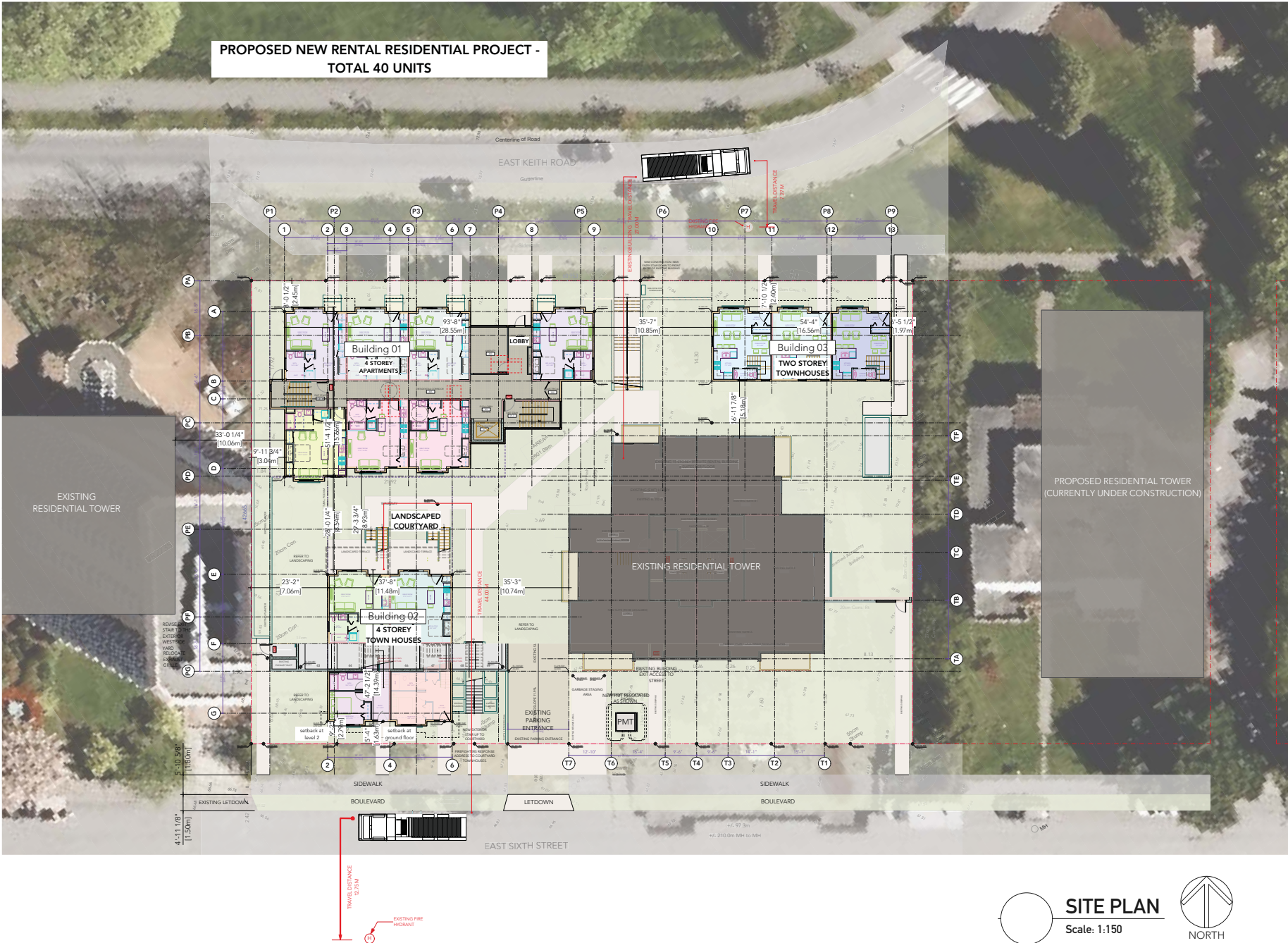
plotted: 17-10-24 11:30:46 AM

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**SUPERSEDED**





PROPOSED NEW RENTAL RESIDENTIAL PROJECT -  
TOTAL 40 UNITS

revisions:		
no.	date	description

issues:		
2016-11-22	Issued for Rezoning	
2017-01-09	Reissued for Rezoning	
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**project:**  
Proposed Residential Infill  
151 East Keith  
North Vancouver BC  
**Starlight Investments**

**consultant:**

**seal:**

drawn: MCH/DPH	date: September 2016
checked:	
scale: as noted	project no.: 424

**sheet title:**  
SITE PLAN

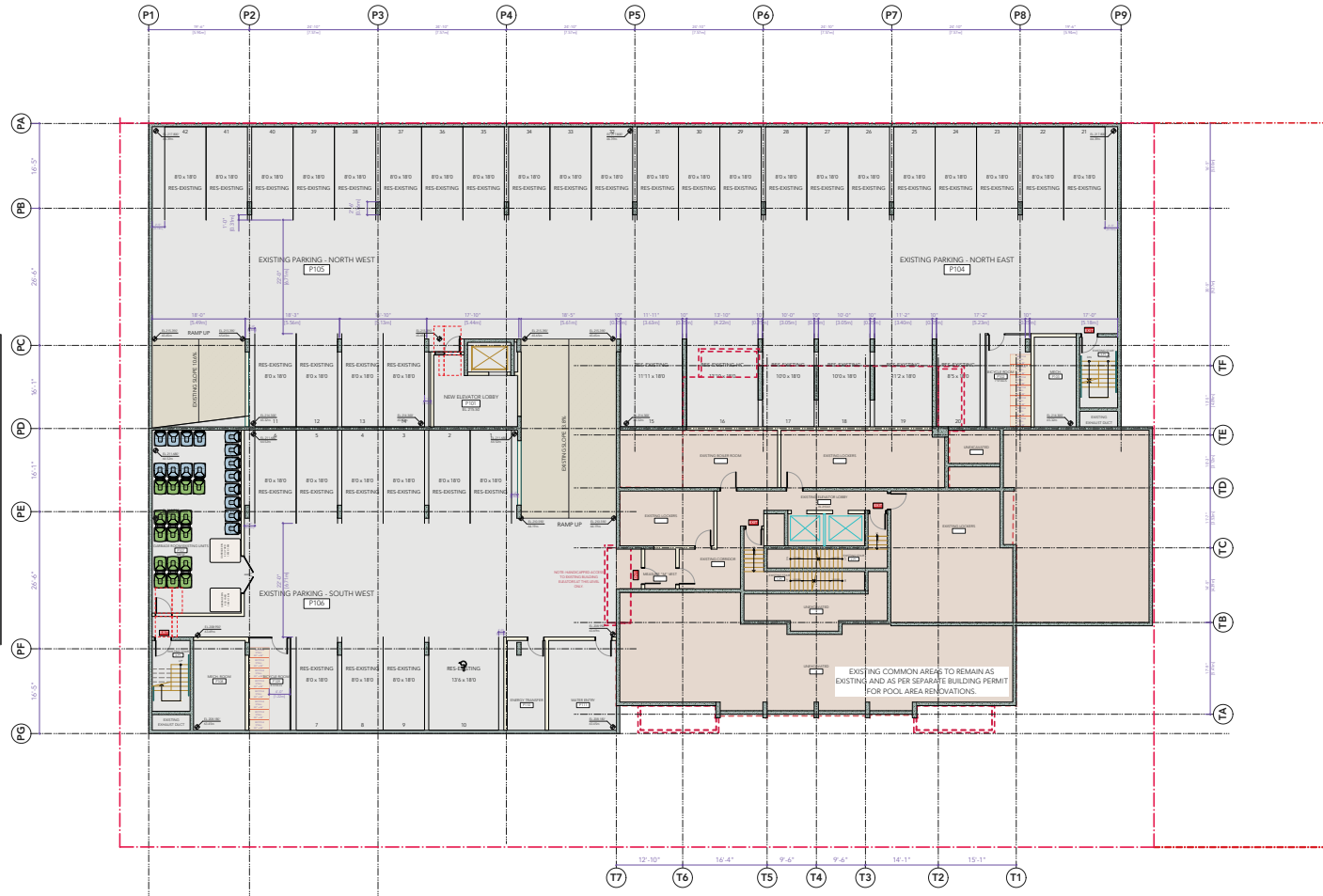
sheet no.:  
**A1.04**  
rev.:  
plotted: 17-10-24 11:30:52 AM

P424 plans.vwx

**SUPERSEDED**



EXISTING  
RESIDENTIAL TOWER



LEVEL P1 PLAN  
Scale: 3/32" = 1'-0"

SUPERSEDED

revisions:		
no.	date	description

issues:	
2016-11-22	Issued for Reasoning
2017-10-25	Reissued for Reasoning

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project:  
Proposed Residential Infill  
151 East Keith  
North Vancouver BC  
Starlight  
immolements

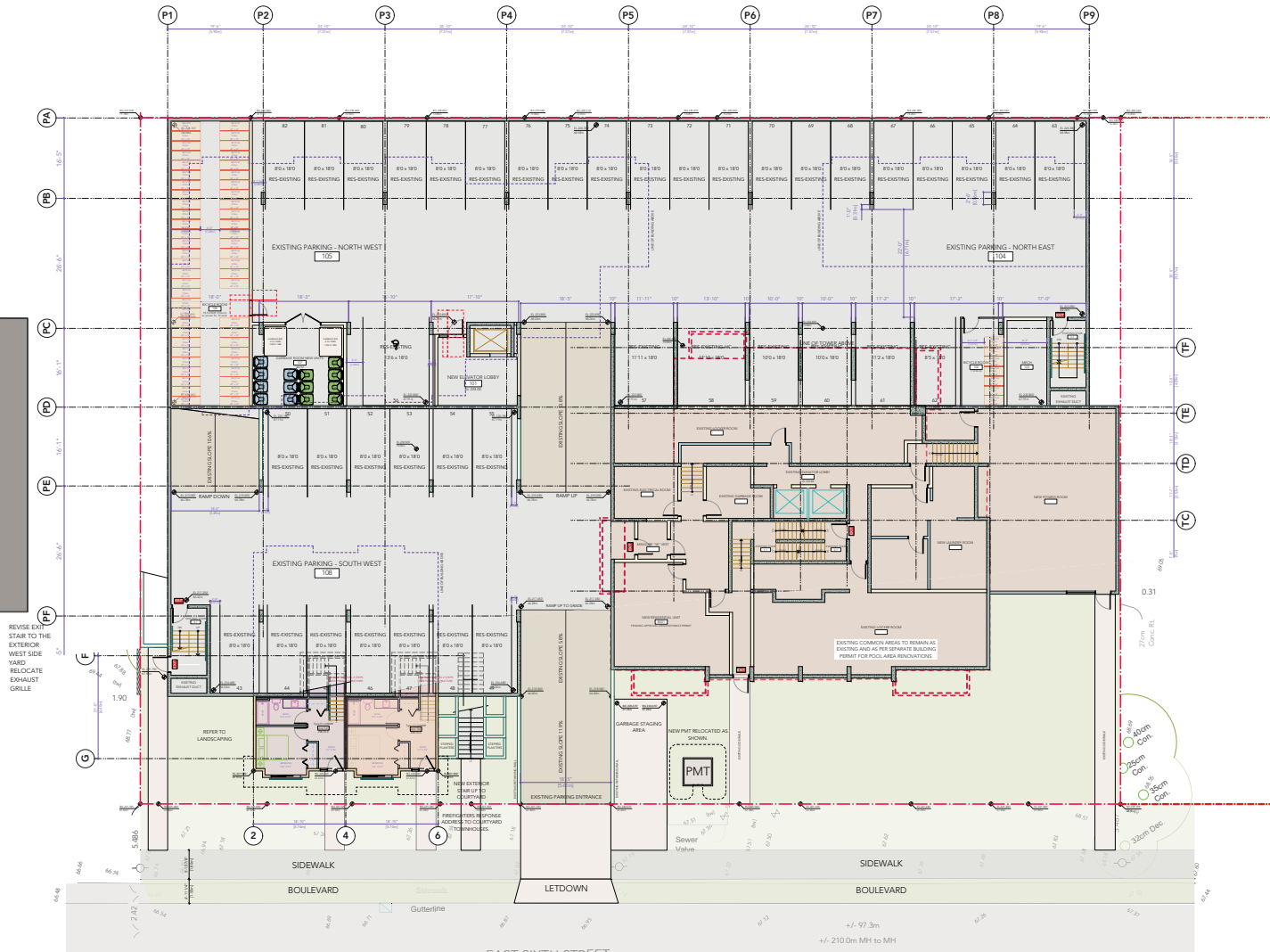
consultant:

seal:

drawn: MCH/POH	date: May 2015
checked	
scale: as noted	project no.: 424
sheet title: PARKING P1 PLAN	

sheet no.: A2.01	plotted: 17-10-24 11:32:02 AM
rev.:	

EXISTING  
RESIDENTIAL TOWER



SUPERSEDED

GROUND FLOOR PLAN  
Scale: 3/32" = 1'-0"

revisions:		
no.	date	description

issues:		
2016-11-22	Issued for Reasoning	
2017-01-09	Reissued for Reasoning	
2017-10-25	Reissued for Reasoning	

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**project:**  
Proposed Residential Infill  
151 East Keith  
North Vancouver BC  
**Starlight**  
Investments

**consultant:**

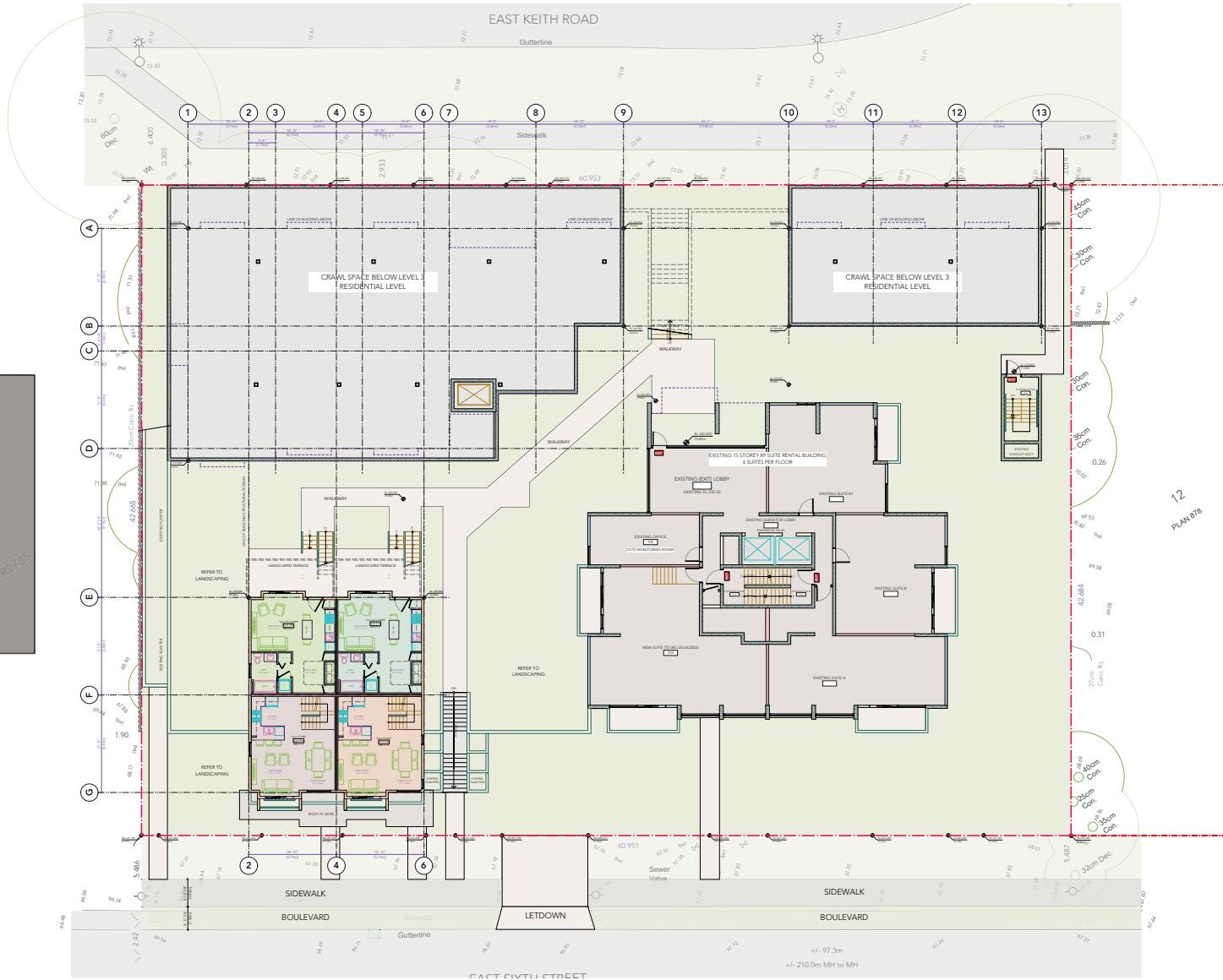
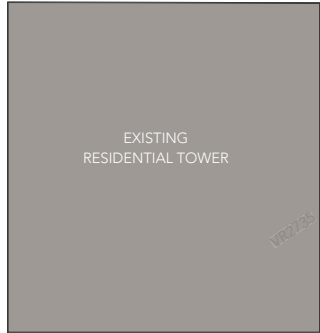
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drawn: MCH/POH	date: May 2015
checked:	
scale: as noted	project no.: 424
sheet title: GROUND FLOOR PLAN	

sheet no.:  
**A3.01**  
rev.:  
plotted: 17-10-24 11:32:16 AM

P428 plans.vcx





LEVEL 2 PLAN  
Scale: 3/32" = 1'-0"

SUPERSEDED

revisions:		
no.	date	description

issues:		
2016-11-22	Issued for Reasoning	
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**project:**  
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North Vancouver BC  


**consultant:**

**seal:**

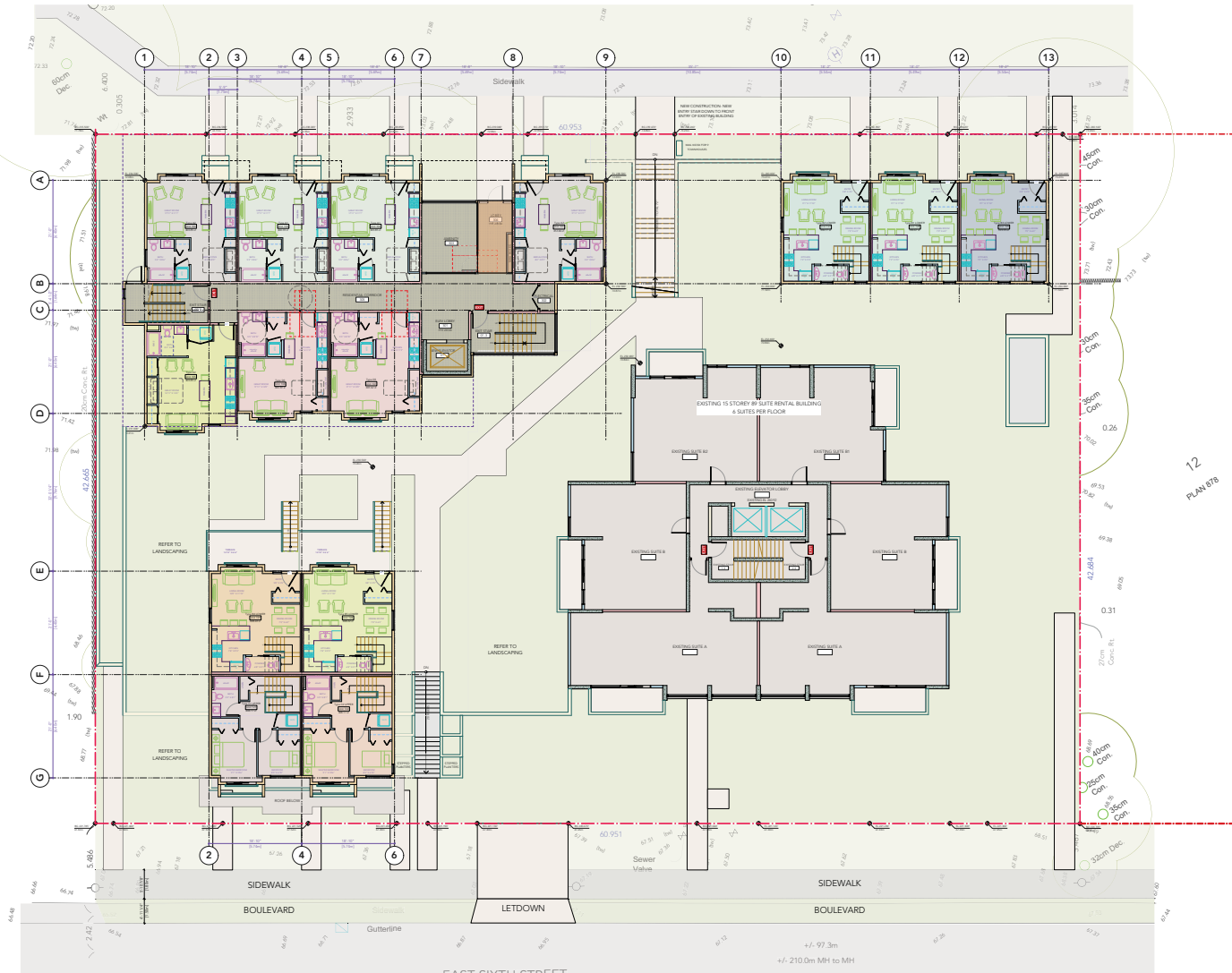
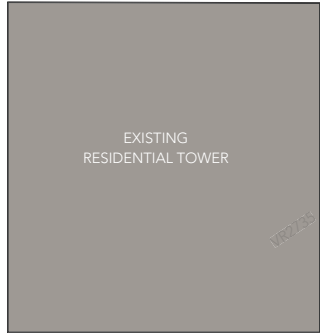
drawn: MCH/POH	date: May 2015
checked:	
scale: as noted	project no.: 424
sheet title: LEVEL 2 PLAN	

sheet no.:  
**A3.02**

FW: 

plotted: 17-10-24 11:32:23 AM





**LEVEL 3 PLAN (UPPER GROUND)**  
Scale: 3/32" = 1'-0"

**SUPERSEDED**

revisions:		
no.	date	description

issues:		
2016-11-22	Issued for Reasoning	
2017-01-09	Reissued for Reasoning	
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North Vancouver BC  

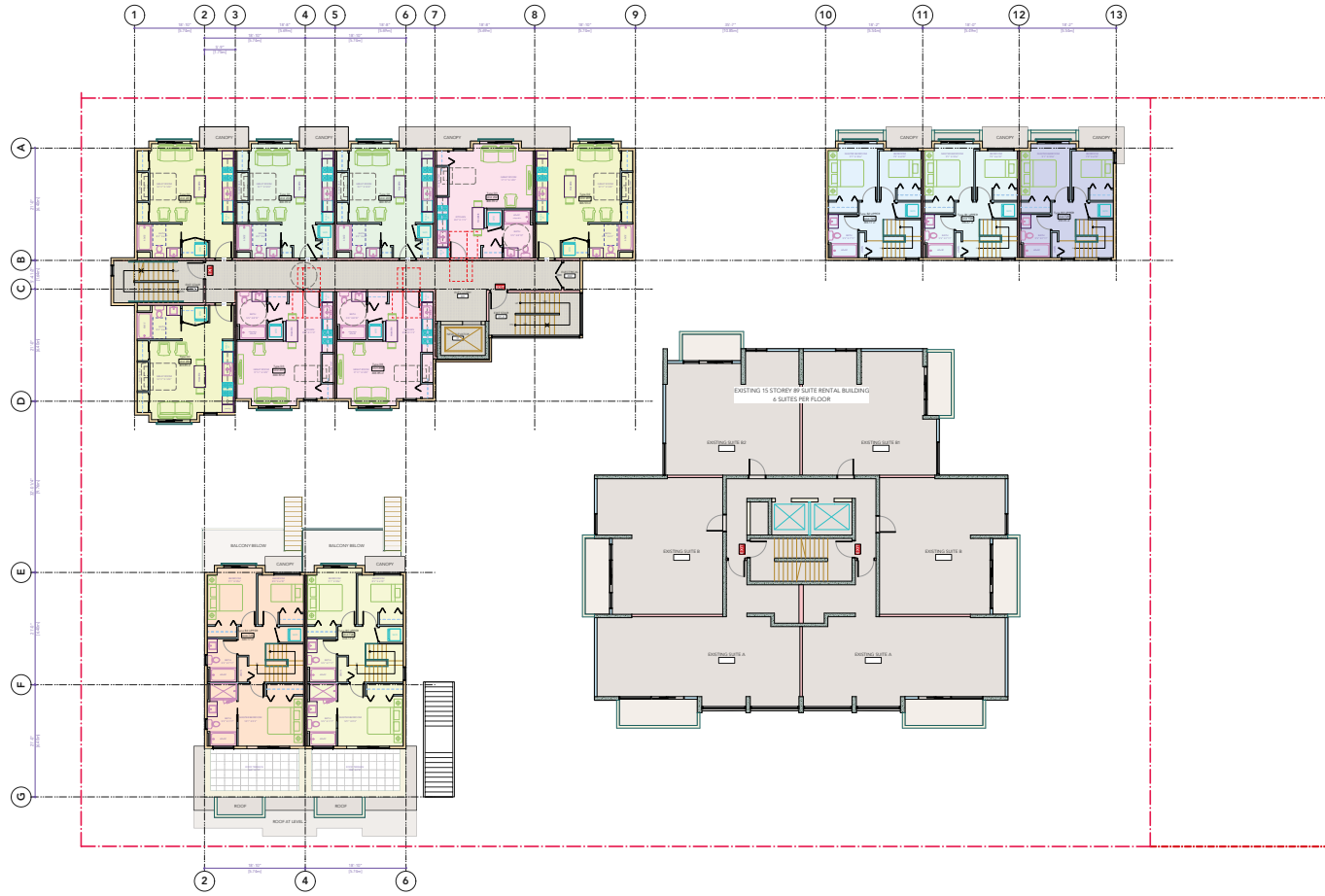

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**seal:**

drawn:	MCH/POH	date:	May 2015
checked:			
scale:	as noted	project no.:	424
sheet title:	LEVEL 3 PLAN		

sheet no.: **A3.03**  
plotted: 17-10-24 11:32:29 AM  


EXISTING  
RESIDENTIAL TOWER



**LEVEL 4 PLAN**  
Scale: 3/32" = 1'-0"

**SUPERSEDED**

revisions:		
no.	date	description

issues:	
2016-11-22	Issued for Reasoning
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**project:**  
Proposed Residential Infill  
151 East Keith  
North Vancouver BC  
**Starlight**  
Investments

**consultant:**

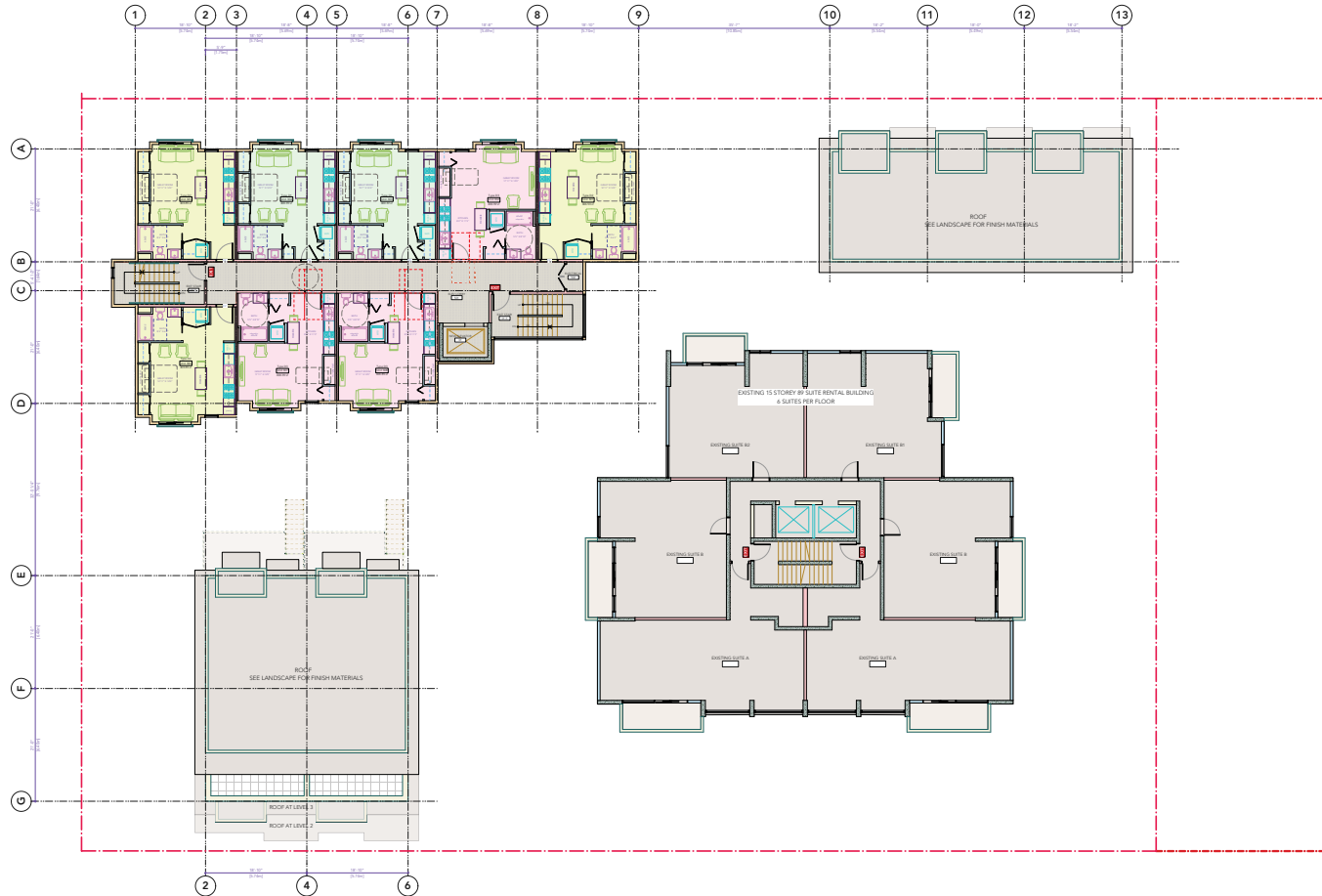
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drawn: MCH/POH	date: May 2015
checked	
scale: as noted	project no.: 424
sheet title: LEVEL 4 PLAN	

sheet no.:  
**A3.04**  
rev.:

plotted: 17-10-24 11:32:35 AM

EXISTING  
RESIDENTIAL TOWER



LEVEL 5 PLAN  
Scale: 1/8" = 1'-0"

SUPERSEDED

revisions:		
no.	date	description

issues:		
2016-11-22	Issued for Re zoning	
2017-10-25	Reissued for Re zoning	

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**Starlight**  
improvements

**consultant:**

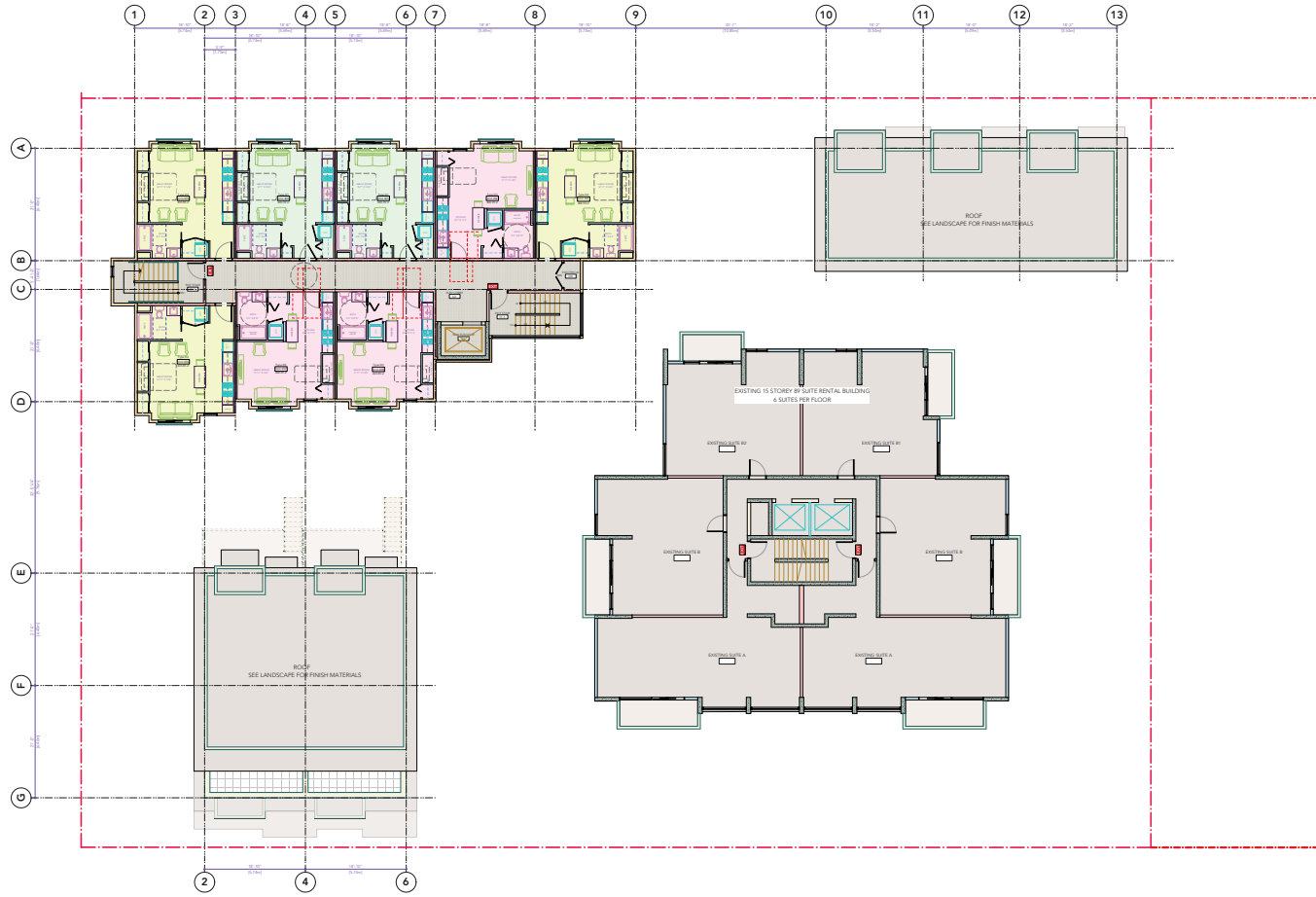
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drawn: MCH/POH	date: May 2015
checked	
scale: as noted	project no.: 424
sheet title: LEVEL 5 PLAN	

sheet no.: <b>A3.05</b>	FD28 plans.vcx rev.: 
plotted: 17-10-24 11:32:40 AM	



EXISTING  
RESIDENTIAL TOWER



**LEVEL 6 PLAN**  
Scale: 1/8" = 1'-0"

**SUPERSEDED**

revisions:		
no.	date	description

issues:	
2016-11-22	Issued for Re zoning
2017-10-25	Reissued for Re zoning

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North Vancouver BC  
**Starlight**  
improvements

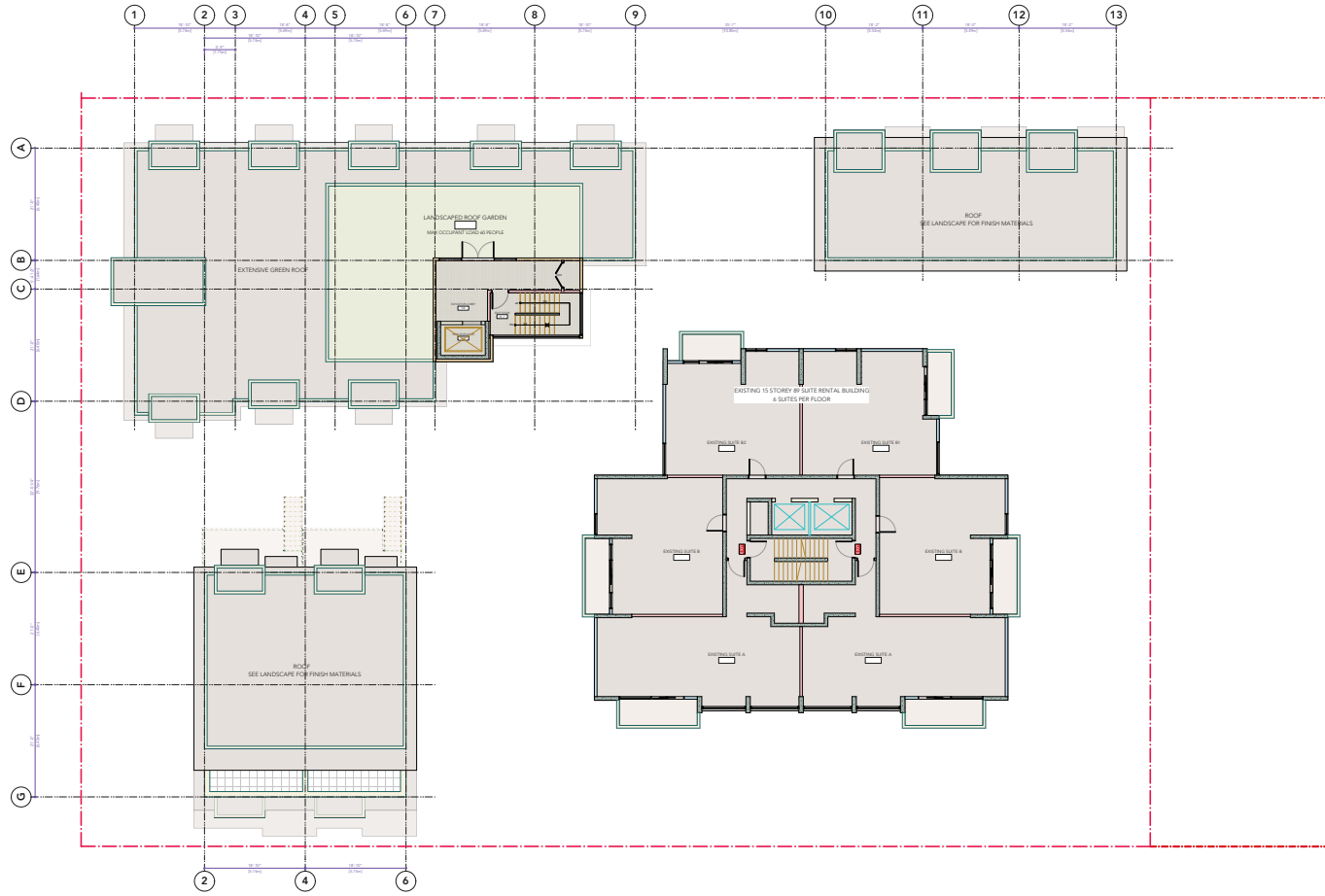
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checked	
scale: as noted	project no.: 424
sheet title: LEVEL 6 PLAN	

sheet no.: <b>A3.06</b>	FD28 plans.vcx 
rev.: 	
plotted: 17-10-24 11:32:46 AM	

EXISTING  
RESIDENTIAL TOWER



LEVEL 7 (ROOF) PLAN  
Scale: 1/8" = 1'-0"

SUPERSEDED

revisions:		
no.	date	description

issues:	
2016-11-22	Issued for Rezonig
2017-10-25	Reissued for Rezonig

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North Vancouver BC  

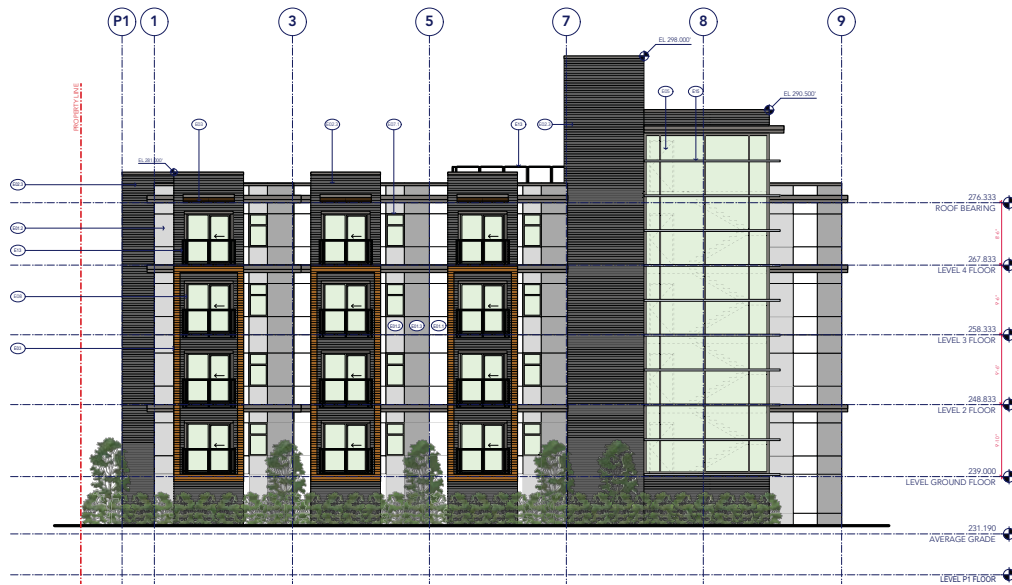

**consultant:**

seal:

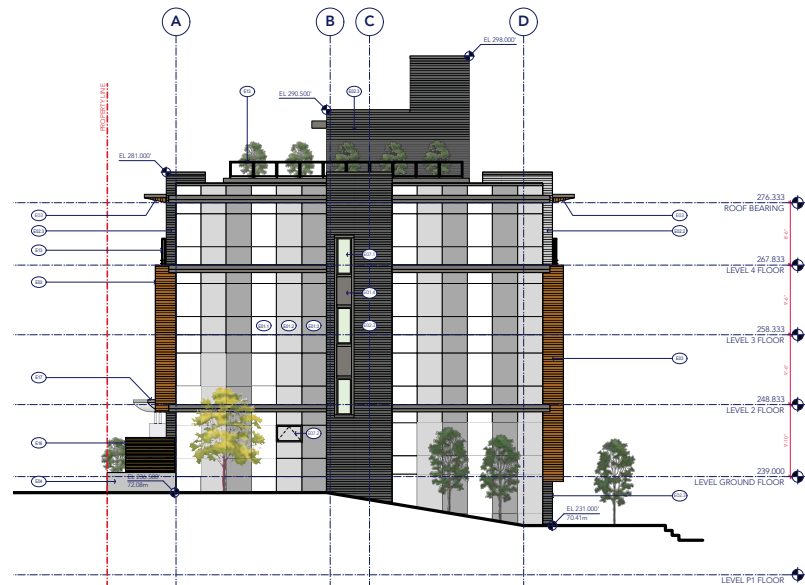
drawn: MCH/POH	date: May 2015
checked	
scale: as noted	project no.: 424
sheet title: LEVEL 7 PLAN	

sheet no.:  
**A3.07**  
rev.:  


plotted: 17-10-24 11:32:52 AM



**1 Building 1 - South Elevation (Courtyard)**  
Scale: 1/8" = 1'-0"



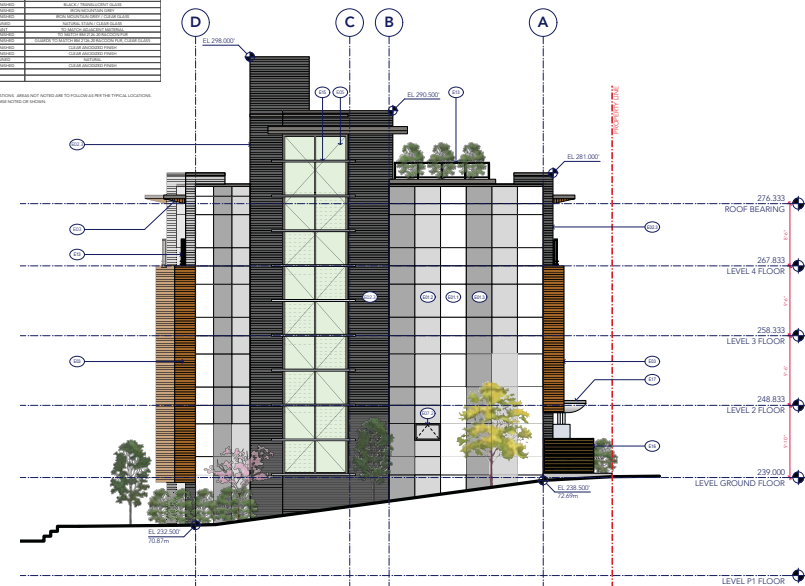
**2 Building 1 - West Elevation**  
Scale: 1/8" = 1'-0"

**EXTERIOR MATERIAL/ COLOUR SCHEDULE**

NO.	DESCRIPTION	FINISH	COLOUR
1	CLADDING - BRICK	CLAY	RED
2	CLADDING - CONCRETE	SMOOTH	GREY
3	CLADDING - GLASS	REFLECTIVE	GREEN
4	CLADDING - METAL	PAINTED	BLACK
5	CLADDING - STONE	NATURAL	BROWN
6	CLADDING - TERRAZZO	POLISHED	MIXED
7	CLADDING - WOOD	STAINED	NATURAL
8	CLADDING - ZEBRANO	STAINED	DARK
9	CLADDING - SLATE	NATURAL	GREY
10	CLADDING - TERRAZZO	POLISHED	MIXED
11	CLADDING - TERRAZZO	POLISHED	MIXED
12	CLADDING - TERRAZZO	POLISHED	MIXED
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100	CLADDING - TERRAZZO	POLISHED	MIXED



**3 Building 1 - North Elevation (Keith Road)**  
Scale: 1/8" = 1'-0"



**4 Building 1 - East Elevation**  
Scale: 1/8" = 1'-0"

NOTE: THE SAME "AVERAGE GRADE" OF 231.190, CALCULATED USING THE HIGHEST FINISHED GROUND LEVEL NORTH EAST OF BUILDING 3, AND THE LOWEST FINISHED GROUND LEVEL AT THE PERIMETER OF BUILDING 2, IS APPLIED TO EACH BUILDING AS THEY ALL SHARE THE SAME PARKING STRUCTURE.

**revisions:**

no.	date	description
1	2016-11-22	Issued for Reasoning
2	2017-01-09	Reissued for Reasoning
3	2017-10-25	Reissued for Reasoning

**issues:**

no.	date	description
1	2016-11-22	Issued for Reasoning
2	2017-01-09	Reissued for Reasoning
3	2017-10-25	Reissued for Reasoning

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**project:**  
Proposed Residential Infill  
151 East Keith  
North Vancouver, BC  
**Starlight**  
Investments

**consultant:**

**scale:**

**drawn:** MCH/DM  
**checked:**  
**scale:** as noted  
**sheet title:** Elevations Building 1  
**date:** October 2017  
**project no.:** 424

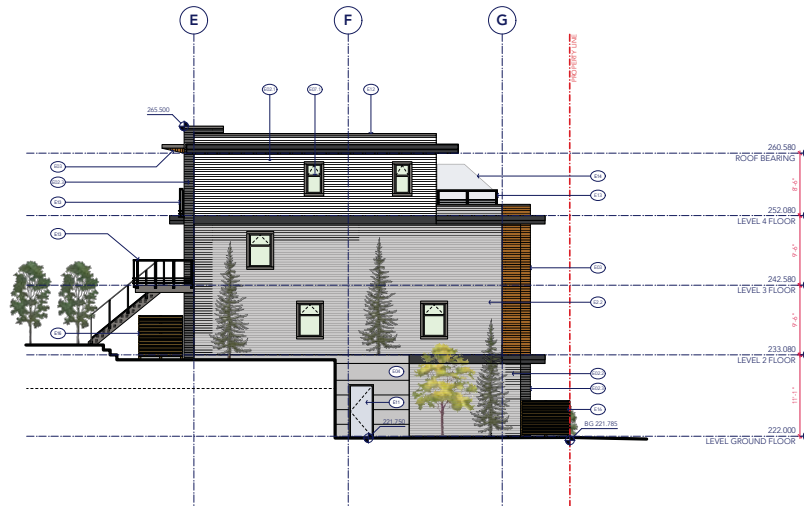
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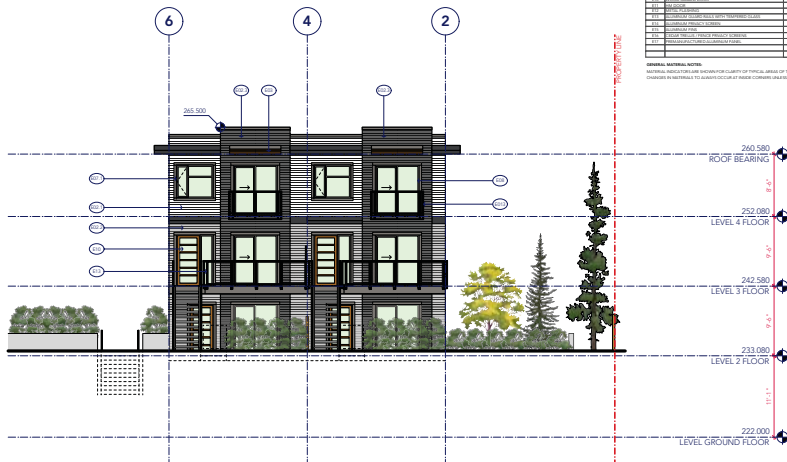




1 Building 2 - South Elevation (East 6th Street)  
Scale: 1/8" = 1'-0"



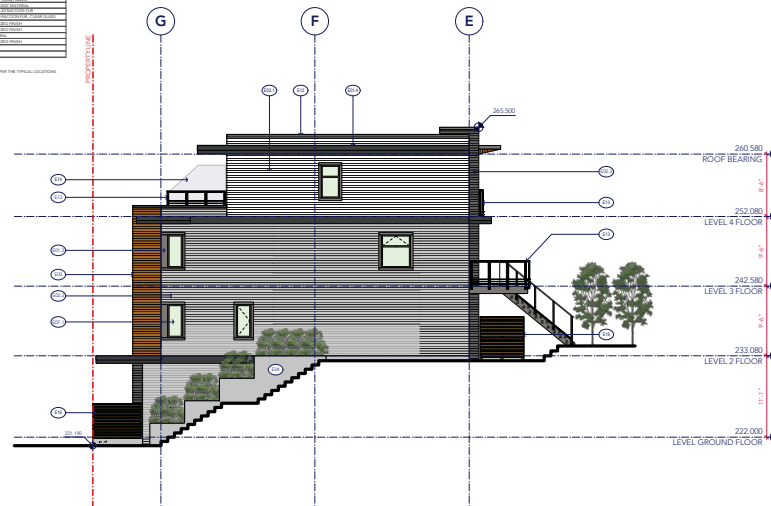
2 Building 2 - West Elevation  
Scale: 1/8" = 1'-0"



3 Building 2 - North Elevation  
Scale: 1/8" = 1'-0"

EXTERIOR MATERIAL / COLOUR SCHEDULE	
ITEM	DESCRIPTION
1	CLADDING - HORIZONTAL SLAT (WOOD GRAIN)
2	CLADDING - VERTICAL SLAT (WOOD GRAIN)
3	CLADDING - HORIZONTAL SLAT (METAL)
4	CLADDING - VERTICAL SLAT (METAL)
5	CLADDING - HORIZONTAL SLAT (CONCRETE)
6	CLADDING - VERTICAL SLAT (CONCRETE)
7	CLADDING - HORIZONTAL SLAT (BRICK)
8	CLADDING - VERTICAL SLAT (BRICK)
9	CLADDING - HORIZONTAL SLAT (GLASS)
10	CLADDING - VERTICAL SLAT (GLASS)
11	CLADDING - HORIZONTAL SLAT (STONE)
12	CLADDING - VERTICAL SLAT (STONE)
13	CLADDING - HORIZONTAL SLAT (CERAMIC TILE)
14	CLADDING - VERTICAL SLAT (CERAMIC TILE)
15	CLADDING - HORIZONTAL SLAT (PAINTED METAL)
16	CLADDING - VERTICAL SLAT (PAINTED METAL)
17	CLADDING - HORIZONTAL SLAT (PAINTED CONCRETE)
18	CLADDING - VERTICAL SLAT (PAINTED CONCRETE)
19	CLADDING - HORIZONTAL SLAT (PAINTED BRICK)
20	CLADDING - VERTICAL SLAT (PAINTED BRICK)
21	CLADDING - HORIZONTAL SLAT (PAINTED GLASS)
22	CLADDING - VERTICAL SLAT (PAINTED GLASS)
23	CLADDING - HORIZONTAL SLAT (PAINTED STONE)
24	CLADDING - VERTICAL SLAT (PAINTED STONE)
25	CLADDING - HORIZONTAL SLAT (PAINTED CERAMIC TILE)
26	CLADDING - VERTICAL SLAT (PAINTED CERAMIC TILE)
27	CLADDING - HORIZONTAL SLAT (PAINTED METAL)
28	CLADDING - VERTICAL SLAT (PAINTED METAL)
29	CLADDING - HORIZONTAL SLAT (PAINTED CONCRETE)
30	CLADDING - VERTICAL SLAT (PAINTED CONCRETE)
31	CLADDING - HORIZONTAL SLAT (PAINTED BRICK)
32	CLADDING - VERTICAL SLAT (PAINTED BRICK)
33	CLADDING - HORIZONTAL SLAT (PAINTED GLASS)
34	CLADDING - VERTICAL SLAT (PAINTED GLASS)
35	CLADDING - HORIZONTAL SLAT (PAINTED STONE)
36	CLADDING - VERTICAL SLAT (PAINTED STONE)
37	CLADDING - HORIZONTAL SLAT (PAINTED CERAMIC TILE)
38	CLADDING - VERTICAL SLAT (PAINTED CERAMIC TILE)
39	CLADDING - HORIZONTAL SLAT (PAINTED METAL)
40	CLADDING - VERTICAL SLAT (PAINTED METAL)
41	CLADDING - HORIZONTAL SLAT (PAINTED CONCRETE)
42	CLADDING - VERTICAL SLAT (PAINTED CONCRETE)
43	CLADDING - HORIZONTAL SLAT (PAINTED BRICK)
44	CLADDING - VERTICAL SLAT (PAINTED BRICK)
45	CLADDING - HORIZONTAL SLAT (PAINTED GLASS)
46	CLADDING - VERTICAL SLAT (PAINTED GLASS)
47	CLADDING - HORIZONTAL SLAT (PAINTED STONE)
48	CLADDING - VERTICAL SLAT (PAINTED STONE)
49	CLADDING - HORIZONTAL SLAT (PAINTED CERAMIC TILE)
50	CLADDING - VERTICAL SLAT (PAINTED CERAMIC TILE)
51	CLADDING - HORIZONTAL SLAT (PAINTED METAL)
52	CLADDING - VERTICAL SLAT (PAINTED METAL)
53	CLADDING - HORIZONTAL SLAT (PAINTED CONCRETE)
54	CLADDING - VERTICAL SLAT (PAINTED CONCRETE)
55	CLADDING - HORIZONTAL SLAT (PAINTED BRICK)
56	CLADDING - VERTICAL SLAT (PAINTED BRICK)
57	CLADDING - HORIZONTAL SLAT (PAINTED GLASS)
58	CLADDING - VERTICAL SLAT (PAINTED GLASS)
59	CLADDING - HORIZONTAL SLAT (PAINTED STONE)
60	CLADDING - VERTICAL SLAT (PAINTED STONE)
61	CLADDING - HORIZONTAL SLAT (PAINTED CERAMIC TILE)
62	CLADDING - VERTICAL SLAT (PAINTED CERAMIC TILE)
63	CLADDING - HORIZONTAL SLAT (PAINTED METAL)
64	CLADDING - VERTICAL SLAT (PAINTED METAL)
65	CLADDING - HORIZONTAL SLAT (PAINTED CONCRETE)
66	CLADDING - VERTICAL SLAT (PAINTED CONCRETE)
67	CLADDING - HORIZONTAL SLAT (PAINTED BRICK)
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69	CLADDING - HORIZONTAL SLAT (PAINTED GLASS)
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71	CLADDING - HORIZONTAL SLAT (PAINTED STONE)
72	CLADDING - VERTICAL SLAT (PAINTED STONE)
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74	CLADDING - VERTICAL SLAT (PAINTED CERAMIC TILE)
75	CLADDING - HORIZONTAL SLAT (PAINTED METAL)
76	CLADDING - VERTICAL SLAT (PAINTED METAL)
77	CLADDING - HORIZONTAL SLAT (PAINTED CONCRETE)
78	CLADDING - VERTICAL SLAT (PAINTED CONCRETE)
79	CLADDING - HORIZONTAL SLAT (PAINTED BRICK)
80	CLADDING - VERTICAL SLAT (PAINTED BRICK)
81	CLADDING - HORIZONTAL SLAT (PAINTED GLASS)
82	CLADDING - VERTICAL SLAT (PAINTED GLASS)
83	CLADDING - HORIZONTAL SLAT (PAINTED STONE)
84	CLADDING - VERTICAL SLAT (PAINTED STONE)
85	CLADDING - HORIZONTAL SLAT (PAINTED CERAMIC TILE)
86	CLADDING - VERTICAL SLAT (PAINTED CERAMIC TILE)
87	CLADDING - HORIZONTAL SLAT (PAINTED METAL)
88	CLADDING - VERTICAL SLAT (PAINTED METAL)
89	CLADDING - HORIZONTAL SLAT (PAINTED CONCRETE)
90	CLADDING - VERTICAL SLAT (PAINTED CONCRETE)
91	CLADDING - HORIZONTAL SLAT (PAINTED BRICK)
92	CLADDING - VERTICAL SLAT (PAINTED BRICK)
93	CLADDING - HORIZONTAL SLAT (PAINTED GLASS)
94	CLADDING - VERTICAL SLAT (PAINTED GLASS)
95	CLADDING - HORIZONTAL SLAT (PAINTED STONE)
96	CLADDING - VERTICAL SLAT (PAINTED STONE)
97	CLADDING - HORIZONTAL SLAT (PAINTED CERAMIC TILE)
98	CLADDING - VERTICAL SLAT (PAINTED CERAMIC TILE)
99	CLADDING - HORIZONTAL SLAT (PAINTED METAL)
100	CLADDING - VERTICAL SLAT (PAINTED METAL)

NOTES: MATERIALS AND FINISHES TO BE USED IN THE CONSTRUCTION OF THIS PROJECT SHALL BE AS SHOWN IN THE EXTERIOR MATERIAL / COLOUR SCHEDULE. MATERIALS AND FINISHES TO BE USED IN THE CONSTRUCTION OF THIS PROJECT SHALL BE AS SHOWN IN THE EXTERIOR MATERIAL / COLOUR SCHEDULE. MATERIALS AND FINISHES TO BE USED IN THE CONSTRUCTION OF THIS PROJECT SHALL BE AS SHOWN IN THE EXTERIOR MATERIAL / COLOUR SCHEDULE.



4 Building 2 - East Elevation  
Scale: 1/8" = 1'-0"

NOTE: THE SAME "AVERAGE GRADE" OF 231.190, CALCULATED USING THE HIGHEST FINISHED GROUND LEVEL NORTH EAST OF BUILDING 3, AND THE LOWEST FINISHED GROUND LEVEL AT THE PERIMETER OF BUILDING 2, IS APPLIED TO EACH BUILDING AS THEY ALL SHARE THE SAME PARKING STRUCTURE.

SUPERSEDED

revisions:

no.	date	description
1	2016-11-22	Issued for Reasoning
2	2017-01-09	Reissued for Reasoning
3	2017-10-25	Reissued for Reasoning

issues:

2016-11-22 Issued for Reasoning  
2017-01-09 Reissued for Reasoning  
2017-10-25 Reissued for Reasoning

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Suite 205 - 1628 West 4th Avenue, Vancouver, B.C.  
Telephone: (604) 755-8100 | Web: www.bha.ca

project:

Proposed Residential Infill  
151 East Keith  
North Vancouver BC  
**Starlight**  
Investments

consultant:

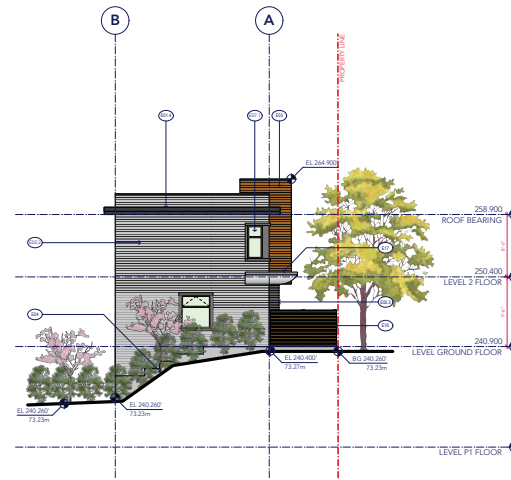
scale:

drawn: MCH/DM date: August 2016  
checked: project no.: 424  
scale: as noted  
sheet title: Elevations Building 2

sheet no.: A5.02  
plotted: 17-10-24 10:55:29 AM



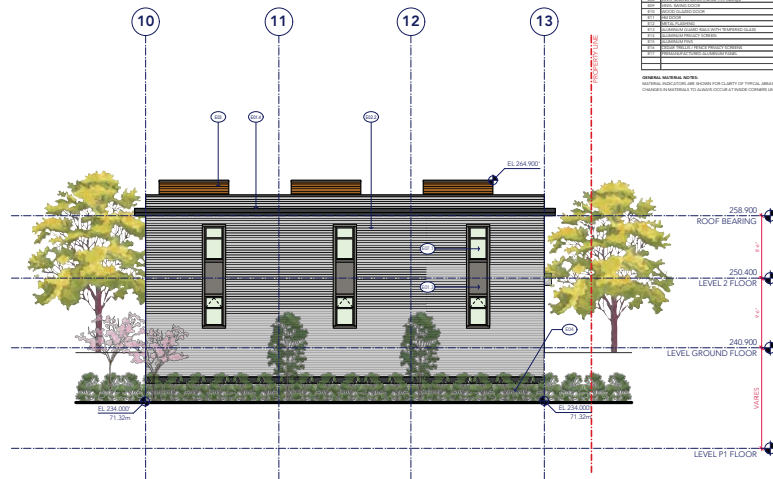
1 Building 3 - North Elevation (Keith Road)  
Scale: 1/8" = 1'-0"



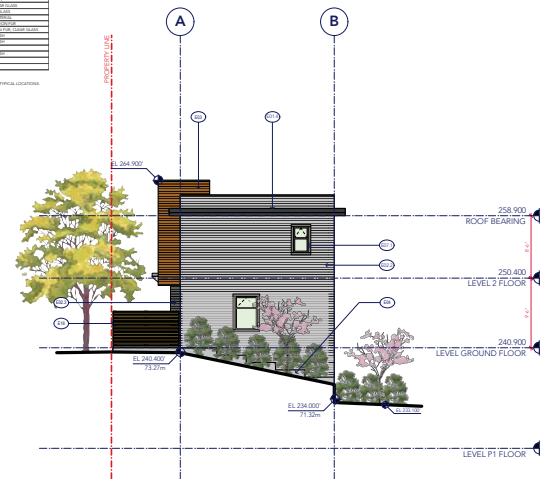
2 Building 3 - East Elevation  
Scale: 1/8" = 1'-0"

EXTERIOR MATERIAL / COLOUR SCHEDULE	FINISH	COLOUR
1. ROOFING	ASPH/FLT	BLACK
2. EXTERIOR WALLS	CONCRETE	GREY
3. EXTERIOR WALLS	BRICK	RED
4. EXTERIOR WALLS	CLAY TILE	BROWN
5. EXTERIOR WALLS	STONE	GREY
6. EXTERIOR WALLS	WOOD	BROWN
7. EXTERIOR WALLS	PLASTER	WHITE
8. EXTERIOR WALLS	PAINT	WHITE
9. EXTERIOR WALLS	PAINT	GREY
10. EXTERIOR WALLS	PAINT	BROWN
11. EXTERIOR WALLS	PAINT	GREEN
12. EXTERIOR WALLS	PAINT	BLUE
13. EXTERIOR WALLS	PAINT	PURPLE
14. EXTERIOR WALLS	PAINT	PINK
15. EXTERIOR WALLS	PAINT	YELLOW
16. EXTERIOR WALLS	PAINT	ORANGE
17. EXTERIOR WALLS	PAINT	RED
18. EXTERIOR WALLS	PAINT	BROWN
19. EXTERIOR WALLS	PAINT	GREY
20. EXTERIOR WALLS	PAINT	WHITE
21. EXTERIOR WALLS	PAINT	BLACK
22. EXTERIOR WALLS	PAINT	BLUE
23. EXTERIOR WALLS	PAINT	GREEN
24. EXTERIOR WALLS	PAINT	PURPLE
25. EXTERIOR WALLS	PAINT	PINK
26. EXTERIOR WALLS	PAINT	YELLOW
27. EXTERIOR WALLS	PAINT	ORANGE
28. EXTERIOR WALLS	PAINT	RED
29. EXTERIOR WALLS	PAINT	BROWN
30. EXTERIOR WALLS	PAINT	GREY
31. EXTERIOR WALLS	PAINT	WHITE
32. EXTERIOR WALLS	PAINT	BLACK
33. EXTERIOR WALLS	PAINT	BLUE
34. EXTERIOR WALLS	PAINT	GREEN
35. EXTERIOR WALLS	PAINT	PURPLE
36. EXTERIOR WALLS	PAINT	PINK
37. EXTERIOR WALLS	PAINT	YELLOW
38. EXTERIOR WALLS	PAINT	ORANGE
39. EXTERIOR WALLS	PAINT	RED
40. EXTERIOR WALLS	PAINT	BROWN
41. EXTERIOR WALLS	PAINT	GREY
42. EXTERIOR WALLS	PAINT	WHITE
43. EXTERIOR WALLS	PAINT	BLACK
44. EXTERIOR WALLS	PAINT	BLUE
45. EXTERIOR WALLS	PAINT	GREEN
46. EXTERIOR WALLS	PAINT	PURPLE
47. EXTERIOR WALLS	PAINT	PINK
48. EXTERIOR WALLS	PAINT	YELLOW
49. EXTERIOR WALLS	PAINT	ORANGE
50. EXTERIOR WALLS	PAINT	RED
51. EXTERIOR WALLS	PAINT	BROWN
52. EXTERIOR WALLS	PAINT	GREY
53. EXTERIOR WALLS	PAINT	WHITE
54. EXTERIOR WALLS	PAINT	BLACK
55. EXTERIOR WALLS	PAINT	BLUE
56. EXTERIOR WALLS	PAINT	GREEN
57. EXTERIOR WALLS	PAINT	PURPLE
58. EXTERIOR WALLS	PAINT	PINK
59. EXTERIOR WALLS	PAINT	YELLOW
60. EXTERIOR WALLS	PAINT	ORANGE
61. EXTERIOR WALLS	PAINT	RED
62. EXTERIOR WALLS	PAINT	BROWN
63. EXTERIOR WALLS	PAINT	GREY
64. EXTERIOR WALLS	PAINT	WHITE
65. EXTERIOR WALLS	PAINT	BLACK
66. EXTERIOR WALLS	PAINT	BLUE
67. EXTERIOR WALLS	PAINT	GREEN
68. EXTERIOR WALLS	PAINT	PURPLE
69. EXTERIOR WALLS	PAINT	PINK
70. EXTERIOR WALLS	PAINT	YELLOW
71. EXTERIOR WALLS	PAINT	ORANGE
72. EXTERIOR WALLS	PAINT	RED
73. EXTERIOR WALLS	PAINT	BROWN
74. EXTERIOR WALLS	PAINT	GREY
75. EXTERIOR WALLS	PAINT	WHITE
76. EXTERIOR WALLS	PAINT	BLACK
77. EXTERIOR WALLS	PAINT	BLUE
78. EXTERIOR WALLS	PAINT	GREEN
79. EXTERIOR WALLS	PAINT	PURPLE
80. EXTERIOR WALLS	PAINT	PINK
81. EXTERIOR WALLS	PAINT	YELLOW
82. EXTERIOR WALLS	PAINT	ORANGE
83. EXTERIOR WALLS	PAINT	RED
84. EXTERIOR WALLS	PAINT	BROWN
85. EXTERIOR WALLS	PAINT	GREY
86. EXTERIOR WALLS	PAINT	WHITE
87. EXTERIOR WALLS	PAINT	BLACK
88. EXTERIOR WALLS	PAINT	BLUE
89. EXTERIOR WALLS	PAINT	GREEN
90. EXTERIOR WALLS	PAINT	PURPLE
91. EXTERIOR WALLS	PAINT	PINK
92. EXTERIOR WALLS	PAINT	YELLOW
93. EXTERIOR WALLS	PAINT	ORANGE
94. EXTERIOR WALLS	PAINT	RED
95. EXTERIOR WALLS	PAINT	BROWN
96. EXTERIOR WALLS	PAINT	GREY
97. EXTERIOR WALLS	PAINT	WHITE
98. EXTERIOR WALLS	PAINT	BLACK
99. EXTERIOR WALLS	PAINT	BLUE
100. EXTERIOR WALLS	PAINT	GREEN

GENERAL MATERIAL NOTES:  
1. MATERIALS AND FINISHES ARE TO BE USED IN ACCORDANCE WITH THE ARCHITECT'S INTENT.  
2. MATERIALS AND FINISHES ARE TO BE USED IN ACCORDANCE WITH THE ARCHITECT'S INTENT.



3 Building 3 - South Elevation  
Scale: 1/8" = 1'-0"



4 Building 3 - West Elevation  
Scale: 1/8" = 1'-0"

NOTE: THE SAME "AVERAGE GRADE" OF 231.190, CALCULATED USING THE HIGHEST FINISHED GROUND LEVEL NORTH EAST OF BUILDING 3, AND THE LOWEST FINISHED GROUND LEVEL AT THE PERIMETER OF BUILDING 2, IS APPLIED TO EACH BUILDING AS THEY ALL SHARE THE SAME PARKING STRUCTURE.

**SUPERSEDED**

revisions:		
no.	date	description

issues:  
2016-11-22 Issued for Reasoning  
2017-01-09 Reissued for Reasoning  
2017-10-25 Reissued for Reasoning

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Telephone: (604) 750-8100 | Web: www.bha.ca

**project:**  
Proposed Residential Infill  
151 East Keith  
North Vancouver BC  
**Starlight**  
Investments

**consultant:**

seal:

drawn:	date:
MCH/DM	October 2017
checked:	
scale:	project no.:
as noted	424

**sheet title:**  
Elevations  
Building 3

sheet no.:  
**A5.03**  
REV:   
plotted: 17-10-24 10:56:03 AM



revisions:		
no.	date	description

issues:		
2016-11-22	Issued for Reasoning	
2017-01-09	Reissued for Reasoning	
2017-10-25	Reissued for Reasoning	

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**project:**  
 Proposed Residential Infill  
 151 East Keith  
 North Vancouver, BC  
**Starlight**  
 Investments

**consultant:**

**scale:**

**1 East 6th Street Elevation**  
 Scale: 1:150

drawn:	date:
MCH/DM	October 2017
checked	
scale:	project no.:
as noted	424

**sheet title:**  
 Street Elevations  
 East 6th Street

sheet no.:

**A5.04**



plotted: 17-10-24 10:56:41 AM

**SUPERSEDED**





revisions:		
no.	date	description

issues:		
2016-11-22	Issued for Reasoning	
2017-01-09	Reissued for Reasoning	
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**project:**  
 Proposed Residential Infill  
 151 East Keith  
 North Vancouver BC  
**Starlight**  
 Investments

**consultant:**

scale:

**1 Keith Road Elevation**  
 Scale: 1:150

drawn:	date:
MCH/DM	October 2017
checked	
scale:	project no.:
as noted	424

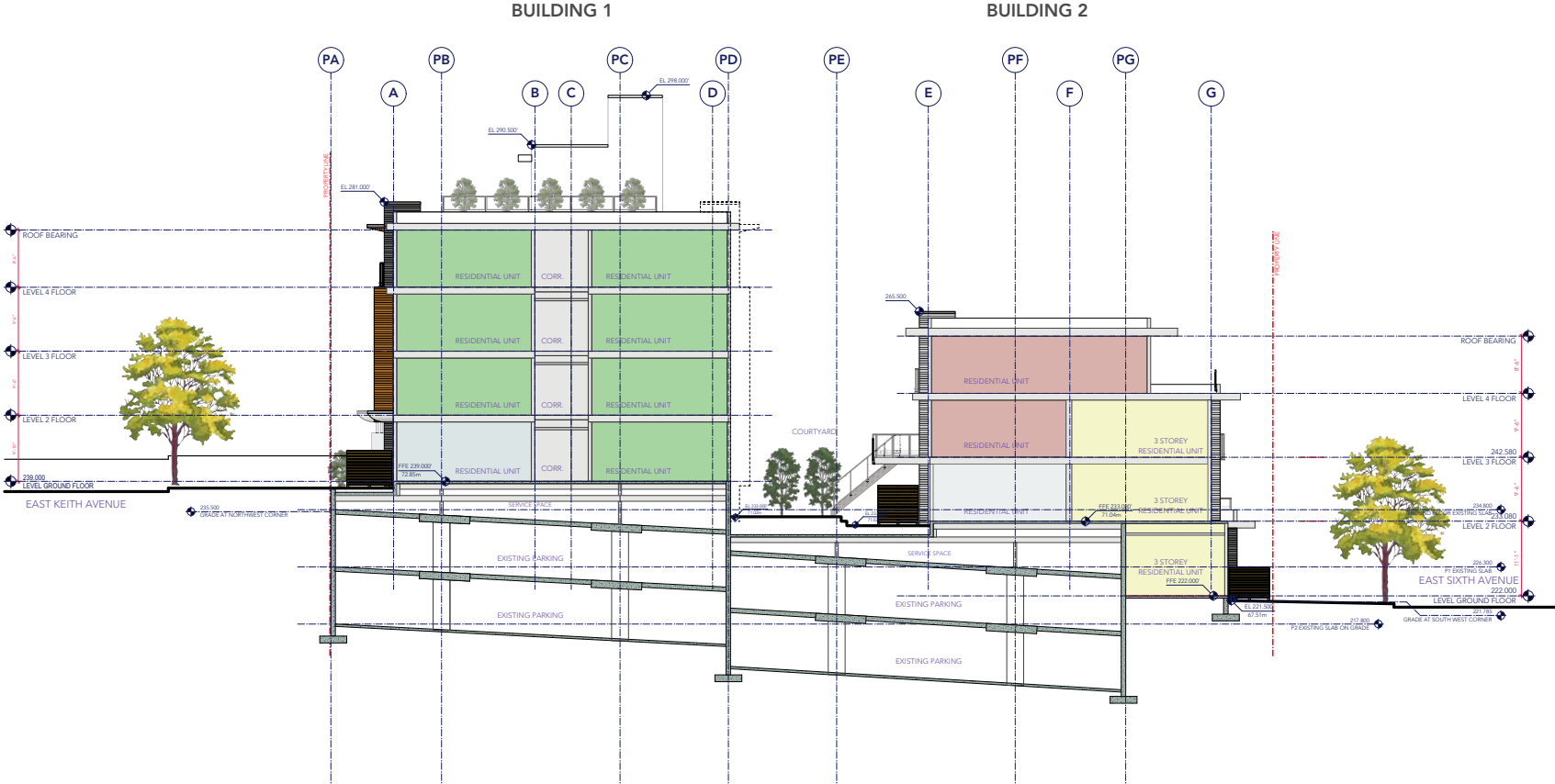
**sheet title:**  
 Street Elevations  
 East 6th Street

sheet no.:  
**A5.05**  
 PKG.:  
 plotted: 17-10-24 10:57:09 AM

**SUPERSEDED**

revisions:		
no.	date	description

**issues:**  
 2016-11-22 Issued for Reasoning  
 2017-10-25 Reissued for Reasoning



1 Cross Section  
 Scale: 1/8" = 1'-0"

SUPERSEDED

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**project:**  
 Proposed Residential Infill  
 151 East Keith  
 North Vancouver BC  


**consultant:**

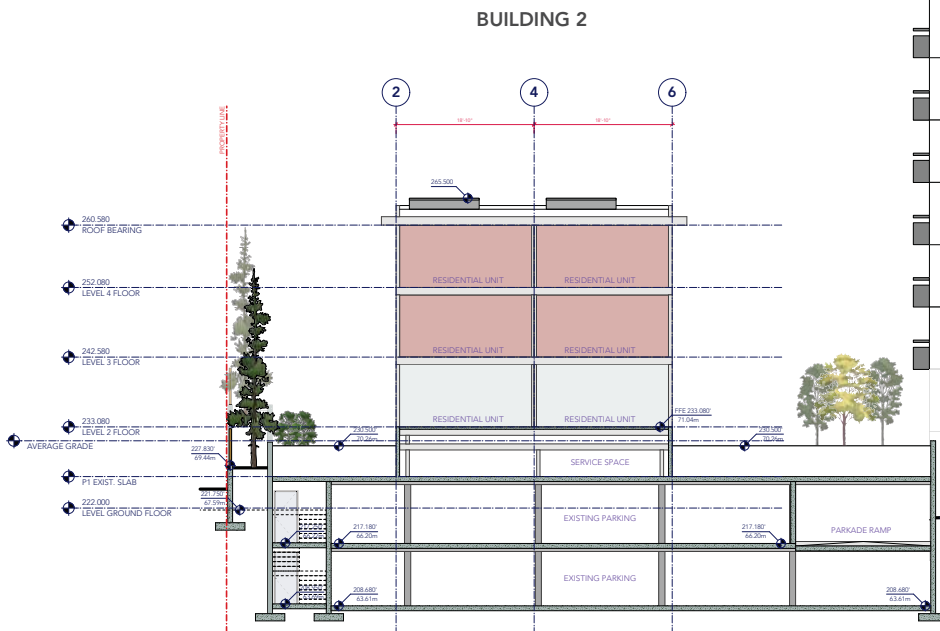
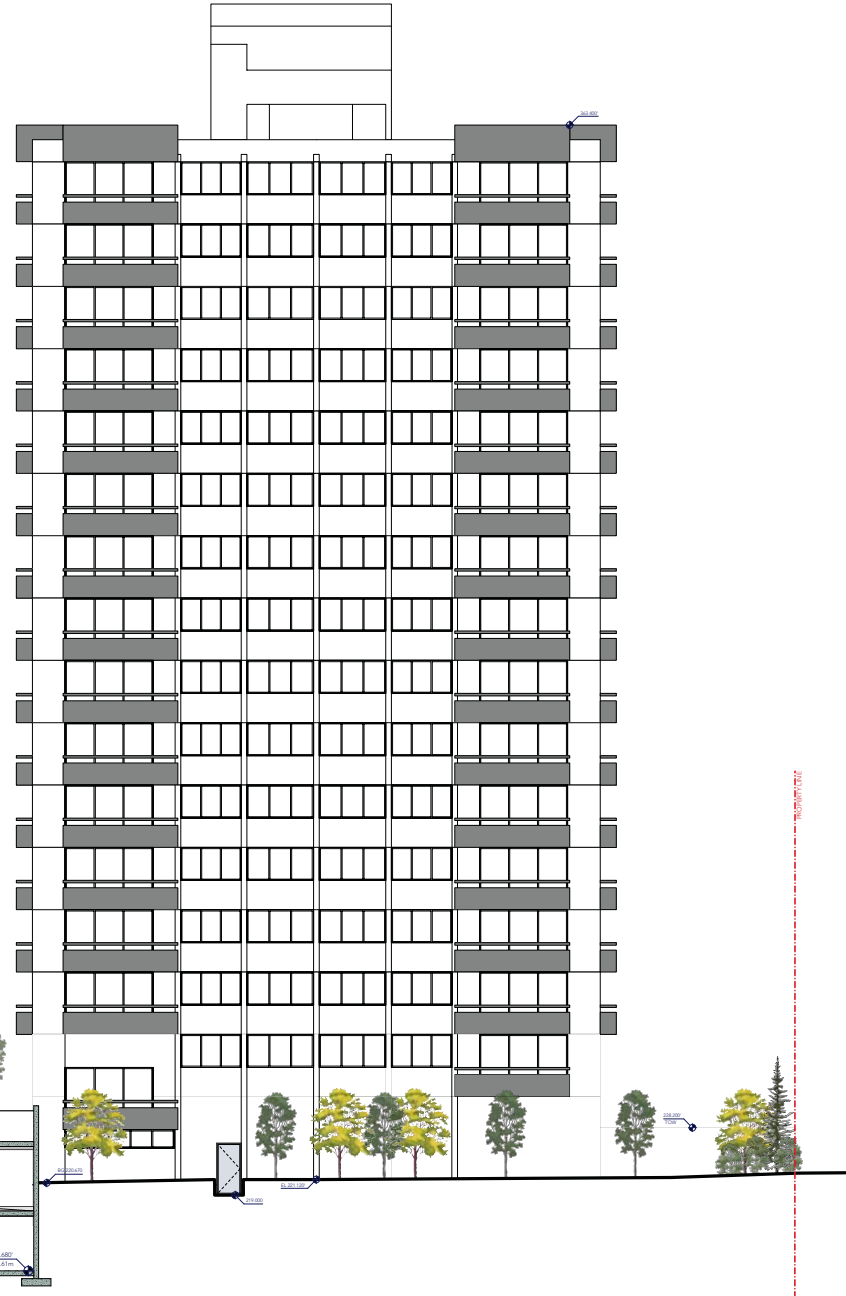
seal:

drawn:	date:
MCH/DM	October 2017
checked:	
scale:	project no.:
as noted	424
sheet title:	
Cross Section	

sheet no.:  
**A5.10**

P424-Elevations v2016.vmax  
 17-10-24 10:57:22 AM  
 plotted:





revisions:		
no.	date	description

issues:  
2017-10-25 Reissued for Reasoning

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**project:**

Proposed Residential Infill  
151 East Keith  
North Vancouver BC

**Starlight**  
Investments

consultant:

seal:

drawn:	date:
MCH/DM	October 2017
checked:	
scale:	project no.:
as noted	424
sheet title:	
Longitudinal Section	

sheet no.:

**A5.11**

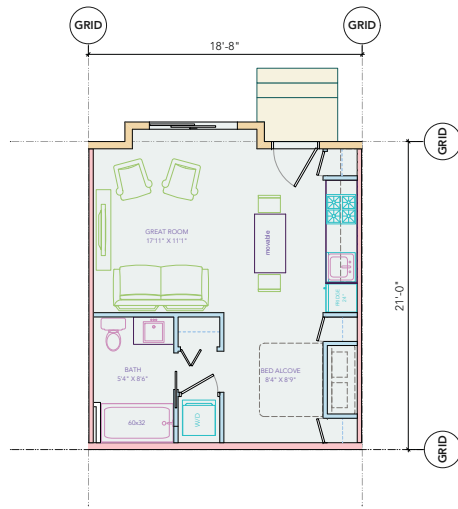
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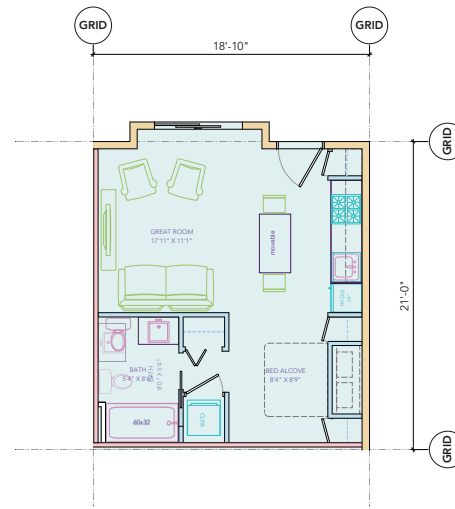
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SUPERSEDED

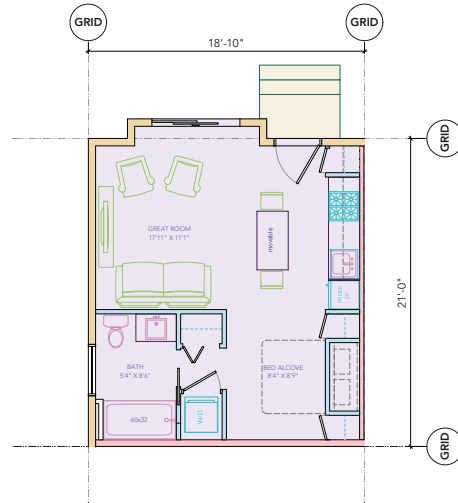




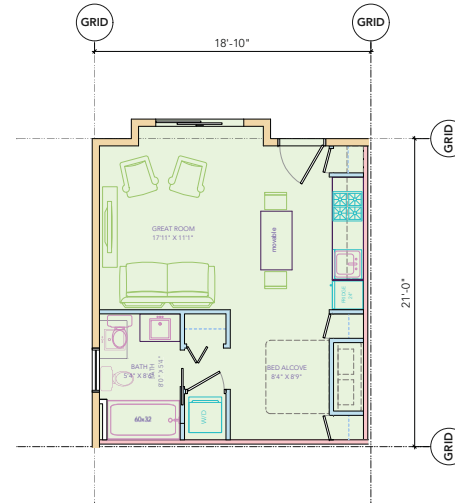
**A1** UNIT TYPE A1 - STUDIO TOWNHOUSE (INTERNAL)  
Scale: 1/4" = 1'-0"



**A3** UNIT TYPE A3 - TOWNHOUSE STUDIO (END)  
Scale: 1/4" = 1'-0"



**A2** UNIT TYPE A2 - TOWNHOUSE STUDIO (END)  
Scale: 1/4" = 1'-0"



**A4** UNIT TYPE A4 - TOWNHOUSE STUDIO (END)  
Scale: 1/4" = 1'-0"

# RESIDENTIAL TOWNHOUSE UNITS

**SUPERSEDED**

revisions:		
no.	date	description

issues:	
2016-11-22	Issued for Reasoning
2017-10-25	Reissued for Reasoning

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**project:**  
Proposed Residential Infill  
151 East Keith  
North Vancouver BC  
**Starlight**  
imcoconments

**consultant:**

**seal:**

drawn:	date:
MCH/POH	April 2015
checked	
scale:	project no.:
as noted	424
sheet title:	
UNIT PLANS	

sheet no.:	FILED plans.vcx
<b>A6.01</b>	REV.:
	plotted: 17-10-24 11:32:58 AM



revisions:		
no.	date	description

**issues:**  
 2016-11-22 Issued for Rezoning  
 2017-10-25 Reissued for Rezoning

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**project:**  
 Proposed Residential Infill  
 151 East Keith  
 North Vancouver BC  
**Starlight**  
 Investments

**consultant:**

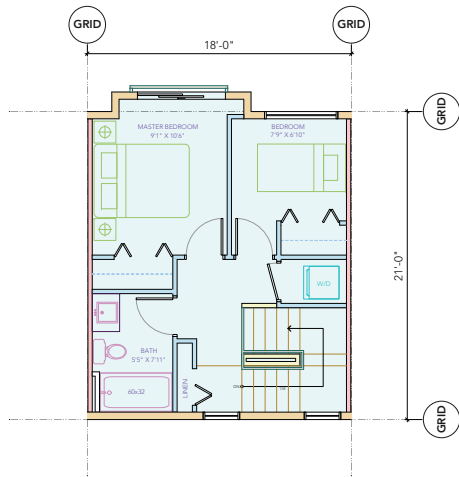
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drawn:	date:
MCH/POH	April 2015
checked:	
scale:	project no.:
as noted	424
<b>sheet title:</b>	
UNIT PLANS	

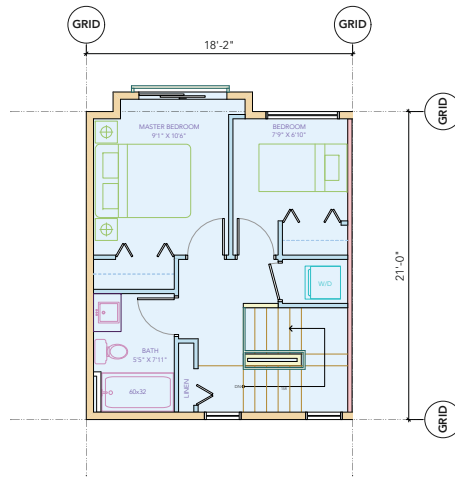
sheet no.:	FILED plans.vcx
<b>A6.02</b>	REV.:
plotted:	17-10-24 11:33:04 AM



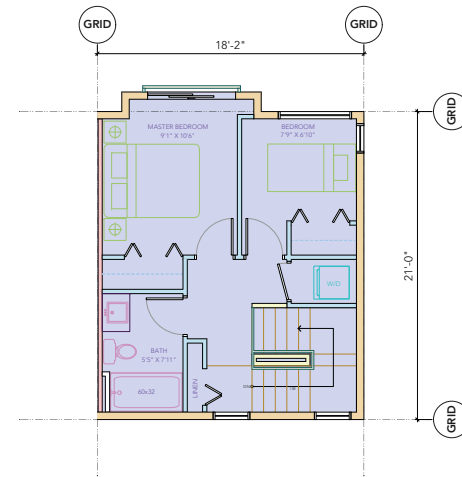
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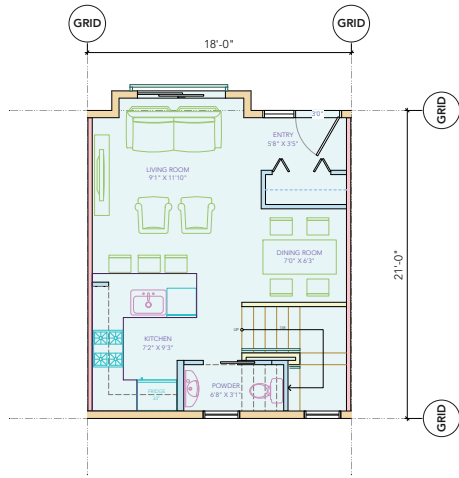
**B1** 2 STOREY TOWNHOUSE (UPPER)  
 Scale: 1/4" = 1'-0"



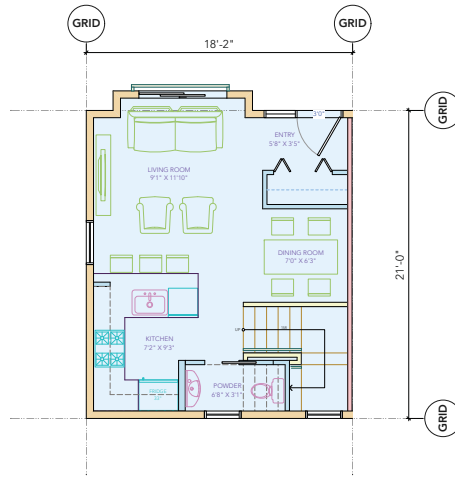
**B2** 2 STOREY TOWNHOUSE (UPPER)  
 Scale: 1/4" = 1'-0"



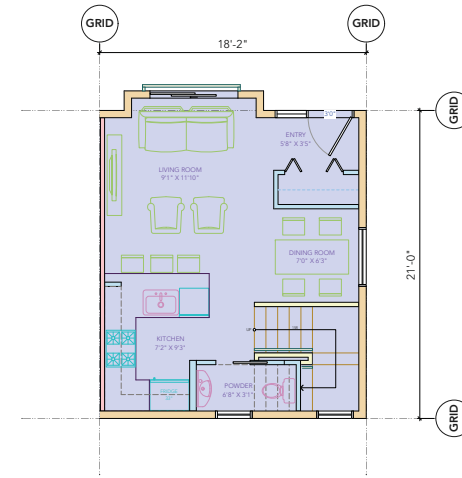
**B3** 2 STOREY TOWNHOUSE (UPPER)  
 Scale: 1/4" = 1'-0"



**B1** 2 STOREY TOWNHOUSE (LOWER)  
 Scale: 1/4" = 1'-0"



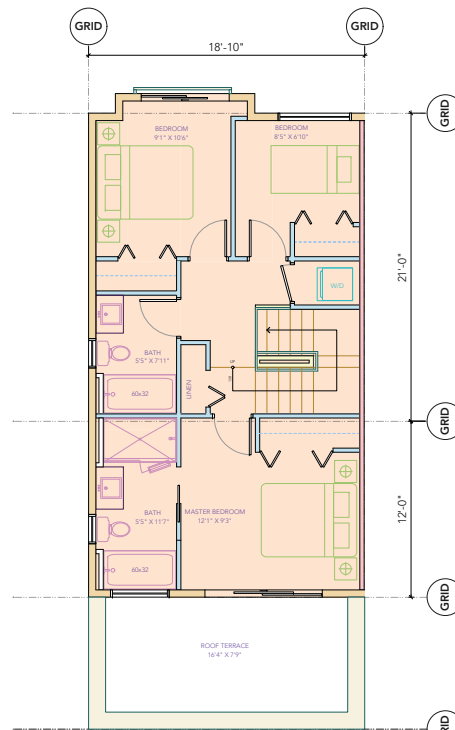
**B2** 2 STOREY TOWNHOUSE (LOWER)  
 Scale: 1/4" = 1'-0"



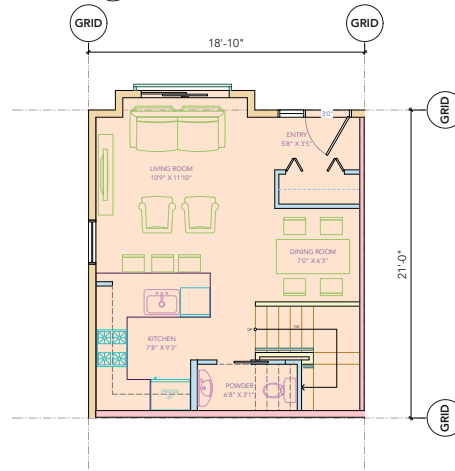
**B3** 2 STOREY TOWNHOUSE (LOWER)  
 Scale: 1/4" = 1'-0"

# RESIDENTIAL TOWNHOUSE UNITS

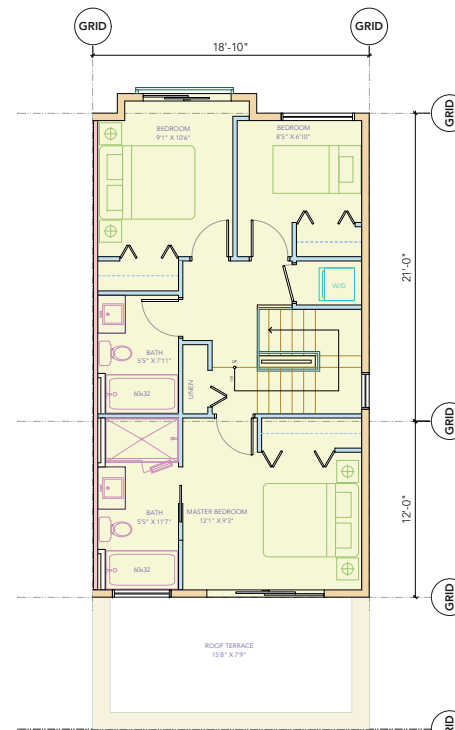
**SUPERSEDED**



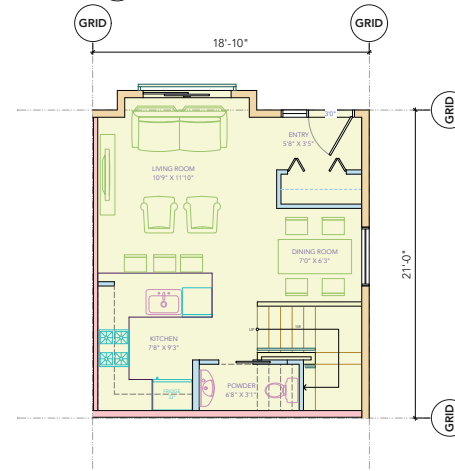
**B4 2 STOREY TOWNHOUSE (UPPER)**  
Scale: 1/4" = 1'-0"



**B4 2 STOREY TOWNHOUSE (LOWER)**  
Scale: 1/4" = 1'-0"



**B5 2 STOREY TOWNHOUSE (UPPER)**  
Scale: 1/4" = 1'-0"



**B5 2 STOREY TOWNHOUSE (LOWER)**  
Scale: 1/4" = 1'-0"

# RESIDENTIAL TOWNHOUSE UNITS

**SUPERSEDED**

revisions:		
no.	date	description

issues:	
2016-11-22	Issued for Rezoning
2017-10-25	Reissued for Rezoning

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**Burrowes Huggins Architects**  
Suite 205 - 1628 West 1st Avenue, Vancouver, B.C.  
Telephone: (604) 730-8100 | Web: www.bha.ca

**project:**  
Proposed Residential Infill  
151 East Keith  
North Vancouver BC  
**Starlight**  
imcolements

**consultant:**

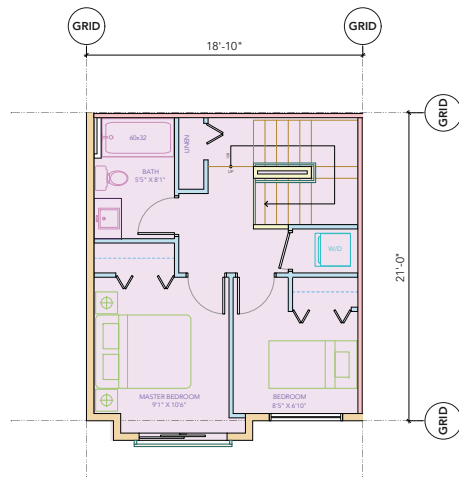
**seal:**

drawn:	MCH/POH	date:	April 2015
checked:			
scale:	as noted	project no.:	424
sheet title:	UNIT PLANS		

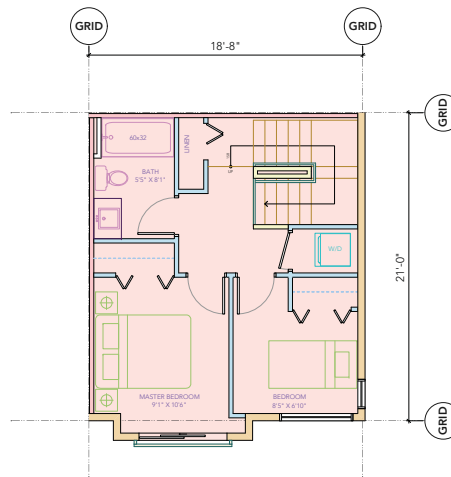
sheet no.:	F428 plans.vcx
<b>A6.03</b>	
rev.:	
plotted:	17-10-24 11:33:09 AM



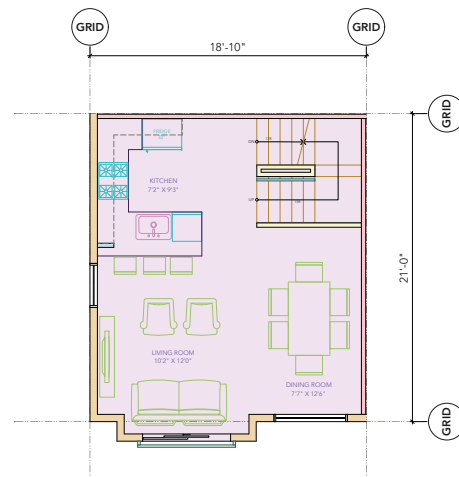




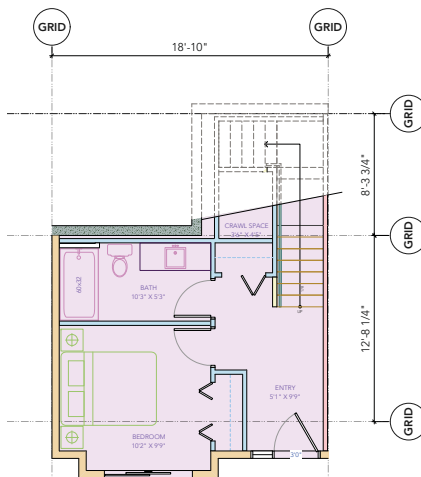
**C1 3 STOREY TOWNHOUSE (UPPER)**  
Scale: 1/4" = 1'-0"



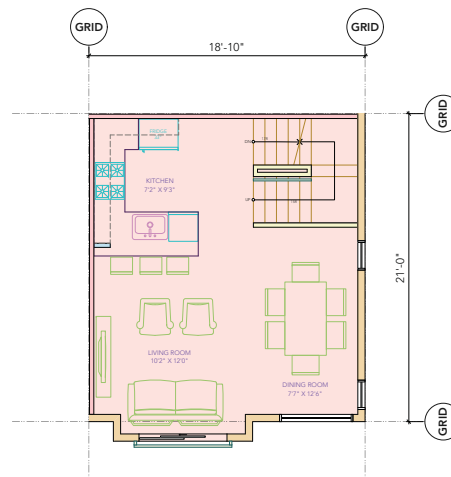
**C2 3 STOREY TOWNHOUSE (UPPER)**  
Scale: 1/4" = 1'-0"



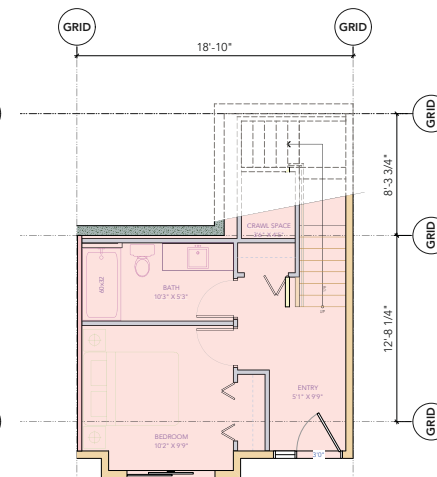
**C1 3 STOREY TOWNHOUSE (MID)**  
Scale: 1/4" = 1'-0"



**C1 3 STOREY TOWNHOUSE (LOWER)**  
Scale: 1/4" = 1'-0"



**C2 3 STOREY TOWNHOUSE (MID)**  
Scale: 1/4" = 1'-0"



**C2 3 STOREY TOWNHOUSE (LOWER)**  
Scale: 1/4" = 1'-0"

revisions:		
no.	date	description

**issues:**  
2016-11-22 Issued for Rezoning  
2017-10-25 Reissued for Rezoning

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Telephone: (604) 730-8100 | Web: www.bha.ca

**project:**  
Proposed Residential Infill  
151 East Keith  
North Vancouver BC  
**Starlight**  
Investments  
**consultant:**

scale:

drawn: MCH/POH	date: August 2016
checked:	
scale: as noted	project no.: 424
sheet title: UNIT PLANS	

sheet no.:  
**A6.04**  
rev.:

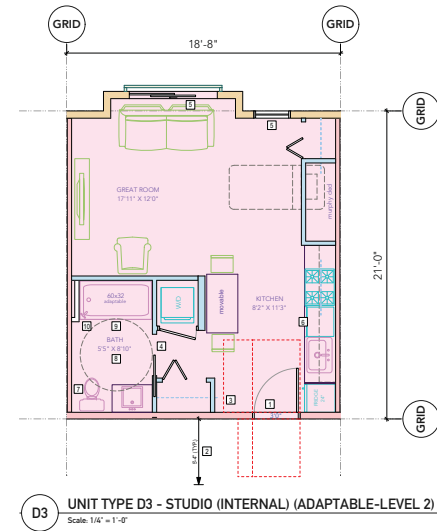
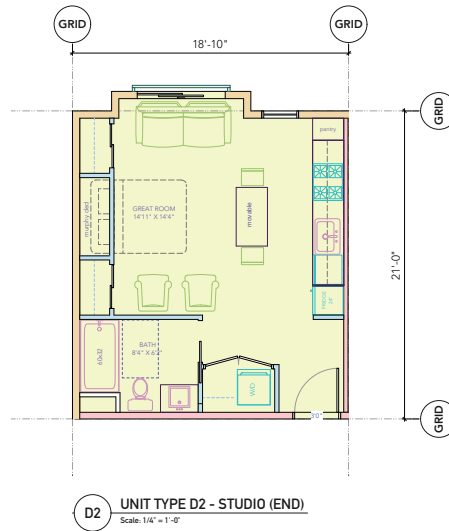
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plotted: 17-10-24 11:33 14 AM

# RESIDENTIAL TOWNHOUSE UNITS

**SUPERSEDED**



revisions:		
no.	date	description
issues:		
2016-11-22	Issued for Reasoning	
2017-10-25	Reissued for Reasoning	

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**project:**  
Proposed Residential Infill  
151 East Keith  
North Vancouver BC  
**Starlight**  
Investments

**consultant:**

seal:

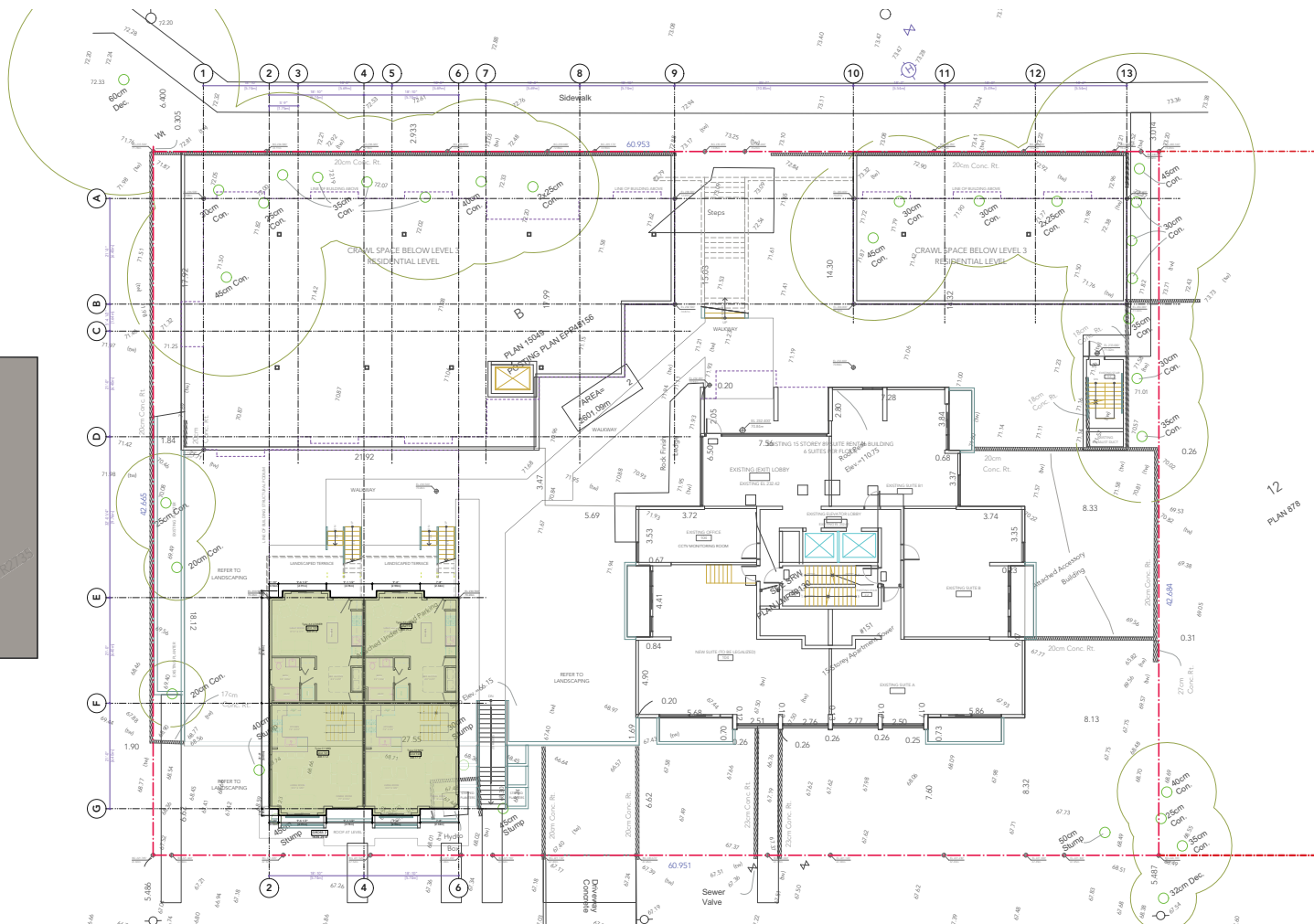
drawn:	date:
MCH/POH	August 2016
checked	
scale:	project no.:
as noted	424
sheet title:	
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sheet no.: **A6.05**  
sheet title: **UNIT PLANS**  
sheet no.: **A6.05**  
sheet title: **UNIT PLANS**  
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sheet title: **UNIT PLANS**

# RESIDENTIAL APARTMENT UNITS

**SUPERSEDED**



seal:P424\_plans.vwx

plotted: 17-10-24 11:34:03 AM

**LEVEL 2 PLAN - AREA OVERLAY**  
Scale: 3/32" = 1'-0"



issues:  
2017-10-25 Reissued for Reasoning

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project

Proposed Residential Inf  
151 East Keith  
North Vancouver BC  
**Starlight**  
Investments

**consultant:**

seal;

[illegible][illegible]

### LEVEL 3 PLAN (UPPER GROUND) - AREA OVERLAY

Scale:  $3/32" = 1'-0"$

**SUPERSEDED**

drawn:	date:
MCH/PDH	September 2017
checked:	

scale:	project no.:
as noted	424

sheet title:  
LEVEL 3 PLAN  
AREA OVERLAY

sheet no.:	
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## A8.03



plotted: 17-10-24 11:34:07 A



revisions:		
no.	date	description

issues:  
2017-10-25 Reissued for Reasoning

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Telephone: (604) 730-8100 | Web: [www.bha.ca](http://www.bha.ca)

**project:**

Proposed Residential Infill  
151 East Keith  
North Vancouver BC

**Starlight**  
Investments

consultant:

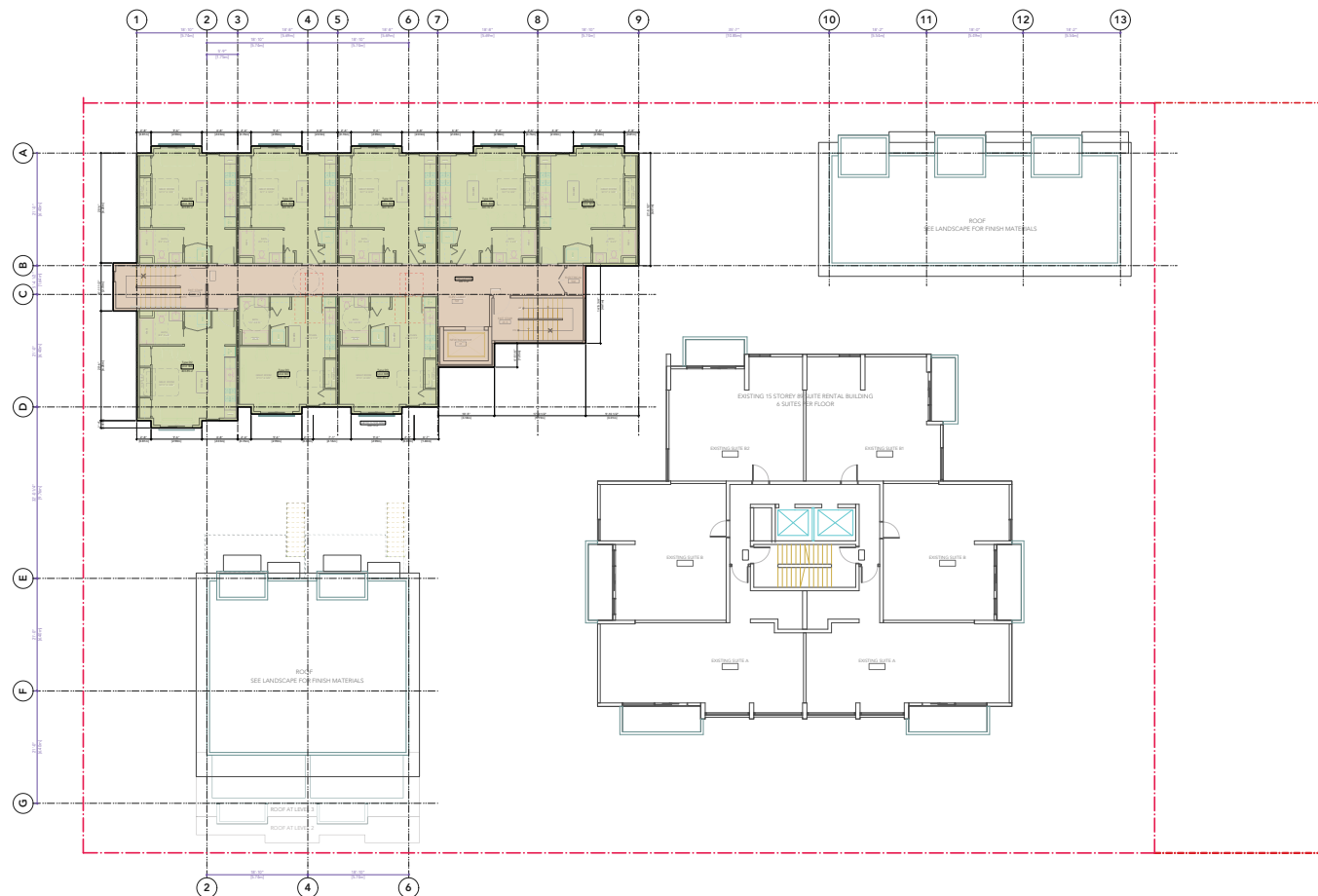
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drawn: MCH/PDH	date: September 2017
checked:	
scale: as noted	project no.: 424
<b>sheet title:</b> LEVEL 4 PLAN ARFA OVERLAY	

sheet no.:  
**A8.04**  
REV.2

Project Name	Project Number	Project Description	Start Date	End Date	Project Manager	Project Sponsor	Project Status	Project Phase	Project Budget	Project Cost	Project Profit	Project Margin	Project ROI
Project A	001	Project A Description	2023-01-01	2023-12-31	John Doe	Jane Smith	Completed	Phase 1	\$1,000,000	\$800,000	\$200,000	20%	100%
Project B	002	Project B Description	2023-01-01	2023-12-31	John Doe	Jane Smith	In Progress	Phase 2	\$1,000,000	\$700,000	\$300,000	30%	120%
Project C	003	Project C Description	2023-01-01	2023-12-31	John Doe	Jane Smith	On Hold	Phase 3	\$1,000,000	\$500,000	\$500,000	50%	150%
Project D	004	Project D Description	2023-01-01	2023-12-31	John Doe	Jane Smith	Cancelled	Phase 4	\$1,000,000	\$1,000,000	\$0	0%	0%
Project E	005	Project E Description	2023-01-01	2023-12-31	John Doe	Jane Smith	Completed	Phase 1	\$1,000,000	\$800,000	\$200,000	20%	100%
Project F	006	Project F Description	2023-01-01	2023-12-31	John Doe	Jane Smith	In Progress	Phase 2	\$1,000,000	\$700,000	\$300,000	30%	120%
Project G	007	Project G Description	2023-01-01	2023-12-31	John Doe	Jane Smith	On Hold	Phase 3	\$1,000,000	\$500,000	\$500,000	50%	150%
Project H	008	Project H Description	2023-01-01	2023-12-31	John Doe	Jane Smith	Cancelled	Phase 4	\$1,000,000	\$1,000,000	\$0	0%	0%
Project I	009	Project I Description	2023-01-01	2023-12-31	John Doe	Jane Smith	Completed	Phase 1	\$1,000,000	\$800,000	\$200,000	20%	100%
Project J	010	Project J Description	2023-01-01	2023-12-31	John Doe	Jane Smith	In Progress	Phase 2	\$1,000,000	\$700,000	\$300,000	30%	120%
Project K	011	Project K Description	2023-01-01	2023-12-31	John Doe	Jane Smith	On Hold	Phase 3	\$1,000,000	\$500,000	\$500,000	50%	150%
Project L	012	Project L Description	2023-01-01	2023-12-31	John Doe	Jane Smith	Cancelled	Phase 4	\$1,000,000	\$1,000,000	\$0	0%	0%
Project M	013	Project M Description	2023-01-01	2023-12-31	John Doe	Jane Smith	Completed	Phase 1	\$1,000,000	\$800,000	\$200,000	20%	100%
Project N	014	Project N Description	2023-01-01	2023-12-31	John Doe	Jane Smith	In Progress	Phase 2	\$1,000,000	\$700,000	\$300,000	30%	120%
Project O	015	Project O Description	2023-01-01	2023-12-31	John Doe	Jane Smith	On Hold	Phase 3	\$1,000,000	\$500,000	\$500,000	50%	150%
Project P	016	Project P Description	2023-01-01	2023-12-31	John Doe	Jane Smith	Cancelled	Phase 4	\$1,000,000	\$1,000,000	\$0	0%	0%
Project Q	017	Project Q Description	2023-01-01	2023-12-31	John Doe	Jane Smith	Completed	Phase 1	\$1,000,000	\$800,000	\$200,000	20%	100%
Project R	018	Project R Description	2023-01-01	2023-12-31	John Doe	Jane Smith	In Progress	Phase 2	\$1,000,000	\$700,000	\$300,000	30%	120%
Project S	019	Project S Description	2023-01-01	2023-12-31	John Doe	Jane Smith	On Hold	Phase 3	\$1,000,000	\$500,000	\$500,000	50%	150%
Project T	020	Project T Description	2023-01-01	2023-12-31	John Doe	Jane Smith	Cancelled	Phase 4	\$1,000,000	\$1,000,000	\$0	0%	0%
Project U	021	Project U Description	2023-01-01	2023-12-31	John Doe	Jane Smith	Completed	Phase 1	\$1,000,000	\$800,000	\$200,000	20%	100%
Project V	022	Project V Description	2023-01-01	2023-12-31	John Doe	Jane Smith	In Progress	Phase 2	\$1,000,000	\$700,000	\$300,000	30%	120%
Project W	023	Project W Description	2023-01-01	2023-12-31	John Doe	Jane Smith	On Hold	Phase 3	\$1,000,000	\$500,000	\$500,000	50%	150%
Project X	024	Project X Description	2023-01-01	2023-12-31	John Doe	Jane Smith	Cancelled	Phase 4	\$1,000,000	\$1,000,000	\$0	0%	0%
Project Y	025	Project Y Description	2023-01-01	2023-12-31	John Doe	Jane Smith	Completed	Phase 1	\$1,000,000	\$800,000	\$200,000	20%	100%
Project Z	026	Project Z Description	2023-01-01	2023-12-31	John Doe	Jane Smith	In Progress	Phase 2	\$1,000,000	\$700,000	\$300,000	30%	120%
Project AA	027	Project AA Description	2023-01-01	2023-12-31	John Doe	Jane Smith	On Hold	Phase 3	\$1,000,000	\$500,000	\$500,000	50%	150%
Project AB	028	Project AB Description	2023-01-01	2023-12-31	John Doe	Jane Smith	Cancelled	Phase 4	\$1,000,000	\$1,000,000	\$0	0%	0%
Project AC	029	Project AC Description	2023-01-01	2023-12-31	John Doe	Jane Smith	Completed	Phase 1	\$1,000,000	\$800,000	\$200,000	20%	100%
Project AD	030	Project AD Description	2023-01-01	2023-12-31	John Doe	Jane Smith	In Progress	Phase 2	\$1,000,000	\$700,000	\$300,000	30%	120%
Project AE	031	Project AE Description	2023-01-01	2023-12-31	John Doe	Jane Smith	On Hold	Phase 3	\$1,000,000	\$500,000	\$500,000	50%	150%
Project AF	032	Project AF Description	2023-01-01	2023-12-31	John Doe	Jane Smith	Cancelled	Phase 4	\$1,000,000	\$1,000,000	\$0	0%	0%
Project AG	033	Project AG Description	2023-01-01	2023-12-31	John Doe	Jane Smith	Completed	Phase 1	\$1,000,000	\$800,000	\$200,000	20%	100%
Project AH	034	Project AH Description	2023-01-01	2023-12-31	John Doe	Jane Smith	In Progress	Phase 2	\$1,000,000	\$700,000	\$300,000	30%	120%
Project AI	035	Project AI Description	2023-01-01	2023-12-31	John Doe	Jane Smith	On Hold	Phase 3	\$1,000,000	\$500,000	\$500,000	50%	150%
Project AJ	036	Project AJ Description	2023-01-01	2023-12-31	John Doe	Jane Smith	Cancelled	Phase 4	\$1,000,000	\$1,000,000	\$0	0%	0%
Project AK	037	Project AK Description	2023-01-01	2023-12-31	John Doe	Jane Smith	Completed	Phase 1	\$1,000,000	\$800,000	\$200,000	20%	100%
Project AL	038	Project AL Description	2023-01-01	2023-12-31	John Doe	Jane Smith	In Progress	Phase 2	\$1,000,000	\$700,000	\$300,000	30%	120%
Project AM	039	Project AM Description	2023-01-01	2023-12-31	John Doe	Jane Smith	On Hold	Phase 3	\$1,000,000	\$500,000	\$500,000	50%	150%
Project AN	040	Project AN Description	2023-01-01	2023-12-31	John Doe	Jane Smith	Cancelled	Phase 4	\$1,000,000	\$1,000,000	\$0	0%	0%
Project AO	041	Project AO Description	2023-01-01	2023-12-31	John Doe	Jane Smith	Completed	Phase 1	\$1,000,000	\$800,000	\$200,000	20%	100%
Project AP	042	Project AP Description	2023-01-01	2023-12-31	John Doe	Jane Smith	In Progress	Phase 2	\$1,000,000	\$700,000	\$300,000	30%	120%
Project AQ	043	Project AQ Description	2023-01-01	2023-12-31	John Doe	Jane Smith	On Hold	Phase 3	\$1,000,000	\$500,000	\$500,000	50%	150%
Project AR	044	Project AR Description	2023-01-01	2023-12-31	John Doe	Jane Smith	Cancelled	Phase 4	\$1,000,000	\$1,000,000	\$0	0%	0%
Project AS	045	Project AS Description	2023-01-01	2023-12-31	John Doe	Jane Smith	Completed	Phase 1	\$1,000,000	\$800,000	\$200,000	20%	100%
Project AT	046	Project AT Description	2023-01-01	2023-12-31	John Doe	Jane Smith	In Progress	Phase 2	\$1,000,000	\$700,000	\$300,000	30%	120%
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Project BA	053	Project BA Description	2023-01-01	2023-12-31	John Doe	Jane Smith	Completed	Phase 1	\$1,000,000	\$800,000	\$200,000	20%	100%
Project BB	054	Project BB Description	2023-01-01	2023-12-31	John Doe	Jane Smith	In Progress	Phase 2	\$1,000,000	\$700,000	\$300,000	30%	120%
Project BC	055	Project BC Description	2023-01-01	2023-12-31	John Doe	Jane Smith	On Hold	Phase 3	\$1,000,000	\$500,000	\$500,000	50%	150%
Project BD	056	Project BD Description	2023-01-01	2023-12-31	John Doe	Jane Smith	Cancelled	Phase 4	\$1,000,000	\$1,000,000	\$0	0%	0%
Project BE	057	Project BE Description	2023-01-01	2023-12-31	John Doe	Jane Smith	Completed	Phase 1	\$1,000,000	\$800,000	\$200,000	20%	100%
Project BF	058	Project BF Description	2023-01-01	2023-12-31	John Doe	Jane Smith	In Progress	Phase 2	\$1,000,000	\$700,000	\$300,000	30%	120%
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Project BH	060	Project BH Description	2023-01-01	2023-12-31	John Doe	Jane Smith	Cancelled	Phase 4	\$1,000,000	\$1,000,000	\$0	0%	0%
Project BI	061	Project BI Description	2023-01-01	2023-12-31	John Doe	Jane Smith	Completed	Phase 1	\$1,000,000	\$800,000	\$200,000	20%	100%
Project BJ	062	Project BJ Description	2023-01-01	2023-12-31	John Doe	Jane Smith	In Progress	Phase 2	\$1,000,000	\$700,000	\$300,000	30%	120%
Project BK	063	Project BK Description	2023-01-01	2023-12-31	John Doe	Jane Smith	On Hold	Phase 3	\$1,000,000	\$500,000	\$500,000	50%	150%
Project BL	064	Project BL Description	2023-01-01	2023-12-31	John Doe	Jane Smith	Cancelled	Phase 4	\$1,000,000	\$1,000,000	\$0	0%	0%
Project BM	065	Project BM Description	2023-01-01	2023-12-31	John Doe	Jane Smith	Completed	Phase 1	\$1,000,000	\$800,000	\$200,000	20%	100%
Project BN	066	Project BN Description	2023-01-01	2023-12-31	John Doe	Jane Smith	In Progress	Phase 2	\$1,000,000	\$700,000	\$300,000	30%	120%
Project BO	067	Project BO Description	2023-01-01	2023-12-31	John Doe	Jane Smith	On Hold	Phase 3	\$1,000,000	\$500,000	\$500,000	50%	150%
Project BP	068	Project BP Description	2023-01-01	2023-12-31	John Doe	Jane Smith	Cancelled	Phase 4	\$1,000,000	\$1,000,000	\$0	0%	0%
Project BQ	069	Project BQ Description	2023-01-01	2023-12-31	John Doe	Jane Smith	Completed	Phase 1	\$1,000,000	\$800,000	\$200,000	20%	100%
Project BR	070	Project BR Description	2023-01-01	2023-12-31	John Doe	Jane Smith	In Progress	Phase 2	\$1,000,000	\$700,000	\$300,000	30%	120%
Project BS	071	Project BS Description	2023-01-01	2023-12-31	John Doe	Jane Smith	On Hold	Phase 3	\$1,000,000	\$500,000	\$500,000	50%	150%
Project BT	072	Project BT Description	2023-01-01	2023-12-31	John Doe	Jane Smith	Cancelled	Phase 4	\$1,000,000	\$1,000,000	\$0	0%	0%
Project BU	073	Project BU Description	2023-01-01	2023-12-31	John Doe	Jane Smith	Completed	Phase 1	\$1,000,000	\$800,000	\$200,000	20%	100%
Project BV	074	Project BV Description	2023-01-01	2023-12-31	John Doe	Jane Smith	In Progress	Phase 2	\$1,000,000	\$700,000	\$300,000	30%	120%
Project BW	075	Project BW Description	2023-01-01	2023-12-31	John Doe	Jane Smith	On Hold	Phase 3	\$1,000,000	\$500,000	\$500,000	50%	150%
Project BX	076	Project BX Description	2023-01-01	2023-12-31	John Doe	Jane Smith	Cancelled	Phase 4	\$1,000,000	\$1,000,000	\$0	0%	0%
Project BY	077	Project BY Description	2023-01-01	2023-12-31	John Doe	Jane Smith	Completed	Phase 1	\$1,000,000	\$800,000	\$200,000	20%	100%
Project BZ	078	Project BZ Description	2023-01-01	2023-12-31	John Doe	Jane Smith	In Progress	Phase 2	\$1,000,000	\$700,000	\$300,000	30%	120%
Project CA	079	Project CA Description	2023-01-01	2023-12-31	John Doe	Jane Smith	On Hold	Phase 3	\$1,000,000	\$500,000	\$500,000	50%	150%
Project CB	080	Project CB Description	2023-01-01	2023-12-31	John Doe	Jane Smith	Cancelled	Phase 4	\$1,000,000	\$1,000,000	\$0	0%	0%
Project CC	081	Project CC Description	2023-01-01	2023-12-31	John Doe	Jane Smith	Completed	Phase 1	\$1,000,000	\$800,000	\$200,000	20%	100%
Project CD	082	Project CD Description	2023-01-01	2023-12-31	John Doe	Jane Smith	In Progress	Phase 2	\$1,000,000	\$700,000	\$300,000	30%	120%
Project CE	083	Project CE Description	2023-01-01	2023-12-31	John Doe	Jane Smith	On Hold	Phase 3	\$1,000,000	\$500,000	\$500,000	50%	150%
Project CF	084	Project CF Description	2023-01-01	2023-12-31	John Doe	Jane Smith	Cancelled	Phase 4	\$1,000,000	\$1,000,000	\$0	0%	0%
Project CG	085	Project CG Description	2023-01-01	2023-12-31	John Doe	Jane Smith	Completed	Phase 1	\$1,000,000	\$800,000	\$200,000	20%	100%
Project CH	086	Project CH Description	2023-01-01	2023-12-31	John Doe	Jane Smith	In Progress	Phase 2	\$1,000,000	\$700,000	\$300,000	30%	120%
Project CI	087	Project CI Description	2023-01-01	2023-12-31	John Doe	Jane Smith	On Hold	Phase 3	\$1,000,000	\$500,000	\$500,000	50%	150%
Project CJ	088	Project CJ Description	2023-01-01	2023-12-31	John Doe	Jane Smith	Cancelled	Phase 4	\$1,000,000	\$1,000,000	\$0	0%	0%
Project CK	089	Project CK Description	2023-01-01	2023-12-31	John Doe	Jane Smith	Completed	Phase 1	\$1,000,000	\$800,000	\$200,000	20%	100%
Project CL	090	Project CL Description	2023-01-01	2023-12-31	John Doe	Jane Smith	In Progress	Phase 2	\$1,000,000	\$700,000	\$300,000	30%</	

BUILDING 1										
UNITS LIST (not Subtotal)										
Unit	Area	Area SqFt	Volume	Volume CuYd	Weight	Weight Tons	Weight Tons	Weight Tons	Weight Tons	Weight Tons
1	225	22	225.00	22.00	22.00	0.00	0.00	0.00	0.00	0.00
2	225	22	225.00	22.00	22.00	0.00	0.00	0.00	0.00	0.00
3	225	22	225.00	22.00	22.00	0.00	0.00	0.00	0.00	0.00
4	225	22	225.00	22.00	22.00	0.00	0.00	0.00	0.00	0.00
5	225	22	225.00	22.00	22.00	0.00	0.00	0.00	0.00	0.00
6	225	22	225.00	22.00	22.00	0.00	0.00	0.00	0.00	0.00
7	225	22	225.00	22.00	22.00	0.00	0.00	0.00	0.00	0.00
8	225	22	225.00	22.00	22.00	0.00	0.00	0.00	0.00	0.00
9	225	22	225.00	22.00	22.00	0.00	0.00	0.00	0.00	0.00
10	225	22	225.00	22.00	22.00	0.00	0.00	0.00	0.00	0.00
11	225	22	225.00	22.00	22.00	0.00	0.00	0.00	0.00	0.00
12	225	22	225.00	22.00	22.00	0.00	0.00	0.00	0.00	0.00
13	225	22	225.00	22.00	22.00	0.00	0.00	0.00	0.00	0.00
14	225	22	225.00	22.00	22.00	0.00	0.00	0.00	0.00	0.00
15	225	22	225.00	22.00	22.00	0.00	0.00	0.00	0.00	0.00
16	225	22	225.00	22.00	22.00	0.00	0.00	0.00	0.00	0.00
17	225	22	225.00	22.00	22.00	0.00	0.00	0.00	0.00	0.00
18	225	22	225.00	22.00	22.00	0.00	0.00	0.00	0.00	0.00
19	225	22	225.00	22.00	22.00	0.00	0.00	0.00	0.00	0.00
20	225	22	225.00	22.00	22.00	0.00	0.00	0.00	0.00	0.00
21	225	22	225.00	22.00	22.00	0.00	0.00	0.00	0.00	0.00
22	225	22	225.00	22.00	22.00	0.00	0.00	0.00	0.00	0.00
23	225	22	225.00	22.00	22.00	0.00	0.00	0.00	0.00	0.00
24	225	22	225.00	22.00	22.00	0.00	0.00	0.00	0.00	0.00
25	225	22	225.00	22.00	22.00	0.00	0.00	0.00	0.00	0.00
26	225	22	225.00	22.00	22.00	0.00	0.00	0.00	0.00	0.00
27	225	22	225.00	22.00	22.00	0.00	0.00	0.00	0.00	0.00
28	225	22	225.00	22.00	22.00	0.00	0.00	0.00	0.00	0.00
29	225	22	225.00	22.00	22.00	0.00	0.00	0.00	0.00	0.00
30	225	22	225.00	22.00	22.00	0.00	0.00	0.00	0.00	0.00
31	225	22	225.00	22.00	22.00	0.00	0.00	0.00	0.00	0.00
32	225	22	225.00	22.00	22.00	0.00	0.00	0.00	0.00	0.00
33	225	22	225.00	22.00	22.00	0.00	0.00	0.00	0.00	0.00
34	225	22	225.00	22.00	22.00	0.00	0.00	0.00	0.00	0.00



**issues:**  
2017-10-25 Reissued for Rezoning

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**Burrowes Huggins Architects**  
Suite 205 - 1628 West 1st Avenue, Vancouver B.C.  
Telephone: (604) 730-8100 | Web: [www.bha.ca](http://www.bha.ca)

**project:**

Proposed Residential Infill  
151 East Keith  
North Vancouver BC  
**Starlight**  
Investments

consultant:

seal:

[illegible][illegible]

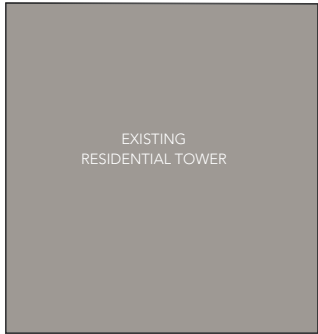
## LEVEL 5 PLAN - AREA OVERLAY

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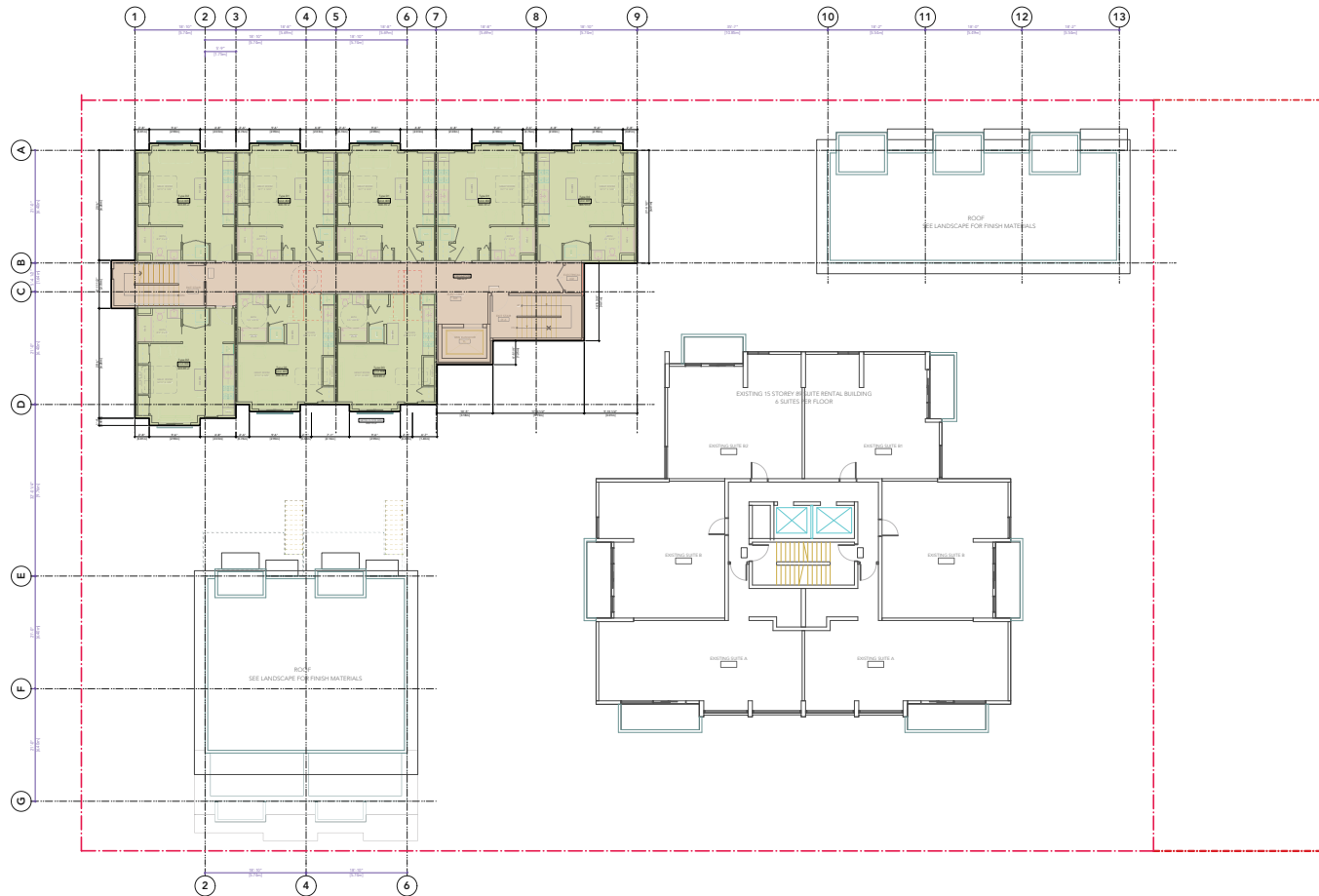
drawn: MCH/PDH	date: September 2017
checked	
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sheet no.:  
**A8.05**  
REV.2

**SUPERSEDED**

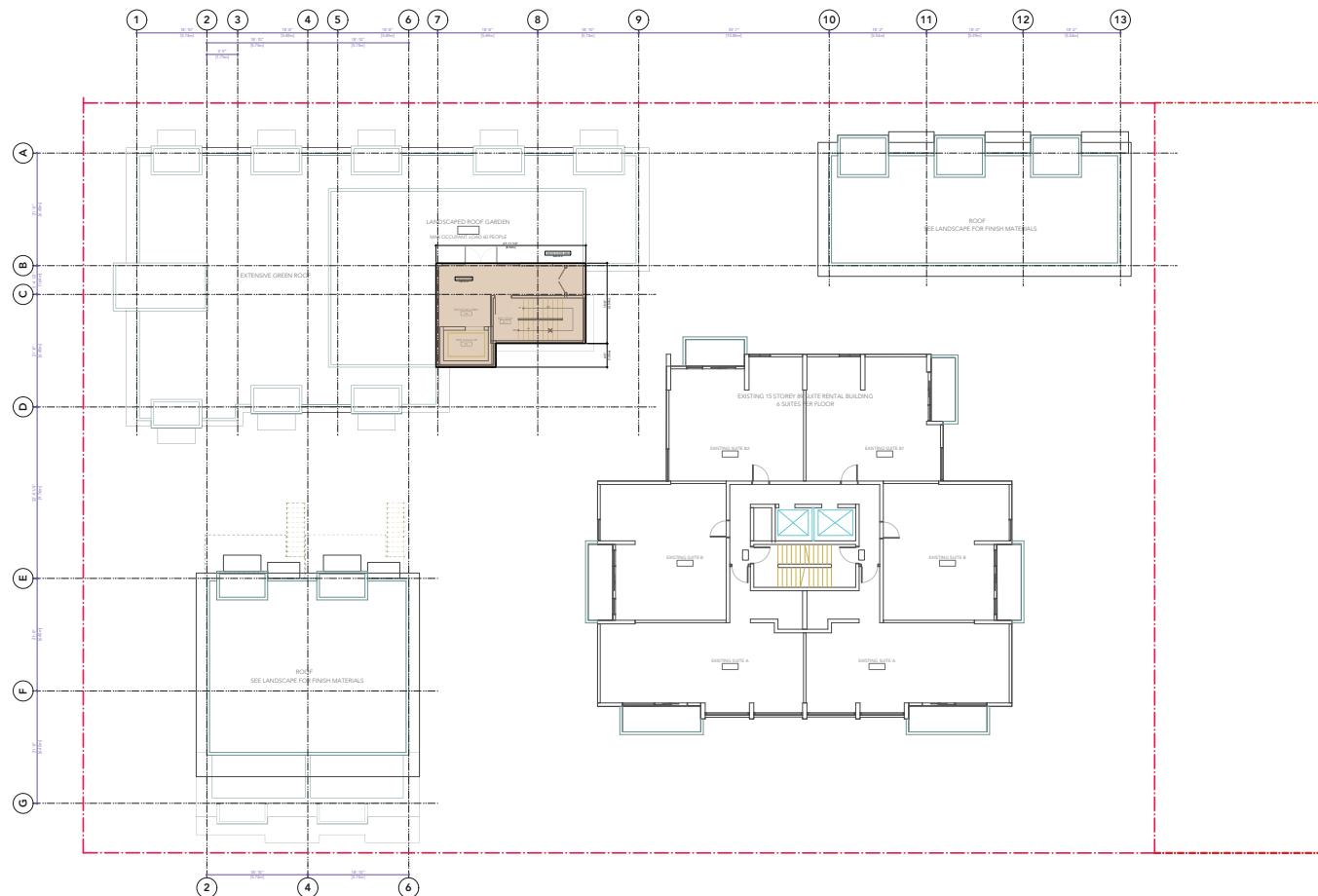


EXISTING  
RESIDENTIAL TOWER



Project Name:	RESIDENTIAL INFILL DEVELOPMENT			SP	NO	AKS1	NA	Units Per Block	Units Per Block	8 Jan 17
Project Number:	151 East Keith			2700 West 1st St	2000 West 1st St	2000 West 1st St	2000 West 1st St	2000 West 1st St	2000 West 1st St	2000 West 1st St
Client:	Burrowes Huggins Architects									
Drawn:	MCH/DPH									
Checked:	MCH/DPH									
Scale:	1/8" = 1'-0"									
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Total										





**issues:**  
2017-10-25      Reissued for Reasoning

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**Burrowes Huggins Architects**  
Suite 205 - 1628 West 1st Avenue, Vancouver B.C.  
Telephone: (604) 730-8100 | Web: [www.bha.ca](http://www.bha.ca)

**project:**

Proposed Residential Infill  
151 East Keith  
North Vancouver BC

**Starlight**  
Investments

consultant:

seal:

[illegible]

BUILDING 1										
UNIT 121 (DET) Subtotal										
			2019-20	2020-21	2021-22	2022-23	2023-24	2024-25	2025-26	2026-27
1	2020	20	2020-20	20.00	0.00	0.00	0.00	0.00	0.00	0.00
2	2021	21	2021-21	21.00	0.00	0.00	0.00	0.00	0.00	0.00
3	2022	22	2022-22	22.00	0.00	0.00	0.00	0.00	0.00	0.00
4	2023	23	2023-23	23.00	0.00	0.00	0.00	0.00	0.00	0.00
5	2024	24	2024-24	24.00	0.00	0.00	0.00	0.00	0.00	0.00
6	2025	25	2025-25	25.00	0.00	0.00	0.00	0.00	0.00	0.00
7	2026	26	2026-26	26.00	0.00	0.00	0.00	0.00	0.00	0.00
8	2027	27	2027-27	27.00	0.00	0.00	0.00	0.00	0.00	0.00
9	2028	28	2028-28	28.00	0.00	0.00	0.00	0.00	0.00	0.00
10	2029	29	2029-29	29.00	0.00	0.00	0.00	0.00	0.00	0.00
11	2030	30	2030-30	30.00	0.00	0.00	0.00	0.00	0.00	0.00
12	2031	31	2031-31	31.00	0.00	0.00	0.00	0.00	0.00	0.00
13	2032	32	2032-32	32.00	0.00	0.00	0.00	0.00	0.00	0.00
14	2033	33	2033-33	33.00	0.00	0.00	0.00	0.00	0.00	0.00
15	2034	34	2034-34	34.00	0.00	0.00	0.00	0.00	0.00	0.00
16	2035	35	2035-35	35.00	0.00	0.00	0.00	0.00	0.00	0.00
17	2036	36	2036-36	36.00	0.00	0.00	0.00	0.00	0.00	0.00
18	2037	37	2037-37	37.00	0.00	0.00	0.00	0.00	0.00	0.00
19	2038	38	2038-38	38.00	0.00	0.00	0.00	0.00	0.00	0.00
20	2039	39	2039-39	39.00	0.00	0.00	0.00	0.00	0.00	0.00
21	2040	40	2040-40	40.00	0.00	0.00	0.00	0.00	0.00	0.00
22	2041	41	2041-41	41.00	0.00	0.00	0.00	0.00	0.00	0.00
23	2042	42	2042-42	42.00	0.00	0.00	0.00	0.00	0.00	0.00
24	2043	43	2043-43	43.00	0.00	0.00	0.00	0.00	0.00	0.00
25	2044	44	2044-44	44.00	0.00	0.00	0.00	0.00	0.00	0.00
26	2045	45	2045-45	45.00	0.00	0.00	0.00	0.00	0.00	0.00
27	2046	46	2046-46	46.00	0.00	0.00	0.00	0.00	0.00	0.00
28	2047	47	2047-47	47.00	0.00	0.00	0.00	0.00	0.00	0.00
29	2048	48	2048-48	48.00	0.00	0.00	0.00	0.00	0.00	0.00
30	2049	49	2049-49	49.00	0.00	0.00	0.00	0.00	0.00	0.00
31	2050	50	2050-50	50.00	0.00	0.00	0.00	0.00	0.00	0.00
32	2051	51	2051-51	51.00	0.00	0.00	0.00	0.00	0.00	0.00
33	2052	52	2052-52	52.00	0.00	0.00	0.00	0.00	0.00	0.00
34	2053	53	2053-53	53.00	0.00	0.00	0.00	0.00	0.00	0.00
35	2054	54	2054-54	54.00	0.00	0.00	0.00	0.00	0.00	0.00
36	2055	55	2055-55	55.00	0.00	0.00	0.00	0.00	0.00	0.00
37	2056	56	2056-56	56.00	0.00	0.00	0.00	0.00	0.00	0.00
38	2057	57	2057-57	57.00	0.00	0.00	0.00	0.00	0.00	0.00
39	2058	58	2058-58	58.00	0.00	0.00	0.00	0.00	0.00	0.00
40	2059	59	2059-59	59.00	0.00	0.00	0.00	0.00	0.00	0.00
41	2060	60	2060-60	60.00	0.00	0.00	0.00	0.00	0.00	0.00
42	2061	61	2061-61	61.00	0.00	0.00	0.00	0.00	0.00	0.00
43	2062	62	2062-62	62.00	0.00	0.00	0.00	0.00	0.00	0.00
44	2063	63	2063-63	63.00	0.00	0.00	0.00	0.00	0.00	0.00
45	2064	64	2064-64	64.00	0.00	0.00	0.00	0.00	0.00	0.00
46	2065	65	2065-65	65.00	0.00	0.00	0.00	0.00	0.00	0.00
47	2066	66	2066-66	66.00	0.00	0.00	0.00	0.00	0.00	0.00
48	2067	67	2067-67	67.00	0.00	0.00	0.00	0.00	0.00	0.00
49	2068	68	2068-68	68.00	0.00	0.00	0.00	0.00	0.00	0.00
50	2069	69	2069-69	69.00	0.00	0.00	0.00	0.00	0.00	0.00
51	2070	70	2070-70	70.00	0.00	0.00	0.00	0.00	0.00	0.00
52	2071	71	2071-71	71.00	0.00	0.00	0.00	0.00	0.00	0.00
53	2072	72	2072-72	72.00	0.00	0.00	0.00	0.00	0.00	0.00
54	2073	73	2073-73	73.00	0.00	0.00	0.00	0.00	0.00	0.00
55	2074	74	2074-74	74.00	0.00	0.00	0.00	0.00	0.00	0.00
56	2075	75	2075-75	75.00	0.00	0.00	0.00	0.00	0.00	0.00
57	2076	76	2076-76	76.00	0.00	0.00	0.00	0.00	0.00	0.00
58	2077	77	2077-77	77.00	0.00	0.00	0.00	0.00	0.00	0.00
59	2078	78	2078-78	78.00	0.00	0.00	0.00	0.00	0.00	0.00
60	2079	79	2079-79	79.00	0.00	0.00	0.00	0.00	0.00	0.00
61	2080	80	2080-80	80.00	0.00	0.00	0.00	0.00	0.00	0.00
62	2081	81	2081-81	81.00	0.00	0.00	0.00	0.00	0.00	0.00
63	2082	82	2082-82	82.00	0.00	0.00	0.00	0.00	0.00	0.00
64	2083	83	2083-83	83.00	0.00	0.00	0.00	0.00	0.00	0.00
65	2084	84	2084-84	84.00	0.00	0.00	0.00	0.00	0.00	0.00
66	2085	85	2085-85	85.00	0.00	0.00	0.00	0.00	0.00	0.00
67	2086	86	2086-86	86.00	0.00	0.00	0.00	0.00	0.00	0.00
68	2087	87	2087-87	87.00	0.00	0.00	0.00	0.00	0.00	0.00
69	2088	88	2088-88	88.00	0.00	0.00	0.00	0.00	0.00	0.00
70	2089	89	2089-89	89.00	0.00	0.00	0.00	0.00	0.00	0.00
71	2090	90	2090-90	90.00	0.00	0.00	0.00	0.00	0.00	0.00
72	2091	91	2091-91	91.00	0.00	0.00	0.00	0.00	0.00	0.00
73	2092	92	2092-92	92.00	0.00	0.00	0.00	0.00	0.00	0.00
74	2093	93	2093-93	93.00	0.00	0.00	0.00	0.00	0.00	0.00
75	2094	94	2094-94	94.00	0.00	0.00	0.00	0.00	0.00	0.00
76	2095	95	2095-95	95.00	0.00	0.00	0.00	0.00	0.00	0.00
77	2096	96	2096-96	96.00	0.00	0.00	0.00	0.00	0.00	0.00
78	2097	97	2097-97	97.00	0.00	0.00	0.00	0.00	0.00	0.00
79	2098	98	2098-98	98.00	0.00	0.00	0.00	0.00	0.00	0.00
80	2099	99	2099-99	99.00	0.00	0.00	0.00	0.00	0.00	0.00
81	2100	100	2100-100	100.00	0.00	0.00	0.00	0.00	0.00	0.00
82	2101	101	2101-101	101.00	0.00	0.00	0.00	0.00	0.00	0.00
83	2102	102	2102-102	102.00	0.00	0.00	0.00	0.00	0.00	0.00
84	2103	103	2103-103	103.00	0.00	0.00	0.00	0.00	0.00	0.00
85	2104	104	2104-104	104.00	0.00	0.00	0.00	0.00	0.00	0.00
86	2105	105	2105-105	105.00	0.00	0.00	0.00	0.00	0.00	0.00
87	2106	106	2106-106	106.00	0.00	0.00	0.00	0.00	0.00	0.00
88	2107	107	2107-107	107.00	0.00	0.00	0.00	0.00	0.00	0.00
89	2108	108	2108-108	108.00	0.00	0.00	0.00	0.00	0.00	0.00
90	2109	109	2109-109	109.00	0.00	0.00	0.00	0.00	0.00	0.00
91	2110	110	2110-110	110.00	0.00	0.00	0.00	0.00	0.00	0.00
92	2111	111	2111-111	111.00	0.00	0.00	0.00	0.00	0.00	0.00
93	2112	112	2112-112	112.00	0.00	0.00	0.00	0.00	0.00	0.00
94	2113	113	2113-113	113.00	0.00	0.00	0.00	0.00	0.00	0.00
95	2114	114	2114-114	114.00	0.00	0.00	0.00	0.00	0.00	0.00
96	2115	115	2115-115	115.00	0.00	0.00	0.00	0.00	0.00	0.00
97	2116	116	2116-116	116.00	0.00	0.00	0.00	0.00	0.00	0.00
98	2117	117	2117-117	117.00	0.00	0.00	0.00	0.00	0.00	0.00
99	2118	118	2118-118	118.00	0.00	0.00	0.00	0.00	0.00	0.00
100	2119	119	2119-119	119.00	0.00	0.00	0.00	0.00	0.00	0.00
101	2120	120	2120-120	120.00	0.00	0.00	0.00	0.00	0.00	0.00
102	2121	121	2121-121	121.00	0.00	0.00	0.00	0.00	0.00	0.00
103	2122	122	2122-122	122.00	0.00	0.00	0.00	0.00	0.00	0.00
104	2123	123	2123-123	123.00	0.00	0.00	0.00	0.00	0.00	0.00
105	2124	124	2124-124	124.00	0.00	0.00	0.00	0.00	0.00	0.00
106	2125	125	2125-125	125.00	0.00	0.00	0.00	0.00	0.00	0.00
107	2126	126	2126-126	126.00	0.00	0.00	0.00	0.00	0.00	0.00
108	2127	127	2127-127	127.00	0.00	0.00	0.00	0.00	0.00	0.00
109	2128	128	2128-128	128.00	0.00	0.00	0.00	0.00	0.00	0.00
110	2129	129	2129-129	129.00	0.00	0.00	0.00	0.00	0.00	0.00
111	2130	130	2130-130	130.00	0.00	0.00	0.00	0.00	0.00	0.00
112	2131	131	2131-131	131.00	0.00	0.00	0.00	0.00	0.00	0.00
113	2132	132	2132-132	132.00	0.00	0.00	0.00	0.00	0.00	0.00
114	2133	133	2133-133	133.00	0.00	0.00	0.00	0.00	0.00	0.00
115	2134	134	2134-134	134.00	0.00	0.00	0.00	0.00	0.00	0.00
116	2135	135	2135-135	135.00	0.00	0.00	0.00	0.00	0.00	0.00
117	2136	136	2136-136	136.00	0.00	0.00	0.00	0.00	0.00	0.00
118	2137	137	2137-137	137.00	0.00					

### LEVEL 7 (ROOF) PLAN - AREA OVERLAY

Scale: 1/8" = 1'-0"

drawn: MCH/PDH	date: September 2017
checked	
scale: as noted	project no.: 424

sheet title:  
LEVEL 7 PLAN  
ARFA OVERLAY

sheet no.:  
**A8.07**  
REV.2



plotted: 17-10-24 11:34:24 AM

**SUPERSEDED**



1 East Keith Road Elevation  
Scale: nts



2 East Keith Road - Looking West  
Scale: nts



3 East Keith Road - Looking East  
Scale: nts

revisions:		
no.	date	description

issues:  
2016-11-22 Issued for Reasoning  
2017-10-25 Reissued for Reasoning

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**Burrowes Huggins Architects**  
Suite 205 - 1628 West 41st Avenue, Vancouver, B.C.  
Telephone: (604) 730-8100 | Web: www.bha.ca

project:  
Proposed Residential Infill  
151 East Keith  
North Vancouver BC  
**Starlight**  
Investments

consultant:

scale:

drawn:	date:
MCH/DM	October 2017
checked:	
scale:	project no.:
as noted	424
sheet title:	
3D Studies	

sheet no.:  
**A9.01**  
P&S: 3D Studies.vrx  
plotted: 17-10-24 10:47:44 AM

**SUPERSEDED**





1 East 6th Street  
Scale: nts



2 East 6th Street - Looking West  
Scale: nts



3 East 6th Street - Looking East  
Scale: nts

revisions:		
no.	date	description

issues:		
2016-11-22	Issued for Reasoning	
2017-10-25	Reissued for Reasoning	

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Telephone: (604) 735-8100 | Web: www.bha.ca

**project:**  
Proposed Residential Infill  
151 East Keith  
North Vancouver BC  
**Starlight**  
Investments

**consultant:**

**scale:**

drawn: MCH/DM	date: October 2017
checked:	
scale: as noted	project no.: 424
sheet title: 3D Studies	

sheet no.:  
**A9.02**  
P&S: 3D Studies.vrx  
plotted: 17-10-24 10:47:29 AM



**SUPERSEDED**





2 Birdseye - Looking Northeast



1 Birdseye - Looking East



3 Birdseye - Looking Southeast



4 Courtyard View

revisions:		
no.	date	description

issues:  
 2016-11-22 Issued for Reasoning  
 2017-10-25 Reissued for Reasoning

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 Telephone: (604) 750-8100 | Web: www.bha.ca

**project:**  
 Proposed Residential Infill  
 151 East Keith  
 North Vancouver, BC  
**Starlight**  
 Investments

**consultant:**

**scale:**

drawn:	date:
MCH/DM	October 2017
checked:	
scale:	project no.:
as noted	424
sheet title:	
3D Studies	

sheet no.:  
**A9.03**  
 P624-3D Studies.vrx  
 plotted: 17-10-24 10:48:09 AM

**SUPERSEDED**



# SHADOW STUDIES



June 21st - 10am



June 21st - 12pm



June 21st - 2pm



September 21st - 10am



September 21st - 12pm



September 21st - 2pm



December 21st - 10am



December 21st - 12pm



December 21st - 2pm



**SUPERSEDED**

revisions:		
no.	date	description

**issues:**  
 2016-11-22 Issued for Reasoning  
 2017-10-25 Reissued for Reasoning

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 Suite 205 - 1608 West 4th Avenue, Vancouver, B.C.  
 Telephone: (604) 735-8100 | Web: www.bha.ca

**project:**  
 Proposed Residential Infill  
 151 East Keith  
 North Vancouver BC  
**Starlight**  
 Investments

**consultant:**

seal:

drawn: MCH/DM	date: October 2016
checked	
scale: as noted	project no.: 424
sheet title: Shadow Studies	

sheet no.:  
**A9.04**  
 rev.:  
 plotted: 17-10-24 10:48:21 AM

# PROPOSED RESIDENTIAL INFILL DEVELOPMENT

## LANDSCAPE SET: RE-ISSUED FOR REZONING

### OCTOBER 23, 2017

#### PROJECT INFORMATION

**ARCHITECT:**  
BURROWES HUGGINS ARCHITECTS  
CONTACT NAME: MIKE HUGGINS  
205 - 1628 WEST 1ST AVENUE  
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PH: 604.730.8100

**LANDSCAPE ARCHITECT:**  
CONNECT LANDSCAPE ARCHITECTURE INC.  
CONTACT NAME: DAVID STOYKO  
2305 HEMLOCK STREET,  
VANCOUVER, BRITISH COLUMBIA, V6H 2V1  
EMAIL: DAVID@CONNECTLA.CA  
PH: 604.681.3303

#### LANDSCAPE DRAWING INDEX

DRAWING INDEX	
SHEET No.	SHEET NAME
L 0.0	COVER SHEET AND DRAWING LIST
L 1.1	LANDSCAPE CONCEPT PLAN
L 1.2	LANDSCAPE CONCEPT LIGHTING PLAN
L 2.1	PLANT MATERIALS / LANDSCAPE MATERIALS
L 2.2	LANDSCAPE PLANTING PLAN
L 2.3	TREE PROTECTION AND REMOVAL

#### GENERAL NOTES

ALL LANDSCAPE ARCHITECTURAL DRAWINGS IN THIS PACKAGE SHALL BE READ IN CONJUNCTION WITH ALL OTHER LANDSCAPE ARCHITECTURAL DRAWINGS, DETAILS, SPECIFICATIONS, AND OTHER CORRESPONDANCE THAT MAY BE ISSUED DURING THE COURSE OF THE CONTRACT.

IF A DISCREPANCY OCCURS BETWEEN THE DRAWINGS AND THE SPECIFICATIONS OR ANY OTHER DOCUMENT ASSOCIATED WITH THE PROJECT, THE CONFLICT SHALL BE REPORTED IN WRITING TO THE LANDSCAPE ARCHITECT TO OBTAIN CLARIFICATION AND APPROVAL BEFORE PROCEEDING WITH WORKS.

THE CONTRACTOR SHALL VISIT THE SITE TO VERIFY THE TRUE EXISTING CONDITIONS. ANY UNCLEAR ISSUES SHALL BE CLARIFIED WITH THE LANDSCAPE ARCHITECT. NO CLAIM SHALL BE ALLOWED FOR EXTRAS WHICH MAY ARISE THROUGH NEGLECT OF THIS ADVICE.

ALL EXISTING INFORMATION IS BASED ON AVAILABLE RECORDS AND SHALL NOT BE CONSTRUED TO BE COMPLETE OR ACCURATE.

LAYOUT OF HARDSCAPE, SITE FURNITURE, SOIL, PLANTING, AND ALL OTHER MATERIALS IS TO BE STAKED OUT AND APPROVED BY THE LANDSCAPE ARCHITECT PRIOR TO INSTALLATION.

ALL PLANTING SHALL BE IN ACCORDANCE WITH BC LANDSCAPE STANDARD, LATEST EDITION.

THE CONTRACTOR IS RESPONSIBLE FOR DETERMINING THE EXISTENCE, LOCATION, AND ELEVATION OF ALL UTILITIES AND CONCEALED STRUCTURES, AND IS RESPONSIBLE FOR NOTIFYING THE APPROPRIATE COMPANY, DEPARTMENT OR PERSON(S) OF ITS INTENTION TO CARRY OUT ITS OPERATIONS.

CONNECT LANDSCAPE ARCHITECTURE INC. DOES NOT GUARANTEE THE EXISTENCE, LOCATION, AND ELEVATION OF UTILITIES OR CONCEALED STRUCTURES AT THE PROJECT SITE.

FINAL SELECTION AND APPROVAL OF ALL STREET TREES TO BE DONE BY THE CITY OF NORTH VANCOUVER.

4	RE-ISSUED FOR REZONING	17-10-23
3	RE-ISSUED FOR REZONING	17-01-09
2	ISSUED FOR REZONING	16-11-22
1	ISSUED FOR REVIEW	16-10-20

#### REVISIONS



#### PROPOSED RESIDENTIAL INFILL 151 EAST KEITH ROAD

North Vancouver, British Columbia

Scale:	N/A
Drawn:	DS
Reviewed:	DS
Project No.	06-516

COVER PAGE  
DRAWING INDEX

L0.0

SUPERSEDED



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4 RE-ISSUED FOR REZONING 17-10-23

3 RE-ISSUED FOR REZONING 17-01-09

2 ISSUED FOR REZONING 16-11-22

1 ISSUED FOR REVIEW 16-10-20

REVISIONS



PROPOSED  
RESIDENTIAL INFILL  
151 EAST KEITH ROAD

North Vancouver, British Columbia

Scale: 3/32" = 1'-0"

Drawn: DS

Reviewed: DS

Project No. 06-516

LANDSCAPE CONCEPT  
PLAN

L1.1



SUPERSEDED



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#### SYMBOL



#### LEGEND LIGHTING

##### TYP. ENTRY DOOR LIGHT

REFER TO ARCHITECTURAL AND ELECTRICAL DWGS



##### WALL / STEP LIGHT

PEDESTRIAN IN-WALL LIGHTS FOR ILLUMINATING STAIRS AND PATHS  
SLV LIGHTING PEMA SQUARE  
30W CFL LAMP  
MODEL# 3230330U - BLACK COLOUR



##### BOLLARD LIGHTING

PEDESTRIAN LIGHT BOLLARD FOR PATH LIGHTING  
SLV LIGHTING F-POL BOLLARD  
33W LED LAMP  
MODEL #4231595U - ANTHRACITE COLOUR



##### UP-LIGHTING

AT SPECIMEN TREES AND MONUMENT LOCATIONS  
SLV LIGHTING SITRA 360 SPIKE (GX53 LED)  
5W LED 60 DEGREE LAMP  
MODEL# 3231525U - ANTHRACITE COLOUR



##### DOWN-LIGHTING

AT TRELLIS FEATURE  
SLV LIGHTING SITRA 360 WL (GX53 LED)  
5W LED 60 DEGREE LAMP  
MODEL# 3231535U - ANTHRACITE COLOUR



4 RE-ISSUED FOR REZONING 17-10-23

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1 ISSUED FOR REVIEW 16-10-20

#### REVISIONS



## PROPOSED RESIDENTIAL INFILL 151 EAST KEITH ROAD

North Vancouver, British Columbia

Scale: 3/32" = 1'-0"

Drawn: DS

Reviewed: DS

Project No. 06-516

## LANDSCAPE CONCEPT LIGHTING PLAN

L1.2

SUPERSEDED



PAVING



FEATURE AREAS



WALKWAYS



PATIOS

PLANTING CHARACTER

EVERGREEN AND SHADE PLANTS

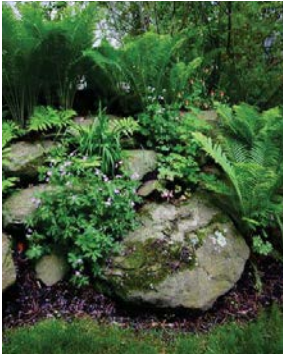


*Dicentra formosa*  
Bleeding Heart



*Polystichum munilum*  
Western Sword Fern

PACIFIC NORTHWEST PLANTS



*Achillea millefolium*  
Yarrow



*Blechnum spicatum*  
Deer Fern

PLANT MATERIAL LIST

QTY.	BOTANICAL NAME	COMMON NAME	SIZE	SPACING
<b>EXISTING TREES</b>				
Existing Tree To Remain: install Tree Protection Fencing around dripline perimeter				
<b>PROPOSED CONIFEROUS TREES</b>				
3	<i>Picea omorika</i>	Serbian Spruce	3m HT. B&B	As shown
3	<i>Picea orientalis aurea</i>	Golden Oriental Spruce	3m HT. B&B	As shown
<b>PROPOSED DECIDUOUS TREES</b>				
13	<i>Acer circinatum</i>	Vine Maple	6cm cal., B&B	As shown
8	<i>Acer griseum</i>	Paperbark Maple	5cm cal., B&B	As shown
4	<i>Acer palmatum 'Osakazuki'</i>	Japanese Maple	5cm cal., B&B	As shown
2	<i>Cercidiphyllum japonicum</i>	Katsura Tree	6cm cal., B&B	As shown
7	<i>Fraxinus penn. 'Prairie Spire'</i>	Prairie Spire Ash	6cm cal., B&B	As shown
5	<i>Liquidambar styraciflua 'Slender Silhouette'</i>	Slender Sweetgum	5cm cal., B&B	As shown
2	<i>Syringa reticulata</i>	Japanese Tree Lilac	5cm cal., B&B	As shown

<b>SHRUBS</b>				
160	<i>Berberis thunbergii 'Rose Glow'</i>	Rose Glow Japanese Barberry	No. 2 Pot	600mm o.c.
49	<i>Cornus stolonifera</i>	Red Twig Dogwood	No. 3 Pot	900mm o.c.
17	<i>Forsythia intermedia</i>	Yellow Forsythia	No. 2 pot	600mm o.c.
194	<i>Lavandula angustifolium 'Hidcote'</i>	Hidcote Blue English Lavender	No. 2 Pot	450mm o.c.
97	<i>Pieris japonica</i>	Japanese Pieris	No. 3 Pot	900mm o.c.
45	<i>Polystichum munilum</i>	Western Sword Fern	No. 1 pot	450mm o.c.
210	<i>Prunus laurocerasus 'Zabelina'</i>	Zabel's Laurel	No. 2 Pot	600mm o.c.
28	<i>Rhododendron 'Ken Janeck'</i>	Pink Rhododendron	No. 3 Pot	450mm o.c.
46	<i>Rhododendron 'Unique'</i>	Pink Rhododendron	No. 3 pot	600mm o.c.
55	<i>Sarcococca hookerana 'Humilis'</i>	Himalayan Sweetbox	No. 2 Pot	600mm o.c.
47	<i>Skimmia japonica</i>	Japanese Skimmia	No. 2 Pot	600mm o.c.
58	<i>Spiraea x bumalda 'Goldflame'</i>	Goldflame Spirea	No. 2 Pot	600mm o.c.
49	<i>Symphoricarpos alba</i>	Snowberry	No. 3 pot	600mm o.c.
255	<i>Taxus x media 'Hilli'</i>	Hicks Yew (male form)	No. 5 Pot	450mm o.c.

<b>GROUNDCOVERS</b>				
127	<i>Ceanothus griseus horizontalis</i>	Creeping Ceanothus	No. 1 pot	380mm o.c.
179	<i>Fragaria chiloensis</i>	Beach Strawberry	No. 1 Pot	380mm o.c.
171	<i>Geranium cantabrigiense</i>	Cranesbill Geranium	4" (10cm) Pot	450mm o.c.
9	<i>Pachysandra terminalis</i>	Japanese Spurge	4" (10cm) Pot	380mm o.c.
105	<i>Rubus calycinoides</i>	Emerald Carpet	4" (10cm) Pot	380mm o.c.
34	<i>Thymus praecox 'Purple Carpet'</i>	Purple Carpet Creeping Thyme	4" (10cm) Pot	300mm o.c.

<b>PERENNIALS</b>				
29	<i>Dicentra formosa</i>	Western Bleeding Heart	No. 1 Pot	450mm o.c.
33	<i>Echinacea purpurea</i>	Purple Coneflower	No. 1 Pot	450mm o.c.
12	<i>Hosta 'Blue Angel'</i>	Blue Angel Hosta	No. 1 Pot	600mm o.c.
29	<i>Iris tenax</i>	Tough-Leaf Iris	4" (10cm) Pot	380mm o.c.

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2 ISSUED FOR REZONING 16-11-22

1 ISSUED FOR REVIEW 16-10-20

REVISIONS

Starlight  
Investments

PROPOSED  
RESIDENTIAL INFILL  
151 EAST KEITH ROAD

North Vancouver, British Columbia

Scale: N/A

Drawn: DS

Reviewed: DS

Project No. 06-516

PLANT MATERIALS  
LANDSCAPE MATERIALS

L2.1

SUPERSEDED



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REVISIONS



PROPOSED  
RESIDENTIAL INFILL  
151 EAST KEITH ROAD

North Vancouver, British Columbia

Scale: 3/32" = 1'-0"

Drawn: DS

Reviewed: DS

Project No. 06-516

LANDSCAPE PLANTING  
PLAN

NOTE: SEE SHEET L2.1 FOR FULL PLANT LIST

L2.2

SUPERSEDED



CONNECT LANDSCAPE ARCHITECTURE INC. DOES NOT GUARANTEE THE EXISTENCE, LOCATION, AND ELEVATION OF UTILITIES AND / OR CONCEALED STRUCTURES AT THE PROJECT SITE.

THE CONTRACTOR IS RESPONSIBLE FOR DETERMINING THE EXISTENCE, LOCATION, AND ELEVATION OF ALL UTILITIES AND / OR CONCEALED STRUCTURES, AND IS RESPONSIBLE FOR NOTIFYING THE APPROPRIATE COMPANY, DEPARTMENT OR PERSON(S) OF ITS INTENTION TO CARRY OUT ITS OPERATIONS.

4 RE-ISSUED FOR REZONING 17-10-23

3 RE-ISSUED FOR REZONING 17-01-09

2 ISSUED FOR REZONING 16-11-22

1 ISSUED FOR REVIEW 16-10-20

REVISIONS



## PROPOSED RESIDENTIAL INFILL 151 EAST KEITH ROAD

North Vancouver, British Columbia

Scale: 3/32"=1'-0"

Drawn: DS

Reviewed: DS

Project No. 06-516

## TREE PROTECTION AND REMOVAL

L2.3

### TREE REPLACEMENT STATISTICS

DESCRIPTION	
EXISTING TREES TO BE RETAINED ON-SITE (1 TREE ON PROPERTY LINE)	3
EXISTING TREES TO BE RETAINED OFF-SITE (BOULEVARDS AND ADJACENT EDGES)	10
EXISTING TREES TO BE REMOVED	21
NEW REPLACEMENT TREES (APPROXIMATELY 2.24:1 REPLACEMENT)	47

### TREE RETENTION LEGEND

DESCRIPTION
EXISTING TREE TO BE RETAINED
EXISTING TREE TO BE REMOVED

TREE PROTECTION FENCING  
TREE PROTECTION FENCING TO BE INSTALLED PER CITY OF NORTH VANCOUVER STANDARDS AND INSPECTED BY LANDSCAPE ARCHITECT PRIOR TO CONSTRUCTION ACTIVITY

### SYMBOL



### TREE RETENTION NOTES

1. REFER ALSO TO SURVEY DRAWINGS. ANY EXISTING TREES NOT IDENTIFIED ON DRAWINGS ARE TO BE PROTECTED UNTIL CONSULTANTS CAN REVIEW AND DETERMINE THE CIRCUMSTANCES.
2. WHERE REQUIRED INSTALL TREE PROTECTION BARRIER AROUND ALL TREES TO BE PRESERVED TO CITY OF NORTH VANCOUVER STANDARDS AND SPECIFICATIONS. INFORM LANDSCAPE ARCHITECT WHEN ALL TREE BARRIERS HAVE BEEN INSTALLED. ARBORIST TO PROVIDE WRITTEN INSPECTION AND APPROVAL OF ALL BARRIERS PRIOR TO CONSTRUCTION ACTIVITY.
3. DO NOT REMOVE OR RELOCATE ANY TREE. ALL RETAINED TREES ARE TO BE PRUNED AND PROTECTED BY CITY OF NORTH VANCOUVER TREE PROTECTION BY-LAWS. CONTRACTOR TO CONTACT CITY ARBORIST AND LANDSCAPE ARCHITECT 48 HOURS PRIOR TO ANY CONSTRUCTION WORK AFFECTING THE DRIP LINE OF TREES.
4. DO NOT ALTER EXISTING GRADE OR STORE MATERIALS UNDER THE DRIP LINE OR WITHIN TREE PROTECTION ZONE. EXCAVATION WITHIN DRIP LINES OF TREES IS NOT PERMISSIBLE. TUNNEL UNDER AND AROUND ALL SIGNIFICANT ROOTS BY HAND DIGGING OR AIR SPADE. DO NOT CUT MAIN LATERAL ROOTS. FOR CARE AND PROTECTION OF ROOT CURTAIN SYSTEM, SEE IRC SPECIFICATIONS. TRENCHING FOR UTILITY CONNECTIONS TO BE COORDINATED WITH ENGINEERING DEPARTMENT TO ENSURE SAFE ROOT ZONES OF RETAINED TREES.

SUPERSEDED

## Public Hearing

151 East Keith Road

Zoning Amendment Bylaw No. 8618

Housing Agreement Bylaw No. 8622

**Presented June 17, 2019**

Planning and Development



## Introduction

For Council's consideration, a rezoning application at 151 East Keith Road to support the development of two infill buildings, to add 40 residential rental units to the property.





## Background

April 2018

- Council considered a proposal for three separate infill buildings:
  - 33 studio units;
  - 5 two bedroom townhouse units; and
  - 2 three bedroom townhouse units.
- Council did not grant 3<sup>rd</sup> Reading.

## Background

Current proposal

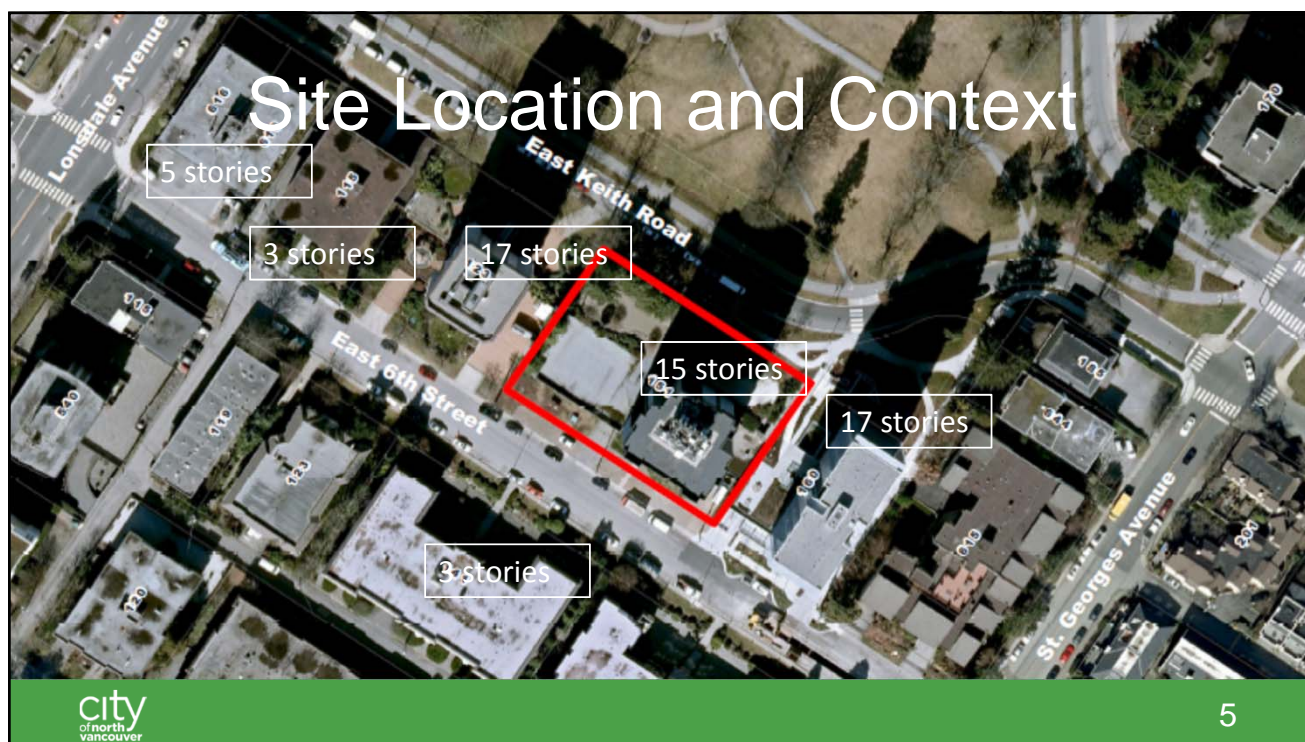
- Changes to the current proposal:

Removed the three unit townhouse building fronting the existing tower;

Added an access ramp from East Keith Road to the main entrance of the existing tower and proposed courtyard;

Added three units to the proposed Building 2 for a total of nine units; and

Adjusted the parkade to accommodate more secured bike areas and recycling and garbage enclosures.

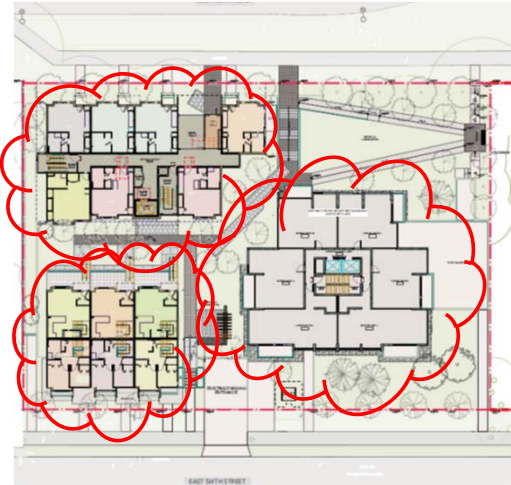


## Proposal – Site Design

The two proposed buildings are located to the west of the existing tower, and placed on top of the existing parkade structure.

Building 1 is a 4 storey apartment consisting of 31 studio suites, an indoor amenity area and a roof-top deck.

Building 2 is a 9 unit townhouse, consisting of 3 studio units and 6 three bedroom units.

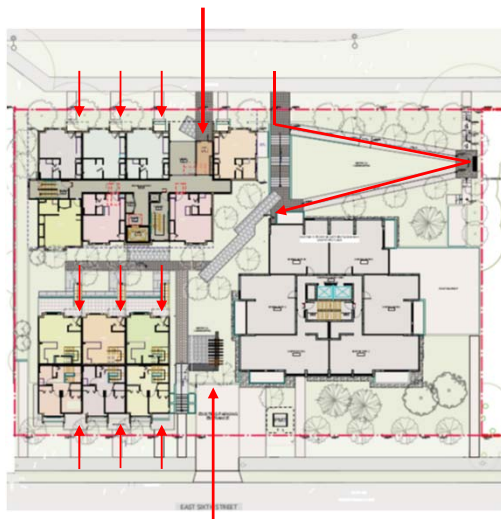


## Proposed Setbacks

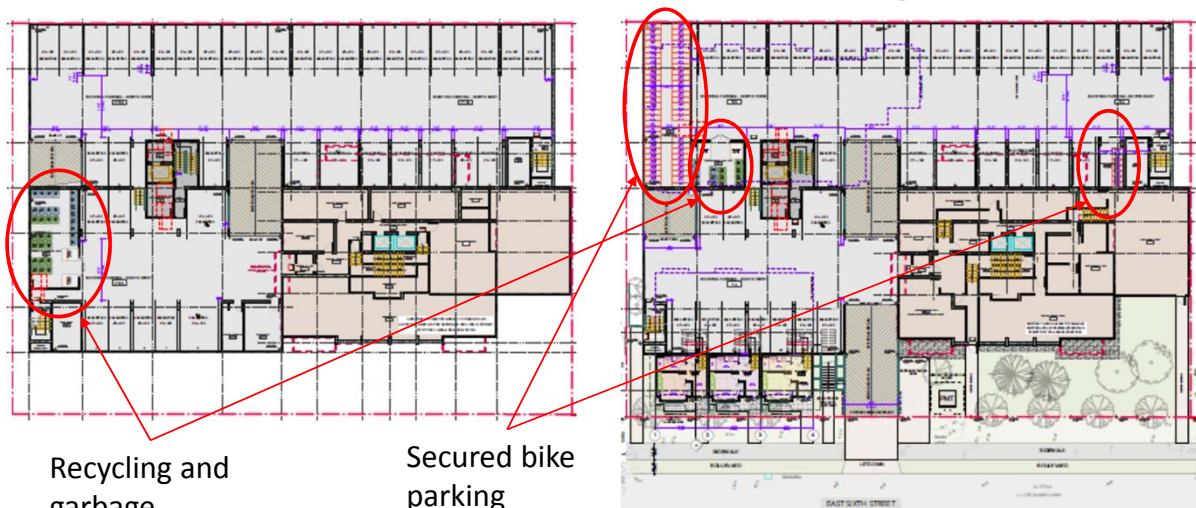




## Proposed Access



## Transportation and Parking



Recycling and  
garbage

Secured bike  
parking



## Official Community Plan

Limits density to 2.30 FSR with a potential bonus of 1.0.  
Maximum 3.30 FSR.

- Existing tower is 2.29 FSR.
- Infill buildings will add 0.88 FSR
- Total FSR = 3.17 FSR

Maximum Height is 46.0 metres.

- Tallest infill building will be 16.13 metres tall.

No amendment to the OCP is required.

## Zoning Bylaw

The proposal will require an amendment to the Zoning Bylaw. Using the current RH-1 Zone as a base, the following variances will be required:

Density increase to a maximum of 3.17 FSR.

Allow the minimum parking stall width to be reduced from 2.5 metres (8.2 feet) to 2.44 metres (8.0 feet).

Reduce building setbacks from 7.62 metres (25.0 feet) to suit the locations of the proposed infill buildings.

## Other Policies

### Housing Action Plan

All infill units will be secured as rental.  
Nine 3 Bedroom Units.

10% below market through SAFER.

### Active Design Guidelines

Indoor and rooftop amenity areas in the infill apartment for all tenants.  
Stairway dominant.

Common Courtyard.

### Sustainability

Using an existing site.  
Contribution to electrical charging station.

Connection to LEC.  
No tenant displacement.

## Urban Design

- Encourages pedestrian use.
- Provides a clear definition between public and private space.
- Helps frame Victoria Park.



## Conclusion

- Proposal does not require an amendment to the OCP.
- Requires a Zoning Bylaw amendment to support increase in density, adjusted building setbacks and parking stall size.
- Proposed infill buildings to be secured as rental.
- 10% of the infill units to be rented below market rates.
- Active Design Guidelines upheld.
- Most oppose the proposal with several in favor.

A photograph of a small, clear stream flowing over mossy rocks in a lush forest. The water is greenish, and the surrounding vegetation is dense and green. A green rectangular box is overlaid on the left side of the image, containing the text "Thank you." in white.

Thank you.

# 151 E. Keith Rd.

40 New Purpose-built Rental Homes in Lower Lonsdale



## Community Benefits



### Increase in Rental Housing Stock

The proposal seeks to add 40 new purpose-built rental homes on the site. 100% of the new homes will be secured as rental homes.



### No Tenant Displacement

There will be no loss of existing rental homes on the site. None of the existing tenants in the 89-suite building will be displaced.



### Support for Seniors

Starlight has established a partnership with a local service provider to provide below-market housing to vulnerable seniors.



### Design Excellence

The project received unanimous support from the City's Advisory Design Panel in February 2017.



### Maximize Existing Site

The development will maximize the use of the existing site, including the currently underutilized underground parkade, resulting in minimal disruption to current tenants and neighbours.



### Affordable Housing

In alignment with the Housing Action Plan, 10% of the homes will be offered at below-market rates. These units will be rented at SAFER rates (30% below CMHC average) and secured in perpetuity, exceeding City requirements.



### Housing for All

The development will include 34 studio units and six 3-bedroom units. 14 of the units will be 'adaptable units'.



### Alignment with City Vision

The project aligns with the City's Official Community Plan (OCP).



### Commitment to Sustainability

68 new bicycle parking spaces, a bike repair station, and e-bike charging infrastructure will be provided on-site. A \$25,000 contribution will help finance a new EV charging station in the City.



### On-Site Amenities

Communal amenities will be provided for all tenants including a new gym and rooftop deck. A new accessible pathway and landscaping will provide enhanced access to the existing building.



## Community (Re)Engagement

- Since May 2018, Starlight has reached out with a voluntary re-engagement program:
  - Open house for 151 E. Keith Rd. residents
  - Meetings with local stakeholders
  - Neighbourhood Open House (including full DIS notification)
  - Meeting with neighbours at 123 E. Keith Rd.

**NEIGHBOURHOOD OPEN HOUSE**

**PLEASE JOIN US**

**Date:** Wednesday, March 27, 2019  
**Time:** 5:00pm – 7:00pm (drop in)  
**Place:** The Friendship Room, St. Andrews Church  
**Address:** 1044 St. Georges Avenue, North Vancouver

**151 EAST KEITH ROAD PROPOSAL**

BHA Architecture, together with Starlight Investments, invite you to a Neighbourhood Open House to view their proposal to increase rental housing capacity at 151 East Keith Road through modest, infill development.

The proposed design will be a modest infill development, adding to the City's Urban Community Plan (UCP) and would be subject to the full and documented review for zoning and land use planning in the City. In alignment with the City's Strategic Plan for 2018-2025, 100% of all units in the development will be affordable rental units and 10% of all units in the development will be 3 bedroom units. The application seeks to secure the property at 151 East Keith Road to allow for the development of 40 units of purpose-built rental housing, which will be housed in two buildings: a 4-story apartment building and a 4-story townhome building, for a floor space ratio of 2.85. Existing features of the property will not be displaced as a result of this proposal.

Starlight Investments



View from 151 East Keith Road looking southwest

**Applicant Details**

**Matthew Steyer, Community Planner**  
 Urban Systems Ltd.  
 msteyer@urbansystems.ca  
 604.235.1701

This is not a City of North Vancouver event. This Open House is being hosted voluntarily by the applicant team.

## Design Considerations

- Sensitive to site and neighbourhood context
- Respecting our neighbours
- Design excellence
- Activate and connect the streets
- Design for livability



## Design Changes

1. Removal of street-facing townhouse building
2. Addition of three units to building 2
3. Accessible ramp provided from street level to main entrance of existing tower
4. Additional elevator stop added at plaza level (between buildings 1 and 2)



## Design Changes

5. Additional elevator stops added in parkade
6. Bicycle enhancements



## Landscape Design & Sustainability

- No excavation or greenfield development
- Refreshing a 40-year-old site
- Retaining 9 existing trees and adding 50 new trees
- Extensive green roof and rooftop amenity space
- Over 60% of site is open space
- Designed to meet Step Code 3 for energy efficiency
- Accessible, walkable, bikeable location



Thank  
you!

**Subject:** FW: 151 East Keith Road

**From:** Angela Girard (Councillor) <agirard@cnv.org>  
**Sent:** May-13-19 2:06 PM  
**To:** Don McDowall <>  
**Cc:** Karla Graham <kgraham@cnv.org>  
**Subject:** RE: 151 East Keith Road

Hello Mr. McDowall,

Thank you for your emailing me your comments and concerns regarding 151 East Keith Road.

I appreciate you taking the time to write in to me, and will consider your comments when making my decision on this proposed project.

Kind regards,

**Angela Girard**  
Councillor, City of North Vancouver  
E: [agirard@cnv.org](mailto:agirard@cnv.org) | C: 604-209-6927

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**From:** Don McDowall <>  
**Sent:** Sunday, May 12, 2019 7:19 PM  
**To:** Angela Girard (Councillor) <agirard@cnv.org>  
**Subject:** Councillor Angela Girard

Dear Councillor,

I am writing to you concerning the third attempt by Starlight Holdings to build thirty six rental units and an undefined number of Townhouses with reduced setback at 151 East Keith Road North Vancouver V7L 1V1.

In this particular case I am talking about a redevelopment to be built on top of an existing underground Parking Lot, adjacent to a fourteen storey high rise owned by the same Company who intend to reduce the size of their Parking Lot from 129 to 77 parking stalls including bicycle storage in an area of the City where parking is at a premium and virtually non existent after 6pm every night.

The Official Community Plan for the City of North Vancouver states that developers should respect open spaces and surrounding properties. Starlight's plan does not meet these criteria. With reduced set back the buildings will further encroach East Keith Road and Victoria Park to the North and 6th Avenue East to the South, not a very pretty sight when one building sticks out further than all the others as one drives by Victoria Park along East Keith Road or 6th St East.

Victoria Park on either side of Lonsdale Ave is without doubt the most beautiful park in the City of North Vancouver multitudes of people spend time there, enjoying the well looked after landscaping and trees and flowers having picnics, watching children playing, people sunbathing and take great pleasure in walking their dogs or just going for a stroll. Not only that it is the site of our Cenotaph and on Remembrance Day each year thousands of people attend the Remembrance Day Service "Lest



we Forget” celebrations to remember the men and woman who never returned after the Great wars as well as other groups throughout the year.

Rather than leave this pristine area well enough alone it appears that the City wishes to prioritise new building and do away with the sightlines looking down on the City of Vancouver in between the current high rises. This in itself does not undermine the integrity of the City’s Planning Department but begs the question does all this make sense?

You have no doubt heard about CBC News Posted May 5th 2019 where you would have seen that Developers and Landlords are now offering bonuses such as a free months rent or paying for move in expenses in an attempt to woo prospective tenants for the first time in six years. It is now officially a buyers market and there is now more supply than demand for rental space.

Not only that but rental prices are falling and it is believed that the empty home tax and speculation tax paired with the completion of so many new apartment buildings in the Greater Vancouver are driving the supply. Fair to say it is time for our Mayor and Council to review our building policies bearing in mind the expected drop in rental prices that are happening now. Surely it is not necessary to overbuild around a Place of Beauty like Victoria Park and shutting out the views of downtown Vancouver when there are other sites that one can focus on with more pleasing results and at the same time building bridges instead of dissatisfaction with the local inhabitants elsewhere.

Sincerely,  
Don McDowall

**Subject:** FW: 151 East Keith Road

**From:** McGregor Wark <>

**Sent:** June-01-19 1:24 PM

**To:** Karla Graham <[kgraham@cnv.org](mailto:kgraham@cnv.org)>

**Subject:** 151 East Keith Road

Mayor and Council,

My name is McGregor Wark and I'd like to lend my support for Starlight's [151 East Keith Road](#) project.

I think this project has a lot of merit for our community and I'd like to encourage you to formally approve the proposed rezoning at the public hearing. North Vancouver is in desperate need of more rental housing and projects like this are a fantastic way to deliver it in a way that doesn't relocate anyone. As someone who is lucky to have a secondary suite to rent, I know there is a high demand for new rental.

In the context of the site and it's surroundings, I also think the design is really well thought-out and will add to the existing streetscape on East Keith. Residents of this project would be really fortunate to live in this quality project.

All in all, I think 40 new rental apartments, with opportunities for seniors, on a site that will allow for infill development, is a fantastic way to go and I really hope its approved!

Appreciate your time,  
McGregor Wark  
455 East 6<sup>th</sup> Street, North Vancouver

**Subject:** FW: 151 East Keith Road

**From:** SPotter <>  
**Sent:** June-01-19 4:15 AM  
**To:** Karla Graham <[kgraham@cnv.org](mailto:kgraham@cnv.org)>  
**Subject:** 151 East Keith Road

Dear Mayor and Council,

I'm writing to share my support for the rental housing project proposed for 151 East Keith Road that was recently referred to a Public Hearing for a second time. As a North Shore resident, living off of Ridgeway and 6th, and someone aware of the housing pressures our community faces, I've been following this project for some time. I am pleased to see it back before you for consideration as I think this is a worthwhile development for the community.

This project is an absolute win for North Vancouver given it's location and the fact that there is already room on site to build. Projects that look for infill opportunities like this are a really great way to deliver more housing, without displacing anyone in the process. I especially appreciate that the applicant has also included below market rental options targeted at seniors looking to age in the community – this is a really important demographic that will benefit hugely as a result of this project.

I look forward to hearing of your decision. Thank you for the opportunity to comment.

Sincerely,  
Samantha Potter  
455 East 6<sup>th</sup> Street  
North Vancouver, BC

**Subject:** FW: 151 East Keith Road

**From:** T A <>

**Sent:** June-06-19 5:07 PM

**To:** Submissions <input@cnv.org>

**Subject:** 151 East Keith Road

Dear City Clerk,

REFERENCE: Zoning Bylaw, 1995, No. 6700, Amendment Bylaw, 2018, No. 8618 (CD-702)  
Housing Agreement Bylaw. 2018, No. 8622  
151 East Keith Road  
Public Hearing Monday, June 17, 2019 at 7:00PM  
Council Chamber, City hall  
141 West 14<sup>th</sup> Street, North Vancouver

As a senior tenant, for the last two years, at suite 905 – 151 East Keith Road, North Vancouver, V7L 4M3, where we pay monthly for one bedroom suite \$1,876.46 CND including garage (the landlord already recently increased 2%), we consider we have the right of tranquility, and peaceful life and environment we have not had for the last year.

**So far, during the last year and the current year we have had always renovations in all our suits around ours, causing tremendous noise every day, drilling, hammering and bumping and sometimes also on Saturdays.**

On top of that, the loses we have had in our suite flooding due to the old age of the heating system pipes, and also the leaking during rainy days in the garages, for which we pay \$60 CND a month, and also in our storage locker, which we have been changing twice for flooding, and no matter the problem persists and the landlord/management recognized only 50% of our loses, still, today June 6, 2019. administration has not been able to resolve the issue of flooding and leaking in storage area we were assigned.

**Last but not least, we had recently two days without fire alarm system, and the landlord/management had to pay for a security guard on site, and we have had enough with the noise caused by the construction of a new building at 161 Keith Road East, besides ours.**

All the above, in order to understand the following: if they are not been able to resolve yet the current issues and problems in this more than 42 years old building , why they are trying to build three more located at the main façade of the building?

Also several questions arise to me:

1. How is going to be the entrance for tenants to our building with a big construction projects obstructing our main entrance?
2. How is going to be the level of noise caused with this 2 infill residential buildings construction added to the one we have already indoors due to the suites renovations?



3. How is going to be deteriorated and obstructed the road and Victoria Park located in front of our rented building? Already damaged due to the construction of the 161 East Keith Road new building.
4. How this project will obstruct the traffic, when we already have frequently in front of our building heavy traffic because the bottle neck we continuously face in North Vancouver?

**Honestly, this is not fair to any tenant in this building, when we pay high rent, and so far for more than two years we have been facing hell no matter we pay our rent on time.**

Regards,  
Tomas Alba  
905-151 Keith Road East  
North Vancouver, V7L 4M3

**Subject:** FW: proposal for 151 East Keith Road

-----Original Message-----

From: szabo  
Sent: June-07-19 7:57 AM  
To: Karla Graham <kgraham@cnv.org>  
Subject: proposal for 151 East Keith Road

Dear Council,

I am totally appalled that this proposal is before Council ...so many things wrong with it. However, if approved, it will close in our lovely Victoria Park which is such a jewel in our City. Do let us keep this openness with its nature that fits so well with our beautiful Park. Please carefully consider the implications of allowing encroachment and building concrete walls instead of trees and bushes.

Thank you.

Judith and Kalman Szabo  
1002-140 East Keith Road  
North Vancouver

**Subject:** FW: 151 east keith road

**From:** Fredrick Pascal < >  
**Sent:** June-07-19 9:28 PM  
**To:** Submissions <input@cnv.org>  
**Subject:** 151 east keith road

What can I say CNV spends time and our money establishing building guidelines like FSR, building height, setbacks etc and here we have another developement that ignores them and yet CNV allows them to be heard. Enough of this.

This developemnt looks like David and Goliath. Sandwich between 2 towers. How can this be attractive.

There is absolutely no parking at all in and around east 6th residences. I am having to find parking along lonsdale, St Georges, east 5th. With this developemnt I'll be parking near the quay.

More construction Yeah, Can hardly wait for more dust, construction noise, street closures.

Buildings are too close together, which means a fire hazard.

Again treees are being cut down thanks to CNV. Planting new trees in lieu does not work, as they take up to 20- 30 years to develope into having the ability to clean our air.

In other words please do not let this development proceed. Do not follow in the shoes of the previous mayor who seems to enjoy increasing our population faster than improving our infastructure.

Thanks for listening. But time will tell I guess.

Fred Pascal  
104-123 east 6th street

**Subject:** FW: 151 East Keith Road

**From:** David Everett < >

**Sent:** June-08-19 5:22 PM

**To:** Submissions <input@cnv.org>; David Johnson <djohnson@cnv.org>

**Subject:** Zoning Bylaw, 1995, No. 6700, Amendment Bylaw, 2018, No. 8618 (CD-702)/Housing Agreement Bylaw, 2018, No.8622

Dear David Johnson, Development Planner/North Vancouver City Clerk

I have lived, and am currently living, in the area bounded by Lonsdale Avenue and St. Georges Avenue in the 100 block East 6th Street almost continually since 1972. I have gradually seen this neighborhood be transformed from a quiet low-density area to a noisy, difficult to find street parking hub of activity. The residents of this area have recently recovered from a two-year construction disruption of a high-rise rental building next door and East of 151 East Keith Road. Your proposal of adding another estimated two years of construction to this area will not bode well for the current residents.

**\* The estimated 70 vehicles parked in the undercover parking garage located under the proposed construction zone will have to be displaced for the period of construction. Where will the displaced vehicles be relocated?**

**Parking on the 100 block of East 6th street is already at a premium.**

**\* How will the current residents navigate the 100 block of East 6th Street while sharing the street with the multitude of construction related vehicles and also be impeded by the necessary digging of the street to allow additional utilities installation?**

**\* The noise resulting from the construction activity will add to the stress of residents after just recovering from the construction disruption of the high-rise rental building next door to the proposed construction zone.**

**\* Where will the additional vehicles resulting from the completion of construction be parked?**

I respectfully request that you vote down the proposed construction for the reasons I have mentioned.

Sincerely,

David Everett  
#209 - 141 East 6th Street  
North Vancouver BC V7L 1P1



**Subject:** FW: 151 East Keith Road INFILL Rezoning Application

**From:** Audrey Brown <>  
**Sent:** June-09-19 2:02 PM  
**To:** Karla Graham <kgraham@cnv.org>  
**Subject:** 151 East Keith Road INFILL Rezoning Application

To the Mayor and Council:

I am contacting you to voice my disagreement with the above mentioned application and I am urging you to vote against it.

Reasons:

- the "street wall" as described by our city planner, will NOT "significantly improve the pedestrian experience"
- a building will never improve ones view over a garden or trees.
- rezoning this property to accommodate a developer/owner is the "thin edge of the wedge" to future requests.
- this company, Starlight, owns the property immediately east of where I live. I fear that will be the next application.
- traffic flow on upper and lower East Keith is already very busy
- the "street wall" on the north side of the property will darken the area and lessen the sunlight to the park, thus closing in the existing openness.
- offering 4 studio units at slightly reduced rates and utilizing the SAFER program (taxpayer supported senior housing) is not a reasonable tradeoff

On a positive note:

- building smaller rental units along the west side of the building will not inhibit the park, however they should be within the current setback bylaws
- we all know that affordable housing is needed but to what end?

Audrey Brown – Owner  
#1201 – 140 East Keith Road, North Vancouver, B.C. V7L 4M9

**Subject:** FW: Rezoning Application for 151 East Keith Road submitted by Starlight Investments.

**From:** [wayne collett](mailto:wayne.collett@cnv.org)

**Sent:** May 17, 2019 2:56 PM

**To:** 'lbuchanan@cnv.org' <[lbuchanan@cnv.org](mailto:lbuchanan@cnv.org)>; 'hback@cnv.org' <[hback@cnv.org](mailto:hback@cnv.org)>; 'dbell@cnv.org' <[dbell@cnv.org](mailto:dbell@cnv.org)>; 'agirard@cnv.org' <[agirard@cnv.org](mailto:agirard@cnv.org)>; 'thu@cnv.org' <[thu@cnv.org](mailto:thu@cnv.org)>; 'jmcilroy@cnv.org' <[jmcilroy@cnv.org](mailto:jmcilroy@cnv.org)>; 'tvalente@cnv.org' <[tvalente@cnv.org](mailto:tvalente@cnv.org)>

**Cc:** 'David Johnson' <[djohnson@cnv.org](mailto:djohnson@cnv.org)>

**Subject:** Rezoning Application for 151 East Keith Road submitted by Starlight Investments.

**Dear Mayor Buchanan and City of North Vancouver Council Members**

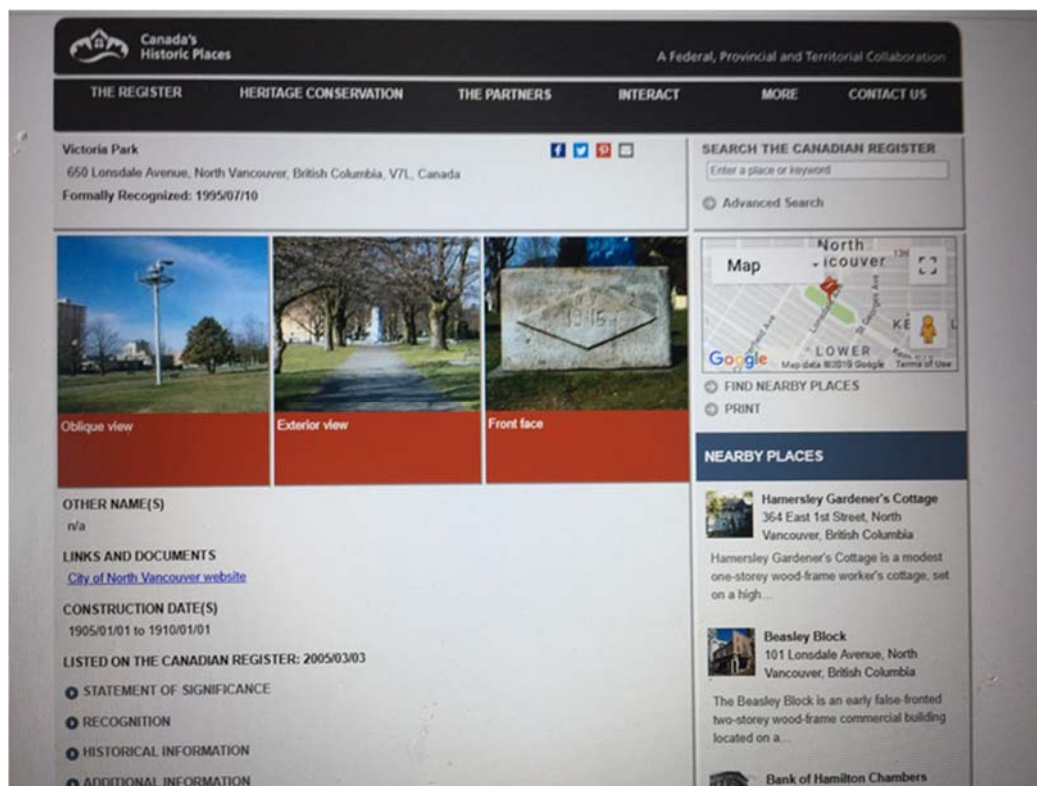
**Re: The Charm and Beauty of Victoria Park and Its Surrounding Neighbourhood**

Victoria Park is formally recognized in The Canadian Registry of Historical Places.

The Canadian Registry is a Federal, Provincial and Territorial collaboration which recognizes the contribution historic places make to communities.

The Statement of Significance is reported upon as follows in The Canadian Registry.

1. Description of Historic Place
2. Heritage Value
3. Character-Defining Elements



#### DESCRIPTION OF HISTORIC PLACE

Victoria Park is located on and bisected by Lonsdale Avenue at Keith Road and now surrounded by a high-density residential area. This urban park is 3.9 hectares in size and includes a lawn and trees, paths, a cenotaph, a granite horse trough, and a Cold War air raid siren.

#### HERITAGE VALUE

The heritage value of Victoria Park is associated with its role as the first and central component of a rectilinear system of boulevards and parks known as North Vancouver's "Green Necklace." This is a grouping of parks, running from Grand Boulevard to Moody Park, that includes Victoria Park, Mahon Park and Ottawa Gardens. The "necklace" is a rough semicircular chain of green spaces threading through suburban and commercial neighbourhoods on some of the higher elevations of land in the city.

The heritage value of Victoria Park is associated with its role as an important green space in the community for nearly one hundred years. As an active civic park, Victoria Park is a landmark and important green space for residents of the North Shore. Surrounded by major north-south and east-west arterial streets, the park is a distinct and recognisable feature that divides Lower Lonsdale from Central Lonsdale. The park is further valued as witness to annual Remembrance Day ceremonies, that honour the citizens of North Vancouver who have lost their lives in war.

Victoria Park is also valued for its picturesque design. The property was donated to the City of North Vancouver by A. St. George Hamersley, Isabella Maud Hamersley, and the North Vancouver Land and Improvement Company in 1905. Local residents began the task of clearing the land, which was later completed by a private contractor. By 1910 it was laid out as a picturesque promenade park with formal paths lined with traditional European tree species, including black locusts, horse chestnuts, birch, copper beeches, English hawthorns and big leaf maples.

There are three notable structures in Victoria Park - the 1915 horse trough, the 1923 cenotaph and the Cold War air raid siren. The horse trough is located in Victoria Park West. Carved from a single piece of grey granite, it was used by horses pulling loads up Lonsdale Avenue; its location on the left side of the road is a reminder that until 1922 traffic in B.C. adhered to the British traffic tradition. The cenotaph in Victoria Park East, designed by architects Blackadder and MacKay, is an important and historic local landmark erected to honour the citizens of the City and District of North Vancouver who gave their lives during the First World War, and was later re-dedicated to commemorate the fallen of both the Boer and Second World Wars. The air raid siren is a reminder of the fear of war in North America during the Cold War, and it remains a conspicuous landmark.

The Beasley Block is an early false-front two-storey wood-frame commercial building located on a...



**Bank of Hamilton Chambers**  
92 Lonsdale Avenue, North Vancouver, British Columbia

The Bank of Hamilton Chambers is a storey plus basement brick and sandstone building of the early Edwardian era...

landmark erected to honour the citizens of the City and District of North Vancouver who gave their lives during the First World War, and was later re-dedicated to commemorate the fallen of both the Boer and Second World Wars. The air raid siren is a reminder of the fear of war in North America during the Cold War, and it remains a conspicuous landmark.

Source: Heritage Planning File, City of North Vancouver

#### CHARACTER-DEFINING ELEMENTS

Key elements that define the heritage character of Victoria Park include its:

- location, bisected by Lonsdale Avenue and encircled by Keith Road on north and south sides
- setting within a high density residential neighbourhood
- open spatial qualities
- radial configuration of pathways leading from the cenotaph
- mature specimen trees and other plantings
- tall tree-lined pathways
- form, scale, massing and central location of the cenotaph
- granite horse trough
- Cold War air raid siren

#### 1 RECOGNITION

#### 1 HISTORICAL INFORMATION

I would emphasise two of the comments in the Character- Defining Elements section. Namely, "setting within a high density residential neighbourhood" and "open spatial qualities" because these two observations would usually be mutually exclusive.

Achieving these two objectives is what has created the charm and beauty of Victoria Park and its surrounding neighbourhood.



How was this accomplished?

In a nutshell, by constructing high rise buildings with generous property line setbacks. The construction of high buildings created the “setting within a high density residential neighbourhood” and the design principle using generous property line setbacks created the “open spatial qualities”.

Attached are several pictures which show the result of applying these two planning and design principles for the Victoria Park neighbourhood.













This application fails to meet the design principles which have been responsible for creating the charm and beauty of the Victoria Park Neighbourhood.

The most offensive request for approval is the radical reduction of property line setbacks both for the Victoria Park street setback and the western property line setback.

Council should not approve these setbacks which totally ignore, and will destroy, the charm and beauty of Victoria Park and surrounding neighbourhood.

Infill projects must respect neighbourhood values and characteristics.

Should this application be approved, it will set a precedent for a couple of other properties adjoining Victoria Park and I am sure that the owners of these properties are watching the consideration of this application very closely.

This project, if approved, is a developer's dream as no additional costs will be incurred for land, parking or amenities. Additionally, the developer will not incur any financial expenses for relocating

tenants and most significantly, will not suffer any rental revenue reductions during the construction period.

What will enable this developers dream project to become a reality?

Councils' approval of the virtual disappearance of property line set backs which have been responsible for creating the charm and beauty of Victoria Park and neighbourhood.

This is not a NIMBY argument.

The charm and beauty of Victoria Park and neighbourhood are not exclusive benefits to only the surrounding residents but are enjoyed by all visitors to the park.

Victoria Park is a gemstone in the City of North Vancouver's Green Necklace bicycle and walking trail system and is used by many from other areas in the city.

Annually the Remembrance Day Ceremony bring crowds to Victoria Park as does the Slide the City summer weekend program.

This project, if approved, will destroy the values created for Victoria Park users and neighbourhood. These values have been created by past wisdom and vision applied to planning and development decisions.

Please recognize, respect and sustain this wisdom and vision by not approving this application which has been rejected by council on two previous occasions.

Respectfully submitted by:  
Wayne Collett  
#1501 – 123 Keith Road East  
North Vancouver

Cc David Johnson, who I kindly request to forward a copy of this submission to all members of the Advisory Design Panel given the joint declaration of Victoria Park as a Canadian Historic Place.



**Subject:** FW: 151 East Keith Road

-----Original Message-----

From: Mike Holliday < >  
Sent: June-10-19 6:56 PM  
To: Karla Graham <kgraham@cnv.org>  
Subject: 151 East Keith Road

To the Mayor and council,

I wish to register my objection to the proposed infill rezoning application and I am requesting that you please vote against it.

The green space as home to wildlife, trees, shrubs and greenery doesn't need further encroachment.

A "street wall" is a horrible idea!

The park will lose its present openness. Instead it will feel closed-in and will reduce sunlight. This is not a beneficial step.

I think this sets a precedent that will be hard to stop later on. There are several older affordable rentals around the park and the developers will surely look at those for tear down opportunities next. This will just be the beginning.

Regards  
Mike Holliday  
Resident 1403-140 Keith E. Road  
North Vancouver BC V7L 4M9

**Subject:** FW: 151 East Keith Road - Infill Rezoning Application

**From:** kcorson  
**Sent:** June-11-19 9:47 AM  
**To:** Karla Graham <kgraham@cnv.org>  
**Subject:** 151 East Keith Road - Infill Rezoning Application

Hi,

I understand that if I have comments on this proposal I send them to you and you forward them to members of city council. If this is not the case please let me know how to direct the e-mail.

I wish to make a few comments for council's consideration in relation to my concerns about the above noted application

I note that the application is a slightly revised proposal to one that was rejected earlier. Having looked at the newest proposal in relation to the old I note that, though there have been some refinements, the revised proposal continues to ignore some major issues most of which were identified when the proposal was first rejected by council:

Some of the comments are related to the proposal versus the Official Community Plan ("OCP"). I would like to believe that the time and effort spent developing the OCP was with the intention of setting out basic principles and a cohesive framework of development. One of the main guiding principles is to create a liveable community recognizing the unique nature of established neighborhoods and the natural beauty of the North Shore. This particular project "fails" in this regard.

First, the new buildings will mainly be built in areas of the property that are currently within the setback allowances specified on the area under the OCP. This alone should be enough to disqualify the project. Ignoring lot line setbacks will change the nature of the surrounding area.

Secondly, the project's height (five stories plus service floor) and the fact that it will, for all practical purposes, be built to the edge of Keith Road (less a small allowance for sidewalk and a fringe of grass) will have a significant impact on the nature of Victoria Park. Strictly speaking it will not physically encroach on the park as a structure but it will remove the "openness" members of the public using the park feel when they are in that area of the park. In particular the area impacted is the area of highest family and group use within the park. To make the concept of openness slightly more concrete let us consider an example. If you stand at the corner of Lonsdale and 13<sup>th</sup> Street you feel hemmed in. On the flip side there are a lot of amenities and residences there so this constriction can be seen as justified. Now consider standing in the middle of Grand Boulevard Park. Here you immediately feel the open spaces and unobstructed views. Though the section of Victoria Park that will be affected does not have a total feeling of openness it is very close to that ideal because of setbacks and spread out buildings. The proposal will significantly decrease the feeling of openness of the key area of the park.

Thirdly, the proposal will set an incredibly dangerous precedent. At the meeting where the proposal was originally rejected there was a statement by council that setting a precedent is not true. Perhaps you will forgive my skepticism because I am given to understand that there are two potential similar

infill proposals waiting in the wings. Is it council's desire to totally ignore the OCP through zoning exemptions and have Victoria Park and/or Green Necklace shrink in stature until it becomes little more than a bike path with a couple of trees instead of the open area that gives life to the center of the City. Is the Council prepared to set in stone that this is not precedent setting?

Before continuing, there are some terms that have come into use in connection with the re-submission of the proposal of which I take umbrage. One is the term "street wall". Supposedly this is a valuable thing and we are told follows many precedents elsewhere. The street wall is still a wall and will subtract from this particular area. Also I do not see any mention of which parks that were enhanced by the street wall concept. Most of the parks that I know of in Western Canada do not have street walls with the exception of one or two which are designed as English walled gardens or a classical Chinese garden. How can such a term (street wall) be justified or used to say that the obstructive building enhances the character of a very nice park? In addition the street wall is supposed to enhance the pedestrian experience. What about the pathways that run through the park itself?

Another term that seems to be floating around is "living edge". While the proposal has re-oriented the project so that the new residents will have a nicer access to the park, it benefits only the residents of the new buildings. To claim that it enhances the park for other North Vancouver residents is nonsensical.

In addition, the arithmetic connected with the value to the City of the low rental units for seniors seems a bit off. The contribution by Starlight seems to be \$36,000 per annum. Assuming inflation offset by the time value of money, it will take 100 years to get to \$36 million, also only assuming that the Hollyburn contract is not cancelled at any time.

Finally, and perhaps most important, an article in the June 6, 2019 edition of the Vancouver Sun calls into serious doubt the population projections that the City of Vancouver is using to drive its housing decision making. Is The City Of North Vancouver also being driven by this overstatement? If so can the City justify the allowances being made to get extra rental units that may not really be needed given current building efforts in progress?

From the above I sincerely urge the council to reject the revised proposal.

Respectfully yours,

Keith Corson,  
301, 140 East Keith Road  
North Vancouver,

**Subject:** FW: 151 Keith Rd East, North Vancouver

-----Original Message-----

From: Susan Brinton < >  
Sent: June-10-19 10:13 PM  
To: Karla Graham <kgraham@cnv.org>  
Subject: 151 Keith Rd East, North Vancouver

To the Mayor and Council Members

I am a resident of Keith Road East and I wish to strongly object to the proposed infill rezoning application at the above address. I strongly urge you all on Council to vote against this application.

In a busy city, we value Victory Park and the surrounding green space for its openness and the home it provides for birds and other wildlife. We appreciate the beauty and pleasure its trees, shrubs and flowers give to all nearby residents and North Van in general. This should not be encroached upon for any reason.

Allowing this rezoning application and erecting a 'street wall' is a terrible idea and will disrupt the light and open nature of the green space, sidewalks and pathways. Zoning regulations, particularly in residential neighbourhoods, exist for a reason and one is to limit development at the expense of neighbourhood green spaces. Allowing this application would set a dangerous precedent and lead to further developer requests to encroach and build 'street walls' along Keith Road.

You would lose my support and my vote as Mayor and Council members if you allow this type of application to proceed.

Sincerely,  
Susan Brinton  
Resident  
1403-140 Keith Road East,  
North Vancouver



June 8, 2019

North Vancouver City Hall  
141 West 14<sup>th</sup> Street,  
North Vancouver, BC V7M 1H9

Subject: 151 Keith Road East – Rezoning Application

To: Mayor Buchanan and Councillors Back, Bell, Girard, Hu, McIlroy and Valente

I want to express my total disagreement with this application and request Council to reject it.

Starlight submitted a very similar request that went to Council a year ago which was not accepted. Now a slightly modified proposal has come forward again. Starlight is obviously hoping the new Council will not be aware of the community opposition.

This proposal completely ignores the bylaws that require buildings to be set back 25 feet from the property lines. These bylaws are in place for good reasons and need to be respected. They ensure that buildings that surround the park are spaced so that sunlight is not cut off and buildings do not overshadow the park. I have seen the description in the application saying that buildings set close to the park will 'provide a sense of enclosure'. I have never read such nonsense. That would be the destruction of the park! The application also describes the building on Keith which would be 45 feet high as 'human scale'. Hardly - this is totally the opposite.

There are approximately ten older rental buildings around the park. The owners of these buildings will be watching this decision. If Council allows owners to build covering the 25 foot setback, the whole park area will soon be wall-to-wall buildings - 'street walls'. These older buildings are well kept and are some of the most affordable living accommodation we have in our City. We cannot do anything that will encourage these buildings to be torn down and replaced with more 'market rentals' which are unaffordable for most people.

The offer being put forward by Starlight to allow four affordable units out of the 40 additional units is totally weighted to provide Starlight with generous profits. They are targeting monthly rents of \$1,550 for 400 square foot units and \$3,200 for 1,100 square foot units. They will still get at least \$800 for each of the four studio SAFER units (including a taxpayer subsidy on each). Starlight would collect over \$800,000 in rent from these 40 units and would only forego \$36,000 income on the 4 units. This is no deal.

Please stop this travesty on June 17. This proposal is unacceptable for our City.

Ellen Henshaw



401 – 140 Keith Road East,  
North Vancouver, BC V7L 4M9

**Subject:** FW: Proposed Development on East 6th Street

**From:** Dean Rath < >

**Sent:** June-12-19 5:50 AM

**To:** Submissions <input@cnv.org>; David Johnson <djohnson@cnv.org>

**Subject:** Proposed Development on East 6th Street

Re: Zoning Agreement Bylaw, 2018, No. 8618  
Housing Agreement Bylaw, 2018, No. 8622

Dear Council

On the North side of 6th Street East, between Lonsdale Avenue and St. Georges Avenue, there are already three high density towers adjacent to each other. Adding 2 infill buildings with another 40 residents to the property of the high density tower in the middle, seems to me overly ambitious and greedy. It would be an addition to an already very densely-populated small stretch of land. What next, more condominiums arching across the three tower-rooftops or some other crazed idea for ever-more-dense housing?

Since the addition of the third adjacent tower, completed in 2018, (160 East 6th Street), I have noticed the already high premium for street parking on 6th Street become even worse. How? People seem to be squeezing their vehicles into tighter and tighter spaces. I marvel at times at how some vehicles manage to leave on occasions when there are only inches of space between the vehicle in front and in back of them. On Sunday May 26, 2019, I saw a vehicle with only 6 inches of space between it and the car parked in front of it. I have never noticed such 'sandwiching' anywhere else in the City of North Vancouver.

Yes, the 40 new residents would have their own parking "within the existing parking structure" but their guests would strain even further the already very in-demand street parking on this block of 6th Street East between Lonsdale Avenue and St. Georges Avenue.

It seems to me that this would create an even more untenable situation. How long before someone reacts violently as they are unable to maneuver out from their parking spot on the street? Increased fender benders already seem inevitable.

Plus, often, people are parked in front of the fire hydrant at The Arlington apartment building at 141 East 6th Street. This poses a danger to access to the hydrant. And the situation would certainly not be improved with even more people clamoring for street parking space.

Question: If this project went through, where would all of the people from the 151 East Keith Road high rise tower park while their underground parking is being dug up? What a nightmare that would create.

Finally, the apartment building just west of the three high density towers on East 6th Street is scheduled for demolition in the future.

No doubt, another high density tower will be erected. That will make 4 towers in a row, side-by-side, on this block. Is there another residential block in the City of North Vancouver with so many high density towers on one street? I do not think so.

I implore you to consider how densely populated this section of 6th Street between Lonsdale and St. Georges already is.

Please vote down this housing proposal.

Thank you.

Best,

Dean Rath

#202 – 141 East 6th Street,  
North Vancouver, BC, V7L 1P1

**To the Mayor and Council - Input for the Public Hearing – June 17, 2019**

I have many issues with this application and request Council to reject it.

A similar rezoning application was brought to Council in March 2018 and was soundly rejected by the community and was defeated in Council. It has now returned in a slightly modified form containing some very questionable comments and financial considerations.

**1. This development proposal violates our stated OCP goals and objectives.**

- CNV OCP Goals and Objectives Section 1.3, one of the first points states:  
**‘Ensure that new development is compatible with the established urban form.’**
- However, this proposal is not compatible with this established neighbourhood. In the ‘Public Realm’ section of this application a number of statements are included seemingly supporting the incompatible building forms being proposed - including the following:
  - this development proposes providing ‘an active edge and street wall to the public realm’;
  - and ‘consistent with many great and large parks across the country, there is a need to provide a sense of enclosure through the provision of a street wall’;
  - and ‘the proposed development incorporates a mid-rise form that is human scaled’.

Where is this coming from? I do not see or want ‘street walls’ around any of our parks! I do not see ‘street walls’ in any park in Vancouver or anywhere else in Canada. Our OCP does not include ‘street walls’ for any park.

And when did buildings 59 feet high set only 8 feet from the property line become ‘human scaled’?

These statements show exactly why this proposal does not belong in our City. The bylaw requires setbacks of 25 feet in order to enhance the park itself. This is important for the enjoyment of all the people from blocks around who come to this park.

**2. Estimated Income for the Owner versus Density Bonus Benefit for our City**

- The Density Bonus Policy ‘seeks to ensure the City receives value for additional density’;
  - the report calculations show the **Density Value as \$3.45 million.**
- However, based on target rental rates provided (\$1,550 for 400 sq. ft. studio units, \$3,200 for 1,100 sq. ft. townhouses), the 40 units will bring in about \$825,000 per year, (including the 4 units supported by the SAFER program at \$800 per month). The **total annual rent to be foregone by Starlight is \$36,000.**

**How does this translate into a \$3.45 MILLION value for our City? – in 100 years?**

**3. The Housing Action Plan is not necessarily in perpetuity**

- The report states ‘rents would revert to mid-market rates in accordance with City Policy if the contract with Hollyburn Family Services is terminated’.
- If this happens, the value from Starlight would be even less – the rent foregone would drop from \$36,000 to \$20,000 per year – an even worse deal.



#### 4. Positioning of Infill Buildings

- The setbacks required on this property are 25 feet from the lot lines.
- This proposal is asking for 8 feet on the north side; 5 feet on the south; and about 10 feet for most of the west side.
- About 75% of the units would be located on the 25' setback area – this is an extreme and unacceptable situation.

#### 5. Advisory Design Panel Review – from Feb. 15, 2017 – no further review has been done

- The 2017 review comments several times that buy-in by the neighbouring building (123 East Keith) would be necessary – including the following:
  - ‘I think we are allowing this building owner to do something which is not really in the zoning.’
  - ‘The setbacks are aggressive and neighbours to the east and west need to be satisfied.’
- **There is no buy-in from their building or from others around the park.**

#### 6. Precedent Setting with Unwanted Consequences

- Allowing such reduced setbacks would cause other building owners to reconsider their options.
- There is already a development application in for 185 East Keith (80 rental units) which would require reduced setbacks. The owners of other well maintained, more affordable, older rental buildings all around the park (**14 buildings with 560 rental units – home to more than 1,000 people**) will be watching this requested bylaw change. If accepted, this change could lead to massive reconstruction and **loss of our most affordable rentals – the exact opposite to what is needed.**

#### 7. Community Feedback

- The people who live in the immediate vicinity are totally opposed to the reduced setbacks and have major concerns with the impact on the park – both immediate and future.

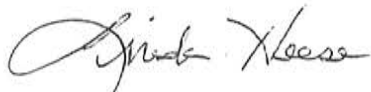
#### Summary:

Our City already has over 2,000 units of rental housing built or in process – just counting mid and high-rise buildings – in the last seven years. We are providing substantial amounts of rentals.

This a Market Rental Project with only 4 units being offered at a subsidized rate in return for a multi-million dollar income stream for the owner. **The benefit is insufficient to support such a major variance from the 25 foot setback.** The community would be supportive of a small infill project but the majority of the footprint needs to fit within the buildable area on this lot, along with providing some SAFER units.

This is not a good deal financially for the City, it is not supported by the people who live around the park, and it could trigger a wave of proposals from other building owners who will demand similar variances. A walled-in Victoria Park is certainly not my vision for this City and I hope it is not the vision for the majority on council. Please vote against this application.

Linda Heese- #1501 - 140 Keith Road East

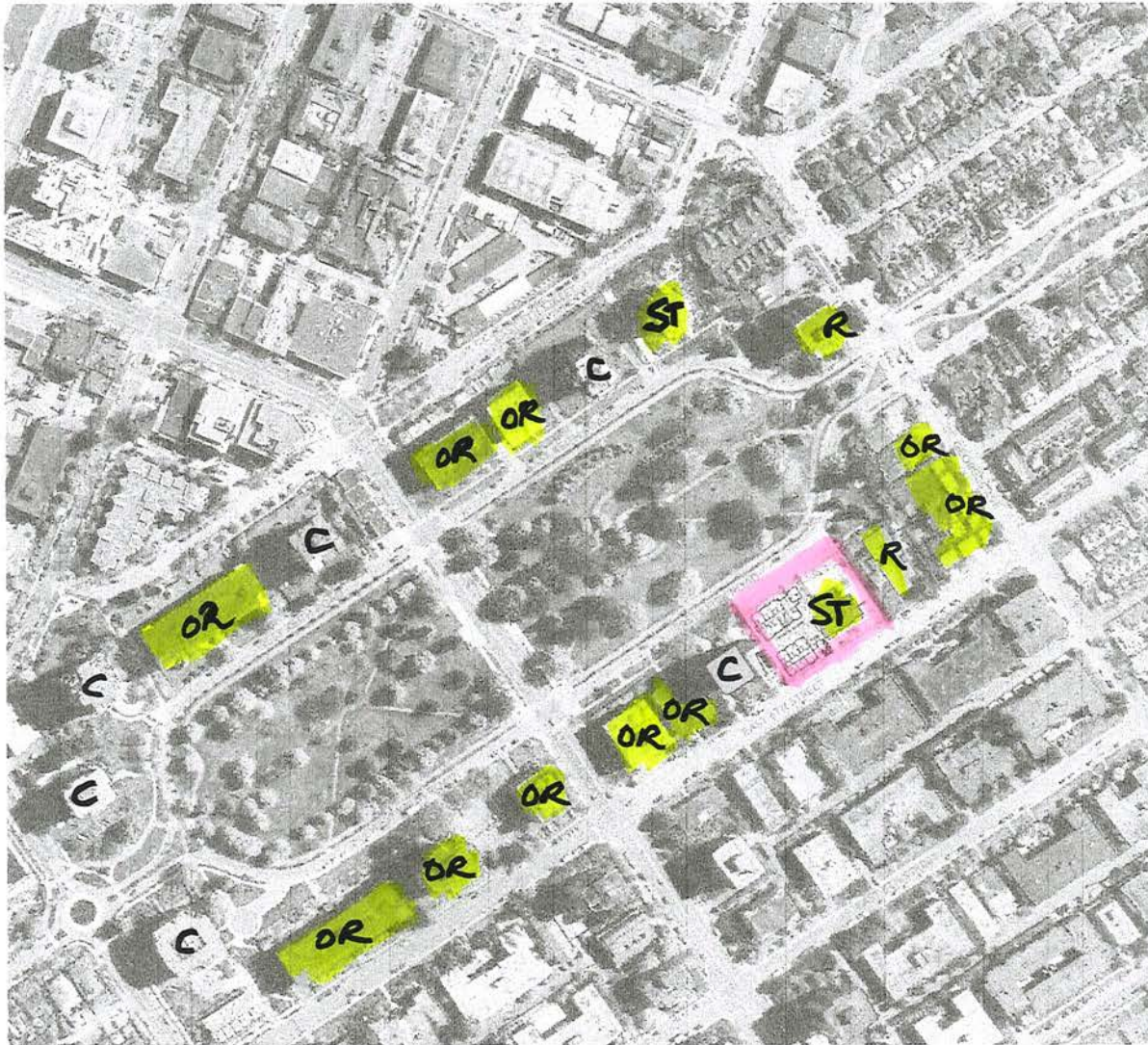




- red - property line
- orange - 25' setback







## AREA PLAN

1" = 200' 0"

### Rental Buildings

- yellow highlight
- 904 rental units

ST - Starlight buildings  
- rental

R - Rental

OR - Older Rental

C - Grade





## NOTICE OF PUBLIC HEARING

**WHO:** Urban Systems

**WHAT:** Zoning Bylaw, 1995, No. 6700, Amendment  
Bylaw, 2018, No. 8618 (CD-702)

Housing Agreement Bylaw, 2018, No. 8622

**WHERE:** 151 East Keith Road

**WHEN:** Monday, June 17, 2019 at 7:00 pm  
Council Chamber, City Hall  
141 West 14<sup>th</sup> Street, North Vancouver

Notice is hereby given that Council will consider:

**Zoning Amendment Bylaw, 2018, No. 8618**  
**Housing Agreement Bylaw, 2018, No. 8622**

to rezone the subject property from a High Density Apartment Residential (RH-1) Zone to a Comprehensive Development 702 (CD-702) Zone to permit the development of 2 infill buildings that would add 40 residential rental units to the existing tower. The infill buildings would add 0.88 FSR to the property, with the tallest building being approximately 18.1 metres (59.3 feet) tall. A total of 77 parking stalls would be located within the existing parking structure.

All persons who believe they may be affected by the proposal will be afforded an opportunity to be heard in person and/or by written submission. Written or email submissions must include your name and address and should be sent to the City Clerk, at [input@cnv.org](mailto:input@cnv.org), or by mail or delivered to City Hall. Submissions must be received no later than 4:00 pm, Monday, June 17, 2019, to ensure their availability to Council at the Public Hearing. Once the Public Hearing has concluded, no further information or submissions can be considered by Council.

The proposed Zoning Amendment and Housing Agreement Bylaws and background material will be available for viewing at City Hall between 8:30 am and 5:00 pm, Monday to Friday, except Statutory Holidays, from June 7, 2019, and online at [cnv.org/PublicHearings](http://cnv.org/PublicHearings).

Please direct any inquiries to **David Johnson**, Development Planner, at [djohnson@cnv.org](mailto:djohnson@cnv.org) or **604-990-4219**.



141 WEST 14TH STREET / NORTH VANCOUVER / BC / V7M 1H9  
T 604 985 7761 / F 604 985 9417 / [CNV.ORG](http://CNV.ORG)





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# THE CORPORATION OF THE CITY OF NORTH VANCOUVER

## BYLAW NO. 8618

### A Bylaw to amend “Zoning Bylaw, 1995, No. 6700”

The Council of The Corporation of the City of North Vancouver, in open meeting assembled, enacts as follows:

1. This Bylaw shall be known and cited for all purposes as “**Zoning Bylaw, 1995, No. 6700, Amendment Bylaw, 2018, No. 8618**” (Urban Systems / IMH 151 E Keith Apartments Ltd., 151 East Keith Road, CD-702).
2. Division VI: Zoning Map of Document “A” of “Zoning Bylaw, 1995, No. 6700” is hereby amended by reclassifying the following lots as henceforth being transferred, added to and forming part of CD-702 (Comprehensive Development 702 Zone):

Lot	Block	D.L.	Plan	
B	114	274	15049	from RH-1

3. Part 11 of Division V: Comprehensive Development Regulations of Document “A” of “Zoning Bylaw, 1995, No. 6700” is hereby amended by:

- A. Adding the following section to Section 1100, thereof, after the designation “CD-701 Comprehensive Development 701 Zone”:

“CD-702 Comprehensive Development 702 Zone”

- B. Adding the following to Section 1101, thereof, after the “CD-701 Comprehensive Development 701 Zone”:

“CD-702 Comprehensive Development 702 Zone”

In the CD-702 Zone, permitted Uses, regulations for permitted Uses, regulations for the size, shape and siting of Buildings and Structures and required Off-Street Parking shall be as in the RH-1 Zone, except that:

- (1) The permitted Principal Use on the Lot shall be limited to:

- (a) Rental Apartment Residential Use:

- i. Accessory Home Occupation Use subject to section 507(5), (6) and (7) of this bylaw;
- ii. Accessory Off-Street Parking Use;
- iii. Accessory Home Office Use;

- (2) Three Principal Buildings shall be permitted on one Lot;

- (3) Gross Floor Area

The maximum Gross Floor Area may be increased upon entering into a Housing Agreement with the City from a base density of 2.3 FSR to a maximum of 3.17 FSR as follows:

BASE DENSITY			
OCP Schedule 'A'		2.30 FSR	
ADDITIONAL (BONUS) DENSITY			
ADDITIONAL DENSITY CATEGORY	DESCRIPTION	ADDITIONAL DENSITY (BONUS)	POLICY REFERENCE
100% Rental Housing	Secured rental apartment building (all units)	0.88 FSR	OCP Section 2.2
TOTAL		3.17 FSR	

Such that the total effective on-site Gross Floor Area is not to exceed 3.17 FSR;

- (4) The north west building shall be sited as follows:
  - (a) 3. metres (9.8 feet) from the west property line;
    - (i) Notwithstanding 4(a), the secondary stairway may extend closer to the west property line, but the setback shall not be less than 1.65 metres (5.41 feet);
  - (b) 2.4 metres (8.0 feet) from the north property line;
- (5) The south west building shall be sited as follows:
  - (a) 3.0 metres (9.8 feet) from the west property line;
  - (b) 1.6 metres (5.2 feet) from the south property line;
- (6) The fifteen (15) storey tower shall be sited as follows:
  - (a) 14 metres (45.9 feet) from the north property line;
  - (b) 8 metres (26.2 feet) from the east property line;
  - (c) 65 metres (19.7 feet) from the south property line;
  - (d) 29 metres (95.1 feet) from the west property line;
- (7) The minimum separation distance between the north west and the south west building shall be no less than 8 metres (26.2 feet).
- (8) Figure 9-2 shall be varied to permit a parking stall width of no less than 2.4 metres (8.0 feet);
- (9) Section 906(3)(a)(i)(a) to permit a parking stall width to be no less than 2.4 metres (8.0 feet);

- (10) All exterior finished, design and landscaping to be approved by the Advisory Design Panel.

READ a first time on the 12<sup>th</sup> day of March, 2018.

READ a second time on the 12<sup>th</sup> day of March, 2018.

SECOND READING RESCINDED on the 13<sup>th</sup> day of May, 2019.

READ a second time, as amended, on the 13<sup>th</sup> day of May, 2019.

READ a third time on the <> day of <>, 2018.

ADOPTED on the <> day of <>, 2018.

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MAYOR

---

CITY CLERK



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THE CORPORATION OF THE CITY OF NORTH VANCOUVER

BYLAW NO. 8622

**A Bylaw to enter into a Housing Agreement (151 East Keith Road)**

**WHEREAS** Section 483 of the *Local Government Act* R.S.B.C. 2015 c.1 permits a local government to enter into a housing agreement for rental housing;

**NOW THEREFORE** the Council of The Corporation of the City of North Vancouver, in open meeting assembled enacts as follows:

1. This Bylaw shall be known and cited for all purposes as “**Housing Agreement Bylaw, 2018, No. 8622**” (Urban Systems / IMH 151 E Keith Apartments Ltd., 151 East Keith Road, CD-702, Rental Housing Commitments).
2. The Council hereby authorizes the agreement substantially in the form attached to this bylaw between The Corporation of the City of North Vancouver and IMH 151 E Keith Apartments LTD with respect to the lands referenced as 151 East Keith Road, “Zoning Bylaw, 1995, No. 6700, Amendment Bylaw, 2018, No. 8618” (Urban Systems / IMH 151 E Keith Apartments LTD, 151 East Keith Road, CD-702).
3. The Mayor and City Clerk are authorized to execute any documents required to give effect to the Housing Agreement.

READ a first time on the 12<sup>th</sup> day of March, 2018.

READ a second time on the 12<sup>th</sup> day of March, 2018.

SECOND READING RESCINDED on the 13<sup>th</sup> day of May, 2019.

READ a second time, as amended, on the 13<sup>th</sup> day of May, 2019.

READ a third time on the <> day of <>, 2018.

ADOPTED on the <> day of <>, 2018.

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MAYOR

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CITY CLERK

## TERMS OF INSTRUMENT – PART 2

### HOUSING AGREEMENT & SECTION 219 COVENANT - RENTAL BUILDING

#### WHEREAS:

- A. The Owner is the registered owner of the Lands;
- B. Section 219 of the *Land Title Act* (British Columbia) permits registration of a covenant in favour of a municipality in respect of the use of land or the use of a building on or to be erected on land, that land is or is not to be built on except in accordance with the covenant and that land is not to be subdivided except in accordance with the covenant;
- C. Section 483 of the *Local Government Act* (British Columbia) permits a local government to, by bylaw, enter into a housing agreement that may include terms and conditions regarding the occupancy of the housing units identified in the agreement, including respecting the form of tenure of the housing units, the availability of the housing units to classes of persons, the administration and management of the housing units and the rents and lease, sale or share prices that may be charged;
- D. The City has enacted a bylaw authorizing this Agreement; and
- E. The Owner and the City wish to enter into this Agreement pursuant to section 219 of the *Land Title Act* and section 483 of the *Local Government Act*.

**NOW THEREFORE** in consideration of the sum of \$10.00 now paid by the City to the Owner and for other good and valuable consideration (the receipt and sufficiency of which the Owner hereby acknowledges), the Owner and the City covenant each with the other as follows:

#### 1. INTERPRETATION

##### 1.1 Definitions

In this Agreement:

- (a) **“Affordable Rent”** means an amount equal to of the “Private Apartment Average Rents” for the corresponding bedroom type in the City of North Vancouver as published by Canada Mortgage and Housing Corporation on its Housing Market Information Portal, using the most recently available Canada Mortgage and Housing Corporation information at the time the applicable Tenancy Agreement is entered into.
- (b) **“Agreement”** means, together, Part 1 and these Terms, including schedules attached hereto.
- (c) **“Building”** means two separate buildings to be constructed on the Lands following the registration of this Agreement on the LTO (the **“New Buildings”**), to be comprised of:
  - i) a four storey building at the north west corner of the Lot, containing 31 studio units;

- ii) a four storey townhouse building at the south west corner of the Lot, containing 3 studio units and 6 three bedroom units; and
  - iii) 2 levels of below-ground parking.
- (d) **“City”** means the Corporation of the City of North Vancouver.
  - (e) **“Claims and Expenses”** means all actions, causes of action, suits, judgments, proceedings, demands and claims, whether at law or in equity, losses, damages, expenses and costs (including legal fees and disbursements on an indemnity basis) of any kind or nature whatsoever, at law or in equity, for any damage, loss, injury or death.
  - (f) **“CPI”** means the All-Items Consumer Price Index for Vancouver, B.C. published from time to time by Statistics Canada, or its successor in function.
  - (g) **“Daily Amount”** means \$50.00 per day as of January 1, 2018 adjusted annually thereafter by adding thereto an amount calculated by multiplying \$100.00 by the percentage change in the CPI since January 1, 2018, to January 1 of the year that a notice of default referred to Section 6.1(b) is delivered to the Owner by the City.
  - (h) **“Household”** means the individuals who occupy a Mid-Market Rental Unit.
  - (i) **“Household Income”** means the aggregate of income from all sources of all the occupants of an Affordable Housing Unit, based on the tax returns filed by such occupants with Canada Customs and Revenue Agency for the most recent taxation year.
  - (j) **“Lands”** means those lands and premises legally described in Item 2 of Part 1.
  - (k) **“LTO”** means the Vancouver/New Westminster Land Title Office.
  - (l) **“Mid-Market Income Level”** means an amount equal to the Affordable Rent for a Mid-Market Rental Unit with respect to the proposed tenancy agreement, multiplied by 12 and divided by 0.3 (i.e. 30%).
  - (m) **“Market Rental Units”** means all residential dwelling units in the New Buildings other than the Mid-Market Rental Units.
  - (n) **“Mid-Market Rental Units”** means those residential dwellings designated by the owner as Mid-Market Rental Units pursuant to this Agreement.
  - (o) **“Owner”** means the person described in Item 5 of Part 1.
  - (p) **“Part 1”** means the General Instrument – Part 1 (*Land Title Act* Form C) to which these Terms of Instrument are attached as Part 2.
  - (q) **“Rental Purposes”** means an occupancy or intended occupancy which is or would be governed by a tenancy agreement as defined in the *Residential Tenancy Act* (British Columbia).
  - (r) **“Rental Units”** means all residential dwellings in the Buildings.



- (s) **“Rent Charge”** has the meaning set out in Section 6.1.
- (t) **“Section 219 Covenant”** means a covenant pursuant to Section 219 of the *Land Title Act*.
- (u) **“Tenancy Agreement”** means an agreement, whether written or oral, express or implied, between the Owner and a tenant respecting possession or occupancy of a Mid-Market Rental Unit.

## 1.2 Interpretation

In this Agreement:

- (a) words importing the singular number include the plural and vice versa and words importing the neuter gender include the masculine and the feminine genders;
- (b) the division of this Agreement into articles and sections and the insertion of headings are for convenience only and will not affect the construction or the interpretation of this Agreement;
- (c) references to any article, section or schedule will, unless the context otherwise requires, mean that article, section or schedule of this Agreement;
- (d) every reference to each party is deemed to include the heirs, executors, administrators, personal representatives, successors, servants, employees, agents, contractors, officers, licensees and invitees of such party, wherever the context so requires or allows;
- (e) the words “include” and “including” are to be construed as meaning “include without limitation” and “including without limitation”;
- (f) all payments to be made will be deemed to be payments in lawful currency of Canada;
- (g) reference to “business day” means all days other than Saturday, Sunday and statutory holidays in the Province of British Columbia;
- (h) reference to “party” and “parties” means the one or more parties to this Agreement, as the context demands;
- (i) reference to a whole, for example, the “Lands”, includes reference to a portion thereof; and
- (j) unless expressly stated otherwise, the term “enactment” has the same meaning as under the *Interpretation Act* (British Columbia) and reference to a specific enactment shall be to that enactment, as amended or replaced from time to time, unless otherwise expressly provided.

## 1.3 Acknowledgements

The Owner acknowledges that:

- (a) nothing in this Agreement will relieve the Owner from any obligation or requirement arising under any enactment, including City bylaws, in respect of the use, subdivision and development of the Lands; and
- (b) nothing contained or implied in this Agreement will prejudice or affect the City's rights, powers, duties or obligations in the exercise of its functions pursuant to the *Local Government Act* (British Columbia), the *Community Charter* (British Columbia) or other enactment, including City bylaws.

## **2. SECTION 219 OF THE LAND TITLE ACT**

### **2.1 Section 219 Covenant**

The Owner hereby covenants and agrees with the City, as a covenant in favour of the City pursuant to Section 219 of the *Land Title Act* (British Columbia), it being the intention and agreement of the Owner that the provisions in this Agreement be annexed to, and run with and be a charge upon the Lands, that notwithstanding the enactment of the Rezoning Bylaw, the Lands will be subdivided, used, built and used only in strict compliance with the terms and conditions of this Agreement.

### **2.2 Section 219 Indemnity**

As an indemnity pursuant to section 219(6) of the *Land Title Act* (British Columbia), the Owner shall indemnify the City against all Claims and Expenses arising out of or, in any way related to or that would not or could not be sustained but for, this Agreement, including, but not limited to, the exercise by the City of any rights granted in this Agreement, or any restrictions imposed pursuant to this Agreement, except if resulting from a negligent action or omission by the City.

### **2.3 Registration of the indemnity in the LTO**

At the City's direction, the indemnity contained herein will be filed for registration in the LTO under a separate registration number from the Section 219 Covenant contained in this Agreement.

### **2.4 Release**

The Owner hereby releases the City from all Claims and Expenses arising out of or in any way related to this Agreement, including, but not limited to, the exercise by the City of any rights granted in this Agreement, or any restrictions imposed pursuant to this Agreement, except if resulting from a negligent action or omission by the City.

### **2.5 Survival of release and indemnity**

The indemnity and release in this Section 2 will survive any discharge, expiration, termination or cancellation of this Agreement.

## **3. RENTAL UNITS**

### **3.1 Rental Units**

The Rental Units shall only be used for Rental Purposes.

### **3.2 No Subdivision**

The Lands shall not be subdivided pursuant to the *Land Title Act* (British Columbia), the *Strata Property Act* (British Columbia) or otherwise without the prior written consent of the City, except that this Section 3.2 shall not prevent the Owner from proceeding to subdivide the Lands to create one or more air space parcels (but not strata lots), provided that each Building is entirely contained within a single air space parcel.

### **3.3 No Separate Sale or Transfer**

In the event that the Lands are subdivided (pursuant to the *Land Title Act* (British Columbia), the *Strata Property Act* (British Columbia) or otherwise), in accordance with section 3.2, with the written consent of the City or in contravention of Section 3.2, the resulting parcels (including strata lots) shall not, without the prior written consent of the City, be sold or otherwise transferred separately.

## **4. MID-MARKET RENTAL UNITS**

### **4.1 Mid-Market Rental Unit Designation**

Four of the Rental Units within the New Buildings shall be used, occupied and rented in accordance with the requirements of this Section 4 and shall constitute the Mid-Market Rental Units. Before using or occupying the New Buildings, and before issuance of an occupancy permit for the New Buildings, the Owner shall designate, in writing, to the City which the Rental Units in the New Buildings are the Mid-Market Rental Units.

### **4.2 Mid-Market Rental Unit Mix**

The Mid-Market Rental Units shall meet the following 'unit mix' requirements, and the Owner's designation under section 4.1 shall comply with such requirements:

- (a) 4 studio units;

### **4.3 Rent Restrictions & Tenure Requirements**

- (a) Mid-Market Rental Units shall only be rented to and occupied by a Household having a Household Income less than or equal to the Mid-Market Income Level.
- (b) Before entering into a Tenancy Agreement for a Mid-Market Rental Unit, the Owner shall:
  - (i) obtain from the prospective tenant, in writing, the names of all members of the Household that will occupy the Mid-Market Rental Unit;
  - (ii) obtain, from the prospective tenant, the tax returns filed with Canada Customs and Revenue Agency for the most recent taxation year for each individual identified under Section 4.3(a)(i) who was required to file a tax return for that taxation year; and
  - (iii) take such other steps as may be reasonably necessary, in the opinion of the Owner, to confirm that the Mid-Market Rental Unit shall be occupied by

a Household having a Household Income less than or equal to the Mid-Market Income Level.

- (c) If a Tenancy Agreement is entered into in respect of a Mid-Market Rental Unit, the Owner shall retain copies of all documents obtained pursuant to Section 4.3(b) and will make and retain records of any information obtained pursuant to Section 4.3(b) in respect of such tenancy for a period of no less than one year following the expiration or earlier termination of such Tenancy Agreement, subject to any applicable restrictions under the *Personal Information Protection Act* (British Columbia) or other applicable privacy legislation, and the Owner shall, within 14 days following a request from the City from time to time, provide copies of such documents and records to the City, subject to any applicable restrictions under the *Personal Information Protection Act* (British Columbia) or other applicable privacy legislation.
- (d) The Owner shall not enter into a Tenancy Agreement for a Mid-Market Rental Unit unless the requirements of Sections 4.3(b) and 4.3(c) have been satisfied in relation to such Tenancy Agreement. If those requirements have been satisfied, the Owner will have no liability to the City in the event that any information provided by the tenant to the Owner under Section 4.3(b) in relation to the Tenancy Agreements proves to be false or if the Household Income of the occupants of the Mid-Market Rental Unit increases during the term of the Tenancy Agreement.
- (e) The monthly rent payable for a Mid-Market Rental Unit shall not exceed the Affordable Rent, except that the Owner may increase the rent under a Tenancy Agreement in accordance with the provisions of the *Residential Tenancy Act* (British Columbia).
- (f) The Owner shall not require a tenant of a Mid-Market Rental Unit to pay any extra charges or fees for use of any common property, limited common property, or other common area, for property taxes or for sanitary sewer, storm sewer, water utilities and similar services, except in respect of the following:
  - (i) any utilities not included in the Tenancy Agreement, including without limitation, television/cable, internet and telephone;
  - (ii) any utilities for which individual meters are provided by the Owner, from time to time;
  - (iii) parking;
  - (iv) use of bicycle storage lockers or other facilities;
  - (v) use of storage lockers; and
  - (vi) booking for exclusive use of any common amenity spaces as may be made available for such booking by the Owner from time to time.
- (g) Mid-Market Rental Units shall be occupied only pursuant to a written Tenancy Agreement;



- (h) The initial term of every Tenancy Agreement for a Mid-Market Rental Unit shall be for 1 year.
- (i) Every Tenancy Agreement for a Mid-Market Rental Unit will identify all members of the Household and will stipulate that anyone not identified in such Tenancy Agreement will be prohibited from residing at the Mid-Market Rental Unit for more than 30 consecutive days or more than 45 days total in any calendar year.
- (j) Every Tenancy Agreement for a Mid-Market Rental Unit shall provide that the tenant shall not sublease the Mid-Market Rental Unit or assign the Tenancy Agreement, without the consent of the Owner.
- (k) The Owner shall not consent to a sublease of a Mid-Market Rental Unit or to an assignment of a Tenancy Agreement in respect of a Mid-Market Rental Unit, except if the requirements of Sections 4.3(b) and (c) are first satisfied in relation to the proposed subtenant or assignee.
- (l) The Owner shall deliver a copy of every Tenancy Agreement in respect of a Mid-Market Rental Unit to the City within 14 days following a request from the City from time to time.
- (m) The restrictions under Sections 4.3(a), (b) and (c) shall not apply to a Tenancy Agreement in respect of a Mid-Market Rental Unit entered into within 6 months following the date of issuance of an occupancy permit for the New Buildings if the tenant under the Tenancy Agreement was a tenant of the Existing Building on the date of registration of this Agreement.
- (n) The requirements of this Section 4.3 shall cease to apply from and after the 10th anniversary of the date this Agreement is registered in the LTO.
- (o) The Owner shall, within 14 days following a written request from the City, but no more than once each calendar year, provide the City with a statutory declaration in a form determined by the City, sworn by the Owner, or by an officer or director of the Owner if the Owner is a corporation, before a commissioner for taking of affidavits for British Columbia under the Evidence Act (British Columbia) setting out the current monthly rent amounts for each Tenancy Agreement in respect of a Mid-Market Rental Unit as of the date of the statutory declaration.

#### **4.4 Partnership with Hollyburn Family Services**

Notwithstanding Section 4.3, the Owner may enter into a partnership (by way of a signed, written agreement) to secure the four Mid-Market Units with Hollyburn Family Services, or similar organization approved in writing by the City, for the purposes of providing affordable housing to vulnerable seniors through the SAFER subsidy, which purposes shall be expressly stated in the agreement and if, and for so long as a Mid-Market Rental Unit is subject to such an agreement, the restrictions under section 4.3 shall not apply to such Mid-Market Rental Unit. The Owner shall provide to the City a copy of every such lease, and every amendment thereto, promptly following the execution thereof.

### **5. DEFAULT AND REMEDIES**

#### **5.1 Default and remedies**

- (a) If the Owner fails to comply with any of its obligations under this Agreement, the City may notify the Owner in writing (at the address shown on title to the Lands in the LTO at the relevant time) that the Owner is in default, describe the default, and instruct the Owner to correct the default within 15 days of receiving the notice, or such longer period as the City may consider necessary to correct the default given the nature of the default (the “**Cure Period**”).
- (b) Upon receipt of a notice from the City under Section 5.1(a), the Owner will diligently proceed to correct the default within the Cure Period.
- (c) The Owner agrees that the public interest in ensuring that all of the matters described in this Agreement are complied with strongly favours the award of a prohibitory or mandatory injunction, or an order for specific performance or other specific relief, by the Supreme Court of British Columbia at the instance of the City, in the event of an actual or threatened breach of this Agreement.
- (d) No reference to or exercise of any specific right or remedy by the City, shall prejudice or preclude the City from exercising any other right or remedy, whether allowed at law or in equity or expressly provided for in this Agreement, and no such right or remedy is exclusive or dependent upon any other such remedy and the City may from time to time exercise any one or more of such remedies independently or in combination.

## 5.2 City may perform Owner’s obligations

Without limiting Section 5.1, if, following notice from the City under Section 5.1(a), the Owner fails to correct the default within the Cure Period, the City may (but is not obligated to), upon giving to the Owner five days’ prior written notice describing the default, or immediately in the case of an emergency, perform such obligations, for and on behalf of and at the sole cost of the Owner.

## 5.3 Owner will reimburse City for its costs

Upon receipt of written demand for same, the Owner will pay to the City all costs incurred by the City under Section 5.2, including a 30% administrative fee.

# 6. RENT CHARGE

## 6.1 Rent Charge

- (a) The Lands are subject to a daily rent charge (the “**Rent Charge**”), payable by the Owner on the first of each calendar month (the “**due date**”), in the amount equal to the Daily Amount, which Rent Charge is deemed to accrue day to day, from and after the Rent Charge Default Date until the Default Correction Date (each as defined in subsection (b) below).
- (b) The Rent Charge shall abate against the Lands, and no amounts will accrue or be payable by the Owner thereunder, until such time (the “**Rent Charge Default Date**”) as the Owner does not comply with section 4.3(e) in relation to a Mid-Market Rental Unit and the Owner does not, within Cure Period, correct that default by reducing the rent under the Tenancy Agreement to the Affordable Rent

plus any increase permitted under the *Residential Tenancy Act* (British Columbia). For clarity, the Rent Charge shall to accrue on a day to day basis for every day that a default continues after Cure Period until the Owner has corrected that default by reducing the rent under the Tenancy Agreement to the Affordable Rent plus any increase permitted under the *Residential Tenancy Act* (British Columbia) (the "Default Correction Date").

- (c) Any arrears of Rent Charge shall bear interest from the due date until payment at the rate of eleven per cent (11%) per annum and shall be a charge upon the Lands in the same manner as the Rent Charge hereby charged on the Lands.
- (d) The Rent Charge ranks prior to all other financial charges and encumbrances registered at any time against the Lands.
- (e) The Rent Charge is granted both under section 219(6)(b) of the *Land Title Act* (British Columbia) as an integral part of the Section 219 Covenant contained in this Agreement, and as a fee simple rent charge at common law.
- (f) The City may enforce and collect the Rent Charge by any combination or all of:
  - (i) an action against the Owner for the Rent Charge;
  - (ii) distraint against the Lands to the extent of the Rent Charge;
  - (iii) an action for appointment of receiver in respect of the Lands; or
  - (iv) an order for sale of the Lands

## **7. GENERAL**

### **7.1 Building manager**

If the Owner retains a building manager in respect of the Building, the Owner shall instruct and ensure that the building manager complies with the terms of this Agreement.

### **7.2 Severance**

If any portion of this Agreement is held invalid by a court of competent jurisdiction, the invalid portion will be severed and the decision that it is invalid will not affect the validity of the remainder of this Agreement.

### **7.3 Runs with the Lands**

The Section 219 Covenant (including the Rent Charge) herein will run with, and bind the successors in title to, the Lands and each and every part into which the Lands may be divided or subdivided, whether by subdivision plan, strata plan or otherwise.

### **7.4 Notice of Housing Agreement**

This Agreement constitutes both a covenant under section 219 of the *Land Title Act* and a housing agreement entered into under section 483 of the *Local Government Act*. The Owner acknowledges that the City is required to file a notice of housing agreement in the LTO against title to the Land; and once such a notice is filed, this Agreement binds all persons who acquire an interest in the Land as a housing agreement under section 483 of the *Local Government Act*.

### **7.5 Limitation on Owner's Obligations**

In accordance with section 219(8) of the *Land Title Act* (British Columbia), a person is not liable for a breach of this Agreement occurring after that person has ceased to be an owner of the Lands.

### **7.6 Further Assurances**

The parties will execute and do all such further deeds, acts, things and assurances that may be reasonably required to carry out the intent of this Agreement.

### **7.7 Waiver**

Waiver by the City of a default by the Owner will be in writing and will not be deemed to be a waiver of any subsequent or other default.

### **7.8 Enurement**

This Agreement will enure to the benefit of and be binding upon the parties hereto and their respective successors and assigns.

### **7.9 Priority**

The Owner will take all steps necessary to ensure that this Agreement is registered in the LTO in priority to all charges and encumbrances which may impair the covenants granted in this Agreement and, in any event, in priority to all financial charges.

### **7.10 Counterparts and Electronic Delivery**

This Agreement may be executed in any number of counterparts and delivered via facsimile or e-mail, each of which will be deemed to be an original and all of which taken together will be deemed to constitute one and the same instrument, provided that any party delivering this Agreement via facsimile or e-mail will deliver to the other party an originally executed copy of this Agreement forthwith upon request by the other party.



**IN WITNESS OF THIS AGREEMENT** the City and the Owner have executed this Agreement by signing the “Form C - General Instrument - Part 1” or “Form D – Executions Continued” attached hereto:

**THE CORPORATION OF THE CITY OF NORTH VANCOUVER**

\_\_\_\_\_  
MAYOR  
Linda C. Buchanan

\_\_\_\_\_  
CITY CLERK  
Karla D. Graham

**IMH 151 E KEITH APARTMENTS LTD**

\_\_\_\_\_  
Authorized Signatory

\_\_\_\_\_  
Printed Name

\_\_\_\_\_  
Authorized Signatory

\_\_\_\_\_  
Printed Name